



LAND REDEVELOPMENT COMMITTEE



Date: 11 May 2026
Item 2.1: Lot 1 (No. 154) Subiaco Road, Subiaco – 7-18 Storey Mixed Use Development
Submitted by: Matt Ryan

Information

Project Area: Subiaco East
Location: Lot 1 (No. 154) Subiaco Road, Subiaco
Owner: The State Housing Commission
Applicant: Community Housing Limited (CHL)
Proposal: 7-18 Storey Mixed Use Development

Purpose

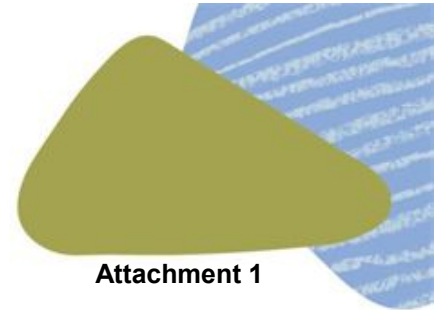
For the Subiaco Land Redevelopment Committee to consider an application for a 7-18 Storey Mixed Use Development at Lot 1 (No. 154) Subiaco Road, Subiaco.

Location Plan



Recommendation

That the Subiaco Land Redevelopment Committee resolves to recommend to the DevelopmentWA Board that it approves the 7-18 Storey Mixed Use Development at Lot 1 (No. 154) Subiaco Road, Subiaco subject to the recommended conditions and advice notes.



Attachment 1

PLANNING ASSESSMENT

Proposal

On 2 February 2026, a development application was received for a 7-18 Storey Mixed Use Development at Lot 1 (No. 154) Subiaco Road, Subiaco. The subject site is located in the Subiaco Oval Precinct of the Subiaco East Project Area, within the Subiaco Redevelopment Area.

The development application proposes four towers with heights ranging from seven to 18 storeys with a connected podium, comprising of:

- 300 affordable dwellings and 147 social dwellings;
- a ground floor office for on-site management;
- two ground floor tenancies of Café/Restaurant and Shop fronting Court Place;
- 308 parking bays comprised of 279 resident bays and 29 visitor bays; and
- amenities and landscaping on the ground floor and podium including 122 on-site trees and 21 retained mature trees.

Refer to Appendix 2 – Development Plans

The land is currently owned by the State Housing Commission (The Department of Housing and Works) and is proposed to be sold to Community Housing Limited (CHL), a registered Tier 1 Community Housing Provider (CHP). DevelopmentWA is overseeing transaction negotiations and agreement terms, including those associated with project funding secured through the Federal Government's Housing Australia Future Fund (HAFF). CHL is proposing to own and manage all the units within the development, including the social housing units.

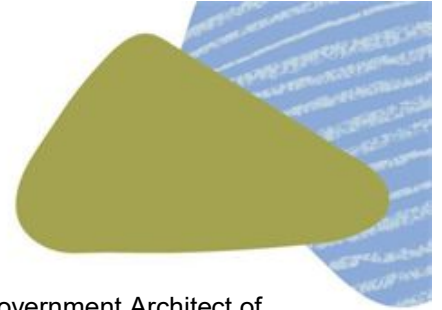
The affordable housing units will provide stable long term rental housing for low to moderate income earners, capped at a maximum of 74.9% of the market rental rate for the area and meet defined income thresholds as set by the Federal Government. Social housing tenants are low to very low-income earners sourced from the Public Housing Waitlist.

Previous Subiaco Land Redevelopment Committee Consideration

The current application was presented to the Subiaco Land Redevelopment Committee at a briefing session on 16 March 2026, where the following matters were queried:

- whether the Design Review Panel (DRP) feedback had been fully addressed through design changes to demonstrate Design Excellence;
- how built form and amenity impacts on podium landscaped communal spaces had been considered and addressed; and
- confirmation that the recommendations from the Crime Prevention Through Environmental Design (CPTED) report are incorporated into the final design.

In response to the above matters, the applicant has provided further information and updated plans. A summary addressing the applicant's response to the matters raised by the Committee is provided below.



Design Review and Design Excellence

Updated information and plans were provided to the DRP Chair, the Government Architect of Western Australia, who noted improvements to the design of the central forecourt, public realm amenity, building entry legibility and landmark building corner element. Based on the updated information, the Chair resolved that the proposal met the threshold of Design Excellence, subject to outstanding matters relating to material selection, CPTED and sustainability being addressed or met through conditions at the detailed design phase. It is recognised that these matters can be addressed through the working drawings (condition clearance) process in consultation with the DRP. A detailed response to DRP feedback is discussed in more detail in the Design Review Panel section of this report.

Podium Amenity

Two separate podium areas will provide residents with a space for gathering, with amenities such as playgrounds, herb gardens, BBQ's and grass areas. These areas are located between towers with an open aspect to the north to take advantage of natural light and to the south to take advantage of prevailing winds and views over Subiaco Oval. It is noted that the amenity areas will be shaded during early morning and late evening due to the location of towers, however it is considered these spaces will receive adequate sunlight during the day, delivering high-quality amenity.

A small number of dwellings have direct access to these podium levels via secondary entrances off external terrace spaces. The amenity of these private dwellings is protected with large planter boxes and gates restricting views and mitigating noise, and through the location of noise generating elements such as BBQ's and playground away from entrances. Use of these amenity spaces will be managed by the on-site CHP ensuring impacts are mitigated. A proposed condition, Condition 13, is recommended which requires an Operational Management Plan to be prepared which demonstrates how shared amenity spaces will be managed.

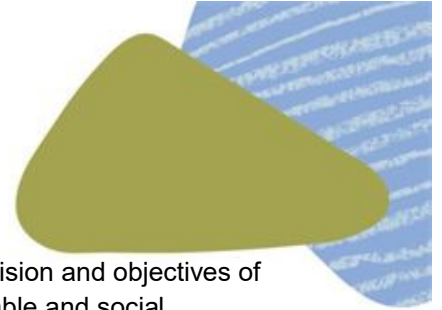
Crime Prevention Through Environmental Design

The final design has been informed by a CPTED report prepared by the architect DKO. This report was informed by advice from CHL given its experience in the social and affordable housing industry. The DRP has recommended that an independent CPTED report should be prepared by an external consultant given the scale of the development, however due to time constraints, a full report could not be provided.

Condition 10 is recommended requiring that a comprehensive, independent CPTED report is prepared prior to construction demonstrating how safety principles are embedded in the design. Condition 10 further recommends that the CTPED is to be subject to review by the DRP to ensure that any recommendations are appropriately incorporated into the design.

Assessment

The proposed development is generally consistent with the applicable Subiaco Redevelopment Scheme (the Scheme), Subiaco Redevelopment Area Development Policies and the Subiaco Oval and Railway Precinct Design Guidelines (the Design Guidelines).



The proposed development is strongly aligned with the Subiaco East vision and objectives of the Scheme and Design Guidelines, delivering 447 high-quality affordable and social dwellings in close proximity to public transport and local amenities. The design has been carefully considered to ensure a quality amenity outcome for future residents, while also providing a landmark building on the eastern edge of the site, fostering a strong sense of place for both residents and the community. The development will provide increased population to enhance the vitality of nearby activity centres within walking distance, while creatively reflecting local architectural styles and responding sympathetically to the surrounding urban form through its contemporary massing and design.

Discretionary Clause

Under Section 1.6 Discretionary Clause of the Design Guidelines, DevelopmentWA may approve a development application where it has departed from the acceptable outcomes, and when the alternative solution(s):

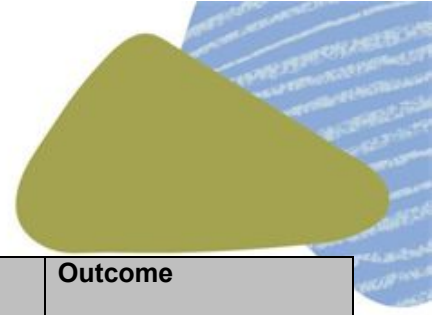
- a) clearly meets the relevant objectives of the Design Guidelines;
- b) forms part of an application which provides enhanced design quality, as determined by the appointed Design Review Panel; and
- c) delivers additional community and environmental benefits beyond that required by the Development Policies.

Alternative Lot Configuration

The Design Guidelines divide the subject site into four lots, with the envisaged Lots 8 to 10 (applicable to Towers 1 to 3) and Lot 11 (applicable to Tower 4) each having distinct built form requirements. The current proposal is for an unsubdivided, consolidated development outcome across the site, featuring four towers atop a continuous podium. The Design Guidelines allow for such consolidation, provided the proposal meets the overall Objectives and relevant built form controls.

The proposed alternative built form outcome delivers community benefits, including a more sympathetic interface to the west, improved solar access to the public realm and social benefits associated with the affordable housing land use. The proposal delivers high quality environmental benefits through retention of existing mature trees, extensive deep soil areas on ground level, and provision of on-structure planting within planter boxes and garden beds. The proposal is also seeking to achieve a 5-Star Green Star environmental rating equivalency across all towers, exceeding the minimum development policy requirement (4-Star Green Star rating, or equivalent) for three of the towers. The proposal further delivers public amenity through high quality landscaped areas inclusive of public seating and pocket parks which will provide a strong sense of community, encouraging casual interactions and promoting a safe and inclusive environment. Given the positive design outcomes resulting from proposal, the extent of community and environmental benefit delivered is considered commensurate and appropriate for the level of discretion that is being applied. These matters are further discussed below.

The following matters are further discussed as the proposed development seeks alternative design solutions to the acceptable development criteria:



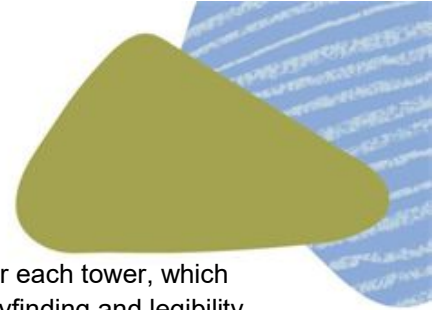
	Design Guidelines Requirement	Proposed	Outcome
Building Height (Maximum)	Lots 8 to 10 (Towers 1 to 3): 11 storeys (up to 39m)	7, 11 and 15 storeys	+4 storeys <i>(average height of 11 storeys across the three towers)</i>
Upper Street Setback (Levels 7 and above)	Subiaco Road – 6m	Tower 1 – 3.6m Tower 2 and 3 – 5.2m to 7.85m	-2.4m <i>(Level 7 only)</i> -0.8m minimum setback <i>(average 6.5m setback)</i>
Car Parking	375 residential bays 57 residential visitor bays 4 commercial visitor bays	279 residential bays 29 residential visitor 0 commercial visitor	-96 bays -28 bays -4 bays
Bicycle Parking	447 residential bays (1 bay per dwelling)	143 residential bays	-304 bays <i>(all dwellings allocated a storeroom capable of accommodating a bicycle)</i>

Building Height (Tower 2)

The proposal includes four towers with heights of 7-storeys (Tower 1), 15-storeys (Tower 2), 11-storeys (Tower 3) and 18-storeys (Tower 4) going from east to west on the site. This proposed built form outcome is generally in keeping with the expected 11 to 18 storey heights envisioned by the Design Guidelines. The redistribution of massing and slender tower design that is oriented north-south is a positive outcome, delivering a more sympathetic design that minimises impacts on adjoining properties and the public realm.

The proposed increased building height of 15 storeys for Tower 2 (above 11 storeys) represents a redistribution of mass across Towers 1 to 3, and is considered consistent with the Objectives of the Design Guidelines as follows:

- despite the varied heights, an average building height of 11 storeys is achieved across Towers 1 to 3 in line with the expectations of the Design Guidelines;
- the redistribution of building mass provides an improved transition of building heights from west to east, with the reduced Tower 1 height of 7 storeys achieving a more sympathetic interface with established neighbouring development to the west compared to the possible 11 storey outcome (considering the potential future neighbouring outcome of four storeys);
- the design reduces overshadowing by approximately 28% when compared to a design that is strictly in accordance with the Design Guidelines, through reduced podium heights and slender tower design;



- the diversity in tower heights creates a sense of individuality for each tower, which when paired with unique architectural expression assists in wayfinding and legibility for visitors and residents; and
- the Design Review Panel was supportive of the overall height and massing, recognising that the site can comfortably accommodate the proposed scale of development.

Upper Level Street Setbacks

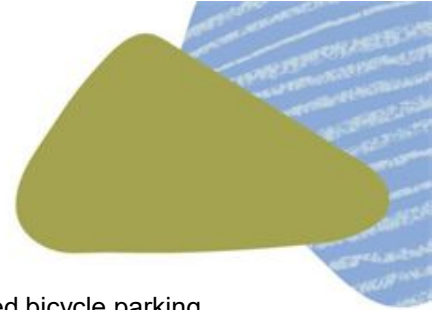
The proposal seeks an alternative design approach to the envisioned 6 metre setback for levels 7 and above to Subiaco Road for Towers 1, 2 and 3. The proposed setbacks are considered to be consistent with the Objectives of the Design Guidelines, and are supported as:

- the Design Guidelines permit a podium up to 6 storeys to be setback 3 metres from Subiaco Road. Tower 1, at 7 storeys, provides a consistent 3.6 metre street setback to all levels. The reduced setback applies only to the seventh storey, and this is considered acceptable due to the overall lower building height compared to a potential 11 storey development outcome;
- the Tower façades provide articulation, which adds visual interest and further reduces the impact of building bulk;
- as a result of the articulation, Towers 2 and 3 achieve an average setback of 6.5 metres, which exceeds the Design Guidelines' requirement of 6 metres; and
- this, combined with a reduced podium height, less overall mass, and slender towers oriented north-south, reduces the perceived building bulk as experienced from the public realm.

Car Parking

The proposal seeks to reduce the overall provision of car parking bays for residents, visitors and commercial staff. The provision of on-site car parking bays is considered consistent with the Objectives of the Design Guidelines as follows:

- the proposed car parking is sufficient to accommodate the anticipated requirements for residents and visitors given the proximity to two high frequency rail stations (West Leederville and Subiaco), bus services, and the connectivity to a high quality shared path network;
- the proposed ratio of 0.62 residential bays per dwelling is aligned with comparable affordable and social developments being delivered through HAFF funding elsewhere in metropolitan Perth;
- the total number of parking bays is supported by a Transport Impact Assessment that demonstrates the parking provision is appropriate for the scale of the development in this location;
- the proposed parking and transport arrangements promote a non-vehicular lifestyle, contributing to reduced car dependency and improved physical health outcomes for residents; and
- the allocation and ongoing management of residential parking bays will be overseen through an Operational Management Plan implemented by CHL. Tenants will be made aware of the parking arrangements as part of the resident application and move-in process, ensuring residents are informed prior to occupancy and allowing flexibility for residents to occupy a dwelling without requiring a dedicated car bay.



Bicycle Parking

The proposal provides 143 residential bicycle bays within four dedicated bicycle parking facilities, one for each tower. The provision of 143 dedicated bicycle bays is considered consistent with the Objectives of the Design Guidelines as follows:

- each dwelling is provided with a storeroom which is suitable for the storage of bicycles and other active transport devices. The CHP has confirmed that bicycle racks will be installed on request within storerooms;
- the Residential Design Codes Volume 2 requires 0.5 bicycle bays per dwelling representing a significant difference to the 1 bay per dwelling requirement of the Design Guidelines;
- the bicycle parking facilities are secure, easily accessible and conveniently located for residents supporting a range of bicycle types; and
- the development site is located adjacent to high frequency rail services improving access to key nodes such as the Perth CBD and shopping centers.

Condition 13, and the associated advice note, is recommended to ensure that the CHP makes tenants aware of the option to have a bicycle rack installed within their storeroom, and that these are installed at the cost of the CHP.

Development Policies

The proposal has been assessed against the applicable Subiaco Redevelopment Area Development Policies, with key considerations outlined below:

Development Policy 1 – Green Buildings

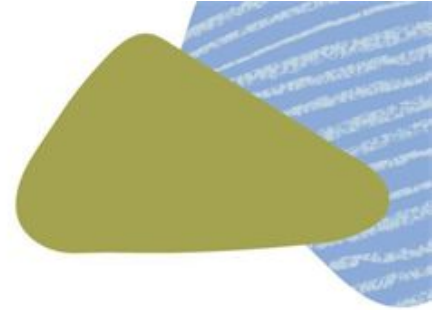
Under the Design Guidelines, Lots 8, 9 and 10 are Tier 3 sites and Lot 11 is a Tier 2 site. Tier 3 sites are required to achieve a minimum 4 Star Green Star or equivalent and Tier 2 sites are required to achieve a minimum 5 Star Green Star.

It is not possible under the Green Star system to have separate ratings within the same development. To address this issue, the applicant proposes that the development as a whole will achieve a 5 Star Green Star equivalency rating without undertaking formal registration through the Green Building Council of Australia. Given that only Lot 11 is required to achieve 5 Star Green Star, this outcome is considered to address the objectives of Development Policy 1 with environmental benefits expected to exceed the minimum standard. This approach has been supported by the DRP.

Conditions 15 and 16 are recommended to ensure the commitment to a 5 Star Green Star equivalency is achieved and delivered to the satisfaction of DevelopmentWA.

Development Policy 3 – Sound and Vibration Attenuation

An Acoustic Report that considered sound and vibration was submitted addressing noise impacts from the nearby rail line and the emissions from mechanical plant servicing the development. The Acoustic Report incorporates specific recommendations for noise attenuation measures to be applied to the development to satisfy Development Policy 3. Conditions 17 and 18 are recommended to ensure that the recommendations of the Acoustic Report are implemented into the building, with Condition 25 is recommended to inform prospective landowners and residents of the likelihood of higher noise levels from transport.



Development Policy 4 – Providing Public Art

The estimated cost of development exceeds \$2 million and requires the provision of public art in accordance with Development Policy 4. A comprehensive Public Art Strategy has been prepared by Art Theory which establishes a framework for the commissioning, delivery and integration of public art across the Site. The draft strategy proposes that both Aboriginal and non-Aboriginal artists will be commissioned, with artworks to be site-specific, durable, and sympathetic to the cultural and historical context of Subiaco East. Condition 19 is recommended to ensure that public art is incorporated into the final design in accordance with Development Policy 4 including public art concepts, artist selection processes and final artwork documentation being provided prior to delivery.

Development Policy 9 - Affordable and Diverse Housing

As the proposed development includes a permanent residential use, Development Policy 9 applies. The proposed development generally satisfies the performance standards of Development Policy 9 through the delivery of social and affordable housing, however the dwelling mix includes limited three-or-more bedroom dwellings and an increased number of one bedroom dwellings. The proposal meets the objectives of Development Policy 9 by:

- providing 100% of apartments as either social or affordable housing which are to be managed by a CHP, far exceeding the minimum 12% requirement and providing housing access and choice to a vulnerable population;
- providing a diversity of dwelling typologies with 228 one-bedroom and 139 two bedroom and 20 three-bedroom apartments;
- delivering a dwelling mix that caters to the expected demand for the proposed land use of social and affordable dwellings; and
- designing all apartments to satisfy the silver level requirements of the Liveable Housing Design Guidelines.

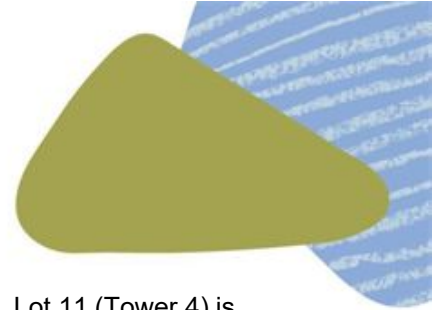
Condition 21 is recommended to ensure that finished development achieves the requirements of Development Policy 9.

Development Policy 10 – Adaptable Housing

As the proposal is for permanent residential development, Development Policy 10 applies. A minimum of 20% of dwellings are required to satisfy the Silver Level requirements of the Liveable Housing Design Guidelines. A total of 100% of the dwellings within the proposed development achieve a minimum Silver level under Liveable Housing Australia standards, with selected upper level dwellings designed to Gold standard. This outcome is significantly over the minimum expectations for this site. Condition 22 is recommended to ensure that the finished development achieves the requirements of Development Policy 10.

Design Review Panel

The proposal has undergone a collaborative design review process with the pre-lodgement plans presented on 27 May 2025 and 18 September 2025. Following lodgement of the development application, on 17 February 2026 the proposal was presented back to the Design Review Panel for further consideration, and the final plans were subsequently considered by the Panel Chair in April 2026 for final comment on Design Excellence.



Design Excellence

In accordance with clause 1.7 Design Quality of the Design Guidelines, Lot 11 (Tower 4) is required to achieve Design Excellence as it is a landmark site. DevelopmentWA sought advice from the Design Review Panel on the interpretation of Design Excellence for the proposal noting that the applicant was seeking to deliver a 'tenure blind' design for the social and affordable units, and therefore not seeking to differentiate Tower 4 aesthetically to highlight its landmark status.

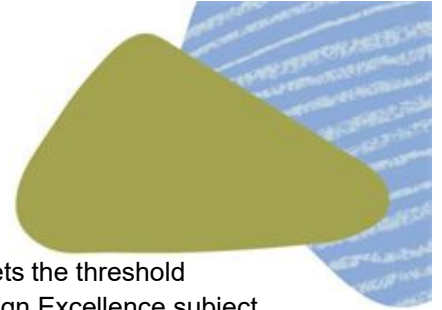
The Panel considered that a high-quality social and affordable housing design outcome should focus on affordability delivered through climate-responsive design that minimises energy usage, inclusion of durable materials that minimise long-term maintenance and operational costs, and a design approach that encourages safety, inclusivity, and integration with the wider community. The Panel determined that to achieve Design Excellence the proposal must:

- Demonstrate the way in which the proposal goes above and beyond when addressing amenity, sustainability, durability and maintenance, and safety and security across the entire development.
- Demonstrate the way in which the proposal goes above and beyond in the way in which the Lot 11 portion of the development responds to its prominent, public location.

At its meeting on 17 February 2026, the Panel acknowledged the substantial work undertaken to improve the design including the shift towards a more residential aesthetic, improved landscaping outcomes and delivery of increased sustainability outcomes throughout the development. The Panel agreed that the proposal had substantially improved since the last review and was close to meeting Design Excellence subject to further refinement in the following key areas:

- further design development of the central forecourt to better contribute to public amenity, attraction and activation, and spatial purpose, particularly consideration for after hours;
- further design development of the Subiaco Road / Court Place corner treatment recognising its significance as a key focal location;
- further details of the intended material strategy, particularly for the tower forms, including contingency plans should the preferred material/construction methodology not be considered viable;
- a consolidated, independent CPTED report being provided that demonstrates how safety principles have been imbedded in the design;
- improved entry legibility and architectural expression to residential lobbies;
- improved amenity of lower-level dwellings, including further consideration of privacy, interaction with the street, stoop depth, and planting interfaces; and
- greater amenity within the ground floor public realm through prioritisation of shared, welcoming spaces with improved comfort, shade, seating, and integration of public art.

The applicant team provided additional information and updated plans to address the comments which were received on 8 April 2026. DevelopmentWA circulated the information



to the DRP Chair for comment with the Chair resolving that Lot 11 meets the threshold required for a Landmark Site and that the overall design achieves Design Excellence subject to the following matters being met or conditioned:

- Confirmation that a 5 Star Green Star equivalency will be delivered;
- Confirmation of the final materiality noting that aluminium cladding is not a supported material option; and
- A CPTED report prepared by an external consultant and any recommendations being integrated into the design of the building.

Refer Appendix 3 – DRP Advice Note and Addendum.

Conditions 15 and 16 are recommended to ensure that the 5 Star Green Star equivalency is delivered, Condition 2 is recommended to ensure that the final materials are of high-quality to the satisfaction of the DRP, and Condition 10 will ensure that an independent CPTED report is prepared and any recommendations are integrated into the design.

Consultation

Stakeholder Consultation

In accordance with Section 64 of the *Metropolitan Redevelopment Authority Act 2011*, the application was referred to the City of Subiaco (the City), Town of Cambridge (the Town), Department of Education, Perth Airport, Public Transport Authority of Western Australia (PTA), Main Roads WA, Department of Planning Lands and Heritage (DPLH), Western Power and Water Corporation.

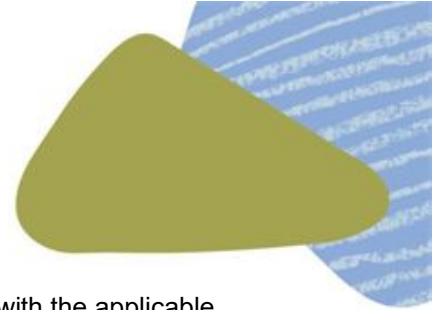
City of Subiaco

The City of Subiaco Council considered the proposal at its meeting held on 24 March 2026 and resolved to support the application subject to consideration of a range of matters including:

- ensuring that all bicycle storage is designed to provide secure, weather-protected, fit-for-purpose storage for a wide range of bicycle types including e-bikes and scooters;
- ensuring compliance with requirements of the R-Codes and relevant DevelopmentWA Development Policies; and
- consideration and investigation of comments provided by the Engineering Team and Parks Team.

Consideration has been given to the matters raised by the City with a review undertaken on storeroom sizes, balcony sizes, and provision of bicycle parking. Comments by the City's Engineering team and Parks teams have been investigated and key matters incorporated into Conditions and Advice Notes where appropriate.

The City provided a list of recommended Conditions and Advice Notes. Where appropriate, these have been recommended for inclusion to address the comments of the City and ensure ongoing discussion with relevant City departments.



Town of Cambridge

The Town's administration supported the proposal as it broadly aligns with the applicable framework and is consistent with future expected development on the northern side of the railway within the West Leederville Precinct Structure Plan area. The Town further noted that the visual and amenity impacts of the proposed development on land within the Town of Cambridge would be negligible given the physical separation provided by the railway line, reserves and roads.

Other Agencies

- Department of Education raised no in-principle objections to the proposal subject to further consideration of the building interface and land use compatibility. The Department further raised that there is an expectation that on-street bays will be utilised for drop-off and pick-up of students and that the development should not have any reliance on these bays for visitor parking. These matters are discussed in public comment section below and addressed by Condition 3.
- Perth Airport raised no objections to the proposal recommending conditions be imposed to ensure necessary approvals from Perth Airport and Airservices Australia are issued prior to construction. These matters have been addressed by Condition 31.
- PTA provided support for the proposal and recommended that conditions be included on the determination to ensure protection of the rail reserve both during and post-construction, and to ensure that recommendations from the Acoustic Report are implemented. The matters have been addressed by Conditions 5, 17 and 18.
- Main Roads raised no objections to the proposal and recommended that conditions be included on the determination requiring minor updated to the Acoustic Report to align with State Planning Policy 5.4, implementation of the recommendation of the Acoustic Report and a notification being placed on the title advising landowners to the possible impact of transport noise. These matters have been addressed by Conditions 17, 18 and 25.
- DPLH, Western Power and Water Corporation did not provide comment on the proposal.

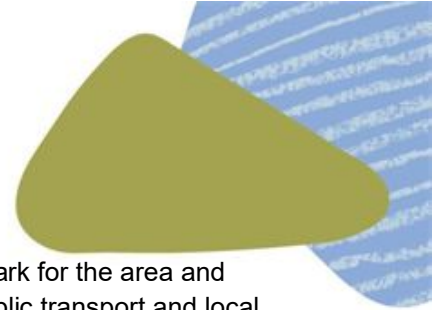
A detailed response to the comments raised by referral agencies have been addressed through recommended conditions and advice notes, as considered appropriate in *Appendix 4 – Summary of Stakeholder Comments*.

Public Consultation

In accordance with clause 5.13 of the Scheme the application was advertised for public comment from 17 February to 19 March 2026. Twenty-five (25) submissions were received, five (5) in support and twenty (20) in objection. The key issues raised by those opposed to the development related to height, traffic congestion, insufficient parking bays, impact on nearby schools, social and affordable housing land use, and impact on amenity.

Height

As discussed previously in the report, the height of the development is considered appropriate and generally in keeping with the outcome envisaged by the Design Guidelines, with the DRP noting that the site can comfortably accommodate the proposed scale of



development. A height of 18-storeys for Tower 4 will serve as a landmark for the area and allow for increased density within close proximity to high-frequency public transport and local amenities.

Parking

As discussed previously in the report, the proposed number of parking bays is appropriate for the site given the scale and location. The reduced parking will minimise the impact on traffic and congestion and promote use of active transport options which are available in close proximity to the development. Tenants will be made aware of the parking arrangements as part of the resident application and move-in process, ensuring residents are informed prior to occupancy.

Impact on nearby schools

The proposed development has been designed to appropriately respond to its interface with Bob Hawke College through building orientation, separation distances and façade articulation. Dwellings are primarily oriented east, north and west minimising actual and perceived overlooking. The two-storey podium scale responds appropriately to the local context providing low-intensity active engagement to Subiaco with offices and dwellings and more active uses of café/restaurant and shop to Court Place improving safety and promoting a sense of community.

Impact on students and the school will be carefully managed through construction, including heavy vehicle movement, access to on-street parking during peak school times and impacts from noise and dust. Conditions are recommended to ensure all works are undertaken in consultation with DevelopmentWA, City of Subiaco and Department of Education.

Traffic

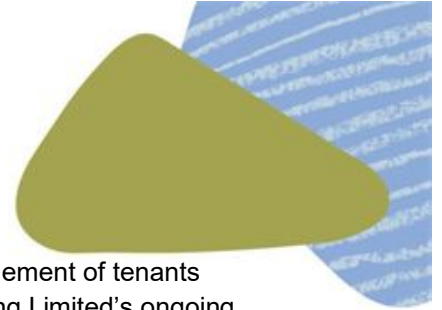
Parking provision has been reduced to reflect the highly accessible location and traffic modelling demonstrates that peak hour vehicle movements associated with the development can be accommodated within the existing network, including during peak school periods, without material impact on intersection performance or safety.

Land use

In relation to social and affordable housing, the site will be managed by a community housing provider with staff available on-site. The Community Housing Provider will provide ongoing onsite management to reduce any risk of antisocial behaviour or amenity issues arising, and to ensure any matters are identified and addressed promptly. Condition 13 is recommended to ensure these matters are appropriately managed and sufficiently mitigated through the management of the CHP and the preparation and implementation of an Operational Management Plan.

Amenity

In relation to amenity, the design assessment by the DRP has carefully considered the potential impacts from overshadowing, noise and light on the public realm and surrounding land uses, confirming that a Design Excellence outcome will be delivered. Preparation of a Management Plan is recommended as a condition of approval to ensure that ground floor commercial tenancies minimise impact on the adjacent residential properties and consider



operations in relation to peak times. The Plan will also consider management of tenants within residential and commercial areas on the site. Community Housing Limited's ongoing, on-site management presence will ensure the Management Plan is followed, confirming that impacts on amenity for residents and the wider community are appropriately managed.

A detailed response to the matters raised is provided in *Appendix 5 – Summary of Public Comments*

Delegation

Under DevelopmentWA's Delegation Schedule, the DevelopmentWA Board is delegated to determine development applications with a development value over \$100 million within the Subiaco Redevelopment Area. The project has an estimated value of \$285 million.

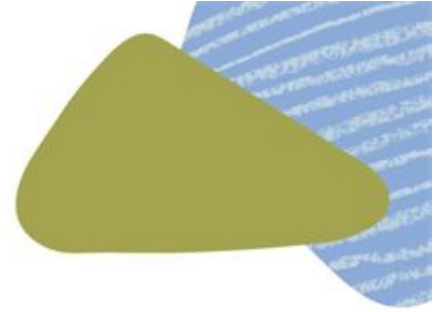
Conclusion

The proposed development is generally consistent with the planning framework for the subject site. The alternative design outcomes including reduced parking provision and redistribution of mass are considered consistent with the Scheme Vision and Objectives, and the Subiaco Oval Vision.

The proposed social and affordable build-to-rent development delivered on a consolidated lot configuration is a positive outcome for the site that is strongly supported by stakeholders. The proposal has undergone an extensive design review process, with the most recent advice confirming that Design Excellence has been achieved. The design carefully balances the importance of delivering a landmark building on the eastern edge that responds to its prominent location whilst delivering a tenure blind design that is cost-effective, inclusive and accessible, meeting the needs of the end users. The delivery of public amenity through high quality landscaped areas inclusive of public seating and pocket parks will provide a strong sense of community, encouraging casual interactions and promoting a safe and inclusive environment. The affordable and social housing will provide stable long term rental housing for low to moderate income earners, assisting in addressing the State and National housing shortages.

The proposal delivers high quality environmental benefits through retention of existing mature trees, extensive deep soil areas on ground level, and provision of on structure planting within planter boxes and garden beds. Substantial community benefit is also delivered through the proposed land use of affordable and social housing managed by an on-site CHP, pocket parks and landscaping providing seating and shade, and new commercial businesses creating a local hub for activity and gathering.

It is recommended the Subiaco Land Redevelopment Committee recommend that the DevelopmentWA Board approves the development application for the 7-18 Storey Mixed Use Development at Lot 1 (No. 154) Subiaco Road, Subiaco, subject to the recommended *conditions and advice notes in Appendix 6*.



Appendices

Appendix 1 – Regulatory Assessment

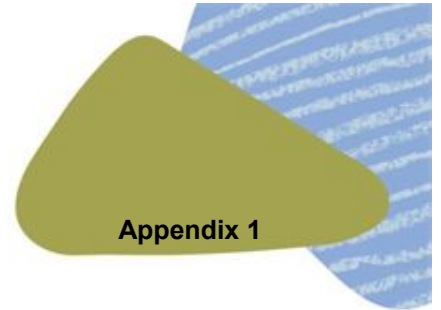
Appendix 2 – Development Plans

Appendix 3 – DRP Advice Note and Addendum

Appendix 4 – Summary of Stakeholder Comments

Appendix 5 – Summary of Public Comments

Appendix 6 – Recommended Conditions and Advice Notes



Appendix 1

REGULATORY ASSESSMENT

Redevelopment Area Objectives

The proposal is considered to satisfy the Redevelopment Area Objectives as it:

- Sense of Place* The proposal is a high-quality development with an architectural design informed by a strong context and character analysis drawing upon diverse past and present buildings as well as contributing to much needed activation to the north of Subiaco Oval.
- Economic Wellbeing* The proposal represents a \$285 million investment in the Project Area and provides 447 new affordable and social dwellings, and 2 café/restaurant and/or shop tenancies in the locality which will contribute to the vibrancy of the area.
- Urban Efficiency* The proposal redevelops an underutilised, vacant piece of land providing efficiency through high density living, helping to facilitate additional population adjacent to high frequency public transport.
- Social Inclusion* The proposal includes 300 affordable and 147 social dwellings which are serviced by high frequency public transport, commercial land uses and support services.
- Connectivity* The proposal offers residential land uses in close proximity to high frequency public transport, with high-quality cycle and pedestrian connections, which promote reduced reliance on car travel.
- Environmental Integrity* The proposal commits to achieving a 5 Star Green Star equivalent rating and incorporates sustainability infrastructure in the design, including rooftop solar panels, and substantial landscaping at ground level.

Appendix 2

Court Place

No	Date	By	On	Description
1	20/01/2008	MLP	MLP	Issue for Information
2	26/01/2008	MLP	MLP	Issue for Development Application
3	20/01/2008	MLP	MLP	Issue for CDA Draft
4	26/01/2008	MLP	MLP	Issue for Conditions
5	18/01/2008	MLP	MLP	Issue for Planning

All works to be in accordance with authority & statutory approvals.
 Plans to be drawn up in accordance with existing site conditions.
 All works to be in accordance with authority & statutory approvals.
 Plans to be drawn up in accordance with existing site conditions.
 All works to be in accordance with authority & statutory approvals.
 Plans to be drawn up in accordance with existing site conditions.



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Project Name	Court Place
Project Number	00013365
Project Address	154 Subiaco Road Subiaco WA 6008
Country	Aust.

Drawing Name	Cover Page
Drawing No.	DA000
Drawing Scale	
© A1	Revision
B	

Subiaco LRC Meeting - Development Management

| DRAWING LIST

SHEET INDEX				
	NO.	DESCRIPTION	SCALE	REV
DA0 Cover Pages				
	DA000	Cover Page		B
	DA001	Drawing List		B
DA1 Site Series				
	DA100	Existing Site Plan	1:500 @A1	B
	DA101	Demolition Plan	1:500 @A1	B
	DA102	Proposed Site Plan	1:500 @A1	B
DA20 Overall Plans				
	DA200	Overall - Lower Ground Plan	1:300 @A1	B
	DA201	Overall - Upper Ground Plan	1:300 @A1	B
	DA202	Overall - Level 01	1:300 @A1	B
	DA203	Overall - Level 02 / Podium	1:300 @A1	B
	DA204	Overall - Level 03-06 / Typical Plates	1:300 @A1	B
	DA205	Overall - Level 07 / T1 Roof Services	1:300 @A1	B
	DA206	Overall - Level 11 / T3 Roof Services	1:300 @A1	B
	DA207	Overall - Level 15 / T2 Roof Services	1:300 @A1	B
	DA208	Overall - T4 Roof Services	1:300 @A1	B
DA21 Plans - Tower 1 & 2				
	DA210	T1 & T2 - Upper Ground Plan	1:200 @A1	B
	DA211	T1 & T2 - Level 01	1:200 @A1	B
	DA212	T1 & T2 - Top of Podium	1:200 @A1	B
	DA213	T1 & T2 - Typical Tower	1:200 @A1	B
	DA214	T1 - Roof Services Plan	1:200 @A1	B
	DA215	T2 - Roof Services Plan	1:200 @A1	B
DA22 Plans - Tower 3 & 4				
	DA220	T3 & T4 - Lower Ground Plan	1:200 @A1	B
	DA221	T3 & T4 - Upper Ground Plan	1:200 @A1	B
	DA222	T3 & T4 - Level 01	1:200 @A1	B
	DA223	T3 & T4 - Top of Podium	1:200 @A1	B
	DA224	T3 & T4 - Typical Tower	1:200 @A1	B
	DA225	T3 - Roof Services Plan	1:200 @A1	B
	DA226	T4 - Roof Services Plan	1:200 @A1	B
DA30 Streetscape Elevations				
	DA300	South Elevation	1:300 @A1	B
	DA301	North Elevation	1:300 @A1	B
DA31 Building Elevations - Towers				
	DA310	T1 & T2 - South Elevation	1:200 @A1	B
	DA311	T3 & T4 - South Elevation	1:200 @A1	B
	DA312	T3 & T4 - North Elevation	1:200 @A1	B
	DA313	T1 & T2 - North Elevation	1:200 @A1	B
	DA314	T1 - West & East Elevation	1:200 @A1	B
	DA315	T2 - West & East Elevation	1:200 @A1	B
	DA316	T3 - West & East Elevation	1:200 @A1	B
	DA317	T4 - West & East Elevation	1:200 @A1	B
DA32 Sections				
	DA320	Section A-A	1:100 @A1	B
	DA321	Section B-B & Section C-C	1:200 @A1	B

SHEET INDEX				
	NO.	DESCRIPTION	SCALE	REV
DA40 Shadow Diagrams				
	DA400	Shadow Diagram - June 21st	@A1	B
	DA401	Shadow Diagram - September 23rd	@A1	B
	DA402	Shadow Diagram - December 21st	@A1	B
DA41 Eye of the Sun Diagrams				
	DA410	Eye of the Sun Diagrams 01	1:500 @A1	B
	DA411	Eye of the Sun Diagrams 02	1:500 @A1	B
	DA412	Eye of the Sun Diagrams 03	1:500 @A1	B
	DA413	Eye of the Sun Diagrams 04	1:500 @A1	B
DA5 Compliance Drawings				
	DA500	Solar Access Diagrams	1:600 @A1	B
	DA501	Cross Ventilation Diagrams	1:600 @A1	B
	DA502	Development Summary		B
DA7 Supporting Documents				
	DA700	Feature Survey		B

Rev	Date	By	Cha	Description
1	20/01/2016	SA DCL	DCL	Issue for DA - Issues
2	20/01/2016	SA DCL	DCL	Issue for Development Application
3	20/01/2016	SA DCL	DCL	Issue for DA Plan

All works to be in accordance with authority & statutory agencies.
 Refer to the terms of any relevant planning or building code conditions.
 Any work to be confirmed to the relevant authority before commencing.
 Refer to the relevant report and planning documentation for all information relating to
 the site and the development.
 Refer to the relevant report and planning documentation for all information relating to
 the site and the development.
 All work to be in accordance with all Specifications and Schedules, as
 applicable to the project.
 All work to be in accordance with all Specifications and Schedules, as
 applicable to the project.
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 applicable to the project.
 All work to be in accordance with all Specifications and Schedules, as
 applicable to the project.



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Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road Subiaco WA 6008
 Country: Aust.



DEVELOPMENT APPLICATION

Drawing Name: Drawing List
 Drawing Scale: DA001

@ A1
 Revision: B

| EXISTING SITE PLAN



LEGEND

- EXISTING TREE
- COUNCIL TREE
- ON STREET PARKING
- EXISTING STEEL CHAINLINK FENCE

Rev	Date	By	Ofc	Description
1	04/04/2008	SA DKL	DL	Issue to Client
2	26/07/2008	SA DKL	DL	Issue to Development Application
3	26/07/2008	SA DKL	DL	Issue to Council

All works to be in accordance with authority & statutory agencies.
 Plans to be shown for all information relating to existing site conditions.
 All dimensions shown to be confirmed to existing site conditions.
 All dimensions shown to be confirmed to existing site conditions.
 Plans to be shown for all information relating to existing site conditions.
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Project Name: Court Place
Project Number: 00013965
Project Address: 154 Subiaco Road Subiaco WA 6008
Country: Aust.

DEVELOPMENT APPLICATION

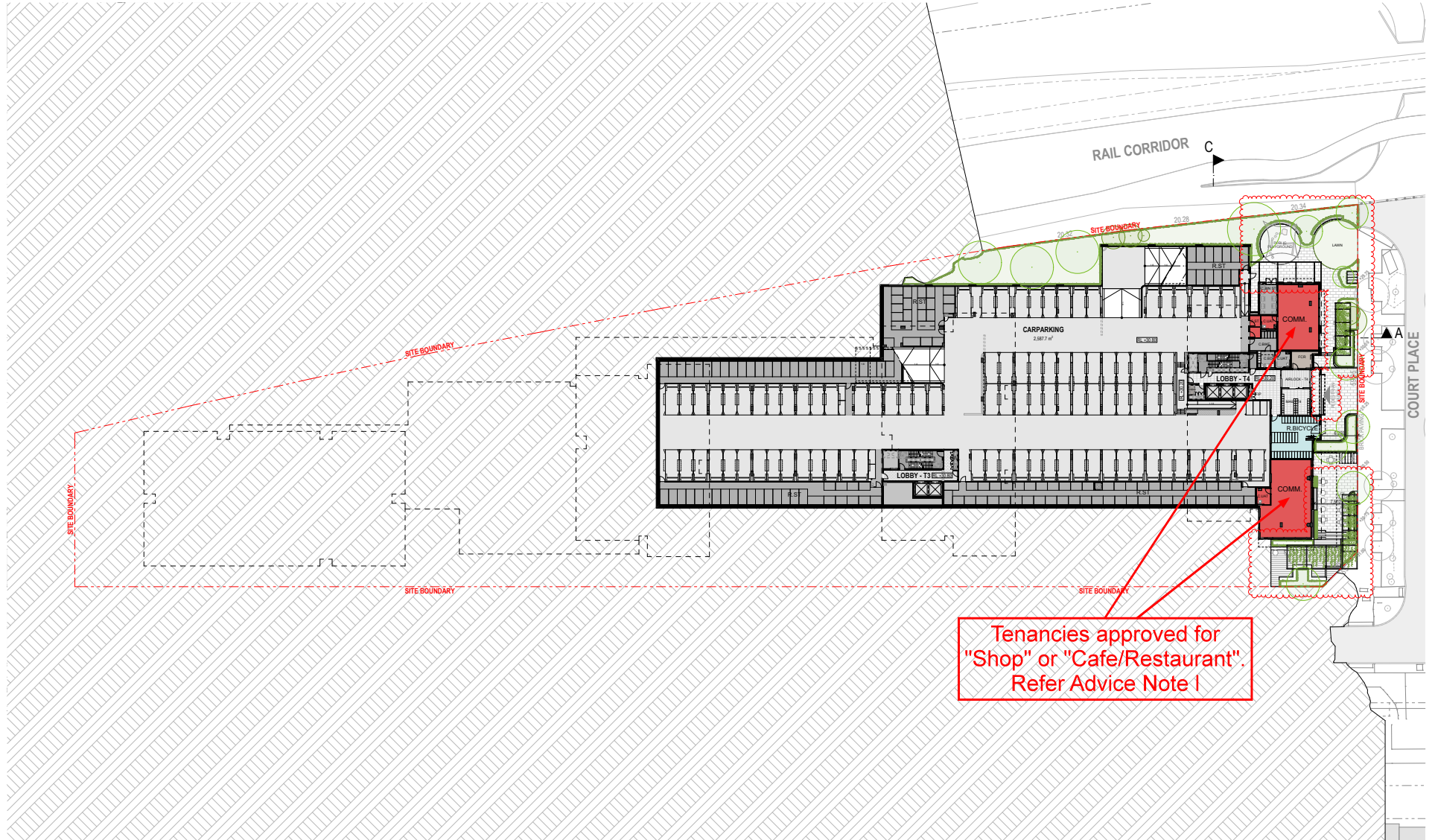
Drawing Name:
Existing Site Plan

Drawing Scale: 1:500 @ A1

Drawing No.: DA100

Revision:
B

| LOWER GROUND



Tenancies approved for "Shop" or "Cafe/Restaurant". Refer Advice Note 1

Rev	Date	By	Ofc	Description
1	04/04/2018	LIA/LM/TJ	DL	Issue for Review - Update
2	26/01/2018	LIA/LM/TJ	DL	Issue for Development Application
3	20/01/2018	LIA/LM/TJ	DL	Issue for CA Plans
4	16/01/2018	LIA/LM/TJ	DL	Issue for CA Reports

All services to be in accordance with authority & statutory approvals. Plans to be prepared for an information only to be submitted to the Council. All services to be in accordance with authority & statutory approvals. Plans to be prepared for an information only to be submitted to the Council. All services to be in accordance with authority & statutory approvals. Plans to be prepared for an information only to be submitted to the Council.

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NOTE:
ALL APARTMENTS DESIGNED TO MINIMUM LHA SILVER LEVEL REQUIREMENTS. REFER TO ACCESS CONSULTANT REPORT FOR DETAIL.

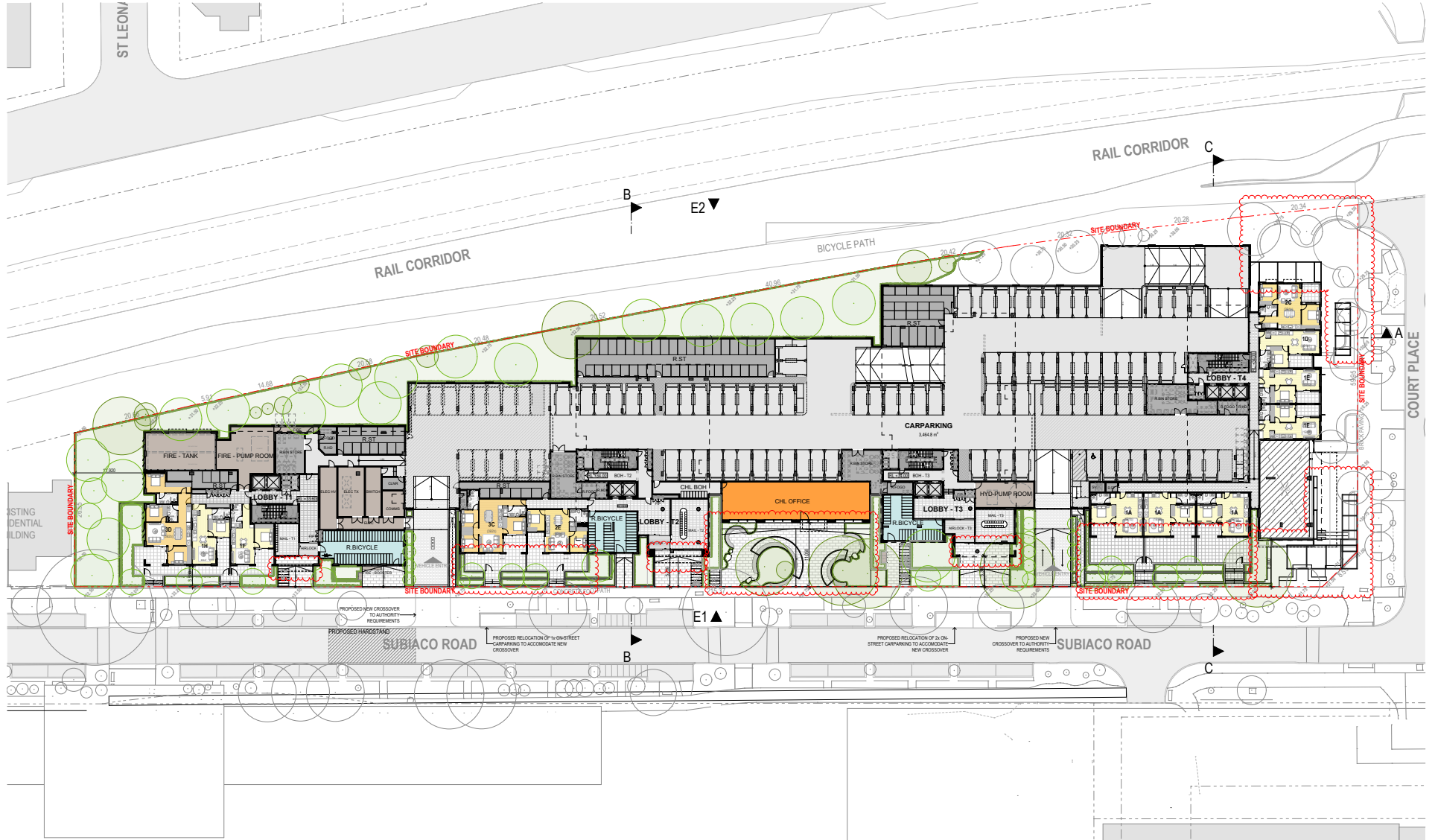


Project Name: Court Place
Project Number: 00013365
Project Address: 154 Subiaco Road, Subiaco WA 6008
Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: Overall - Lower Ground Plan
Drawing Scale: DA200
1:500 @ A1
Revision: B

UPPER GROUND



Rev	Date	By	Ofc	Description
1	04/01/2024	LAL/MF/TD	CLL	Issue for Development Application
2	26/01/2024	LAL/MF/TD	CLL	Issue for CA Plans
3	20/01/2024	LAL/MF/TD	CLL	Issue for CA Reports

All works to be in accordance with authority & statutory approvals.
 Plans to be referred to all relevant authorities to satisfy all conditions.
 Plans on site to be confirmed to registered architect before commencing.
 Plans to be referred to all relevant authorities to satisfy all conditions.
 Plans to be referred to all relevant authorities to satisfy all conditions.
 Plans to be referred to all relevant authorities to satisfy all conditions.

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 AUTHORITY ACT 2011

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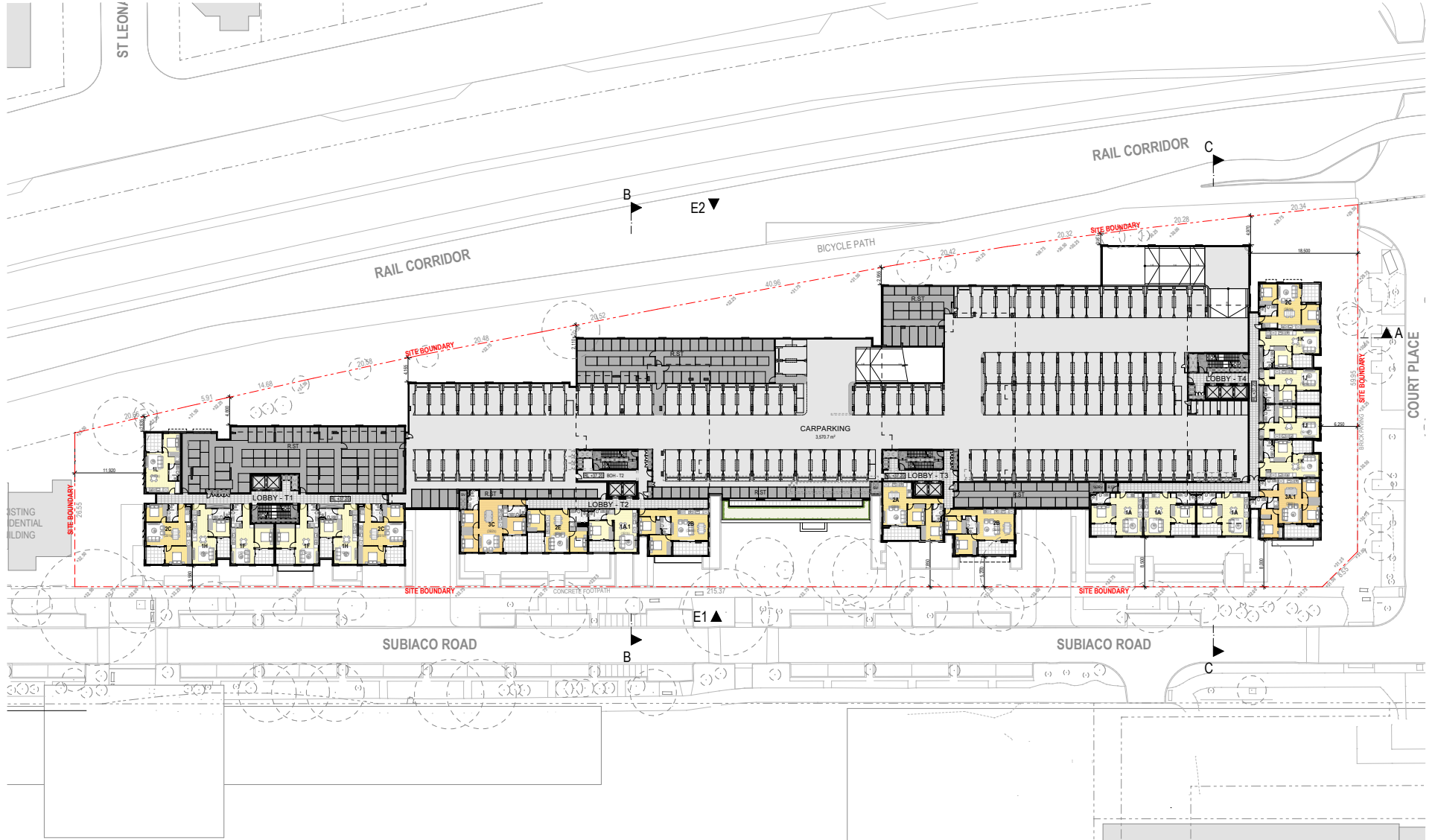


Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: Overall - Upper Ground Plan
 Drawing Scale: 1:300 @ A1
 Drawing No: DA201
 Revision: B

| LEVEL 01



No.	Date	By	Of	Description
1	04/01/2011	LIAK/MPT/CL	CL	Issue for Information
2	26/01/2011	LIAK/MPT/CL	CL	Issue for Development Application
3	26/01/2011	LIAK/MPT/CL	CL	Issue for CA Review
4	16/01/2011	LIAK/MPT/CL	CL	Issue for CA Review
5	16/01/2011	LIAK/MPT/CL	CL	Issue for CA Review
6	24/11/2010	CL	SP	Issue for Construction

All works to be in accordance with authority & statutory approvals.
 Plans to which reference is made in this document are hereby approved.
 Any use of the plans is to be confined to the purposes for which they were issued.
 The Engineer is not responsible for any errors or omissions in the plans or for any consequences arising therefrom.
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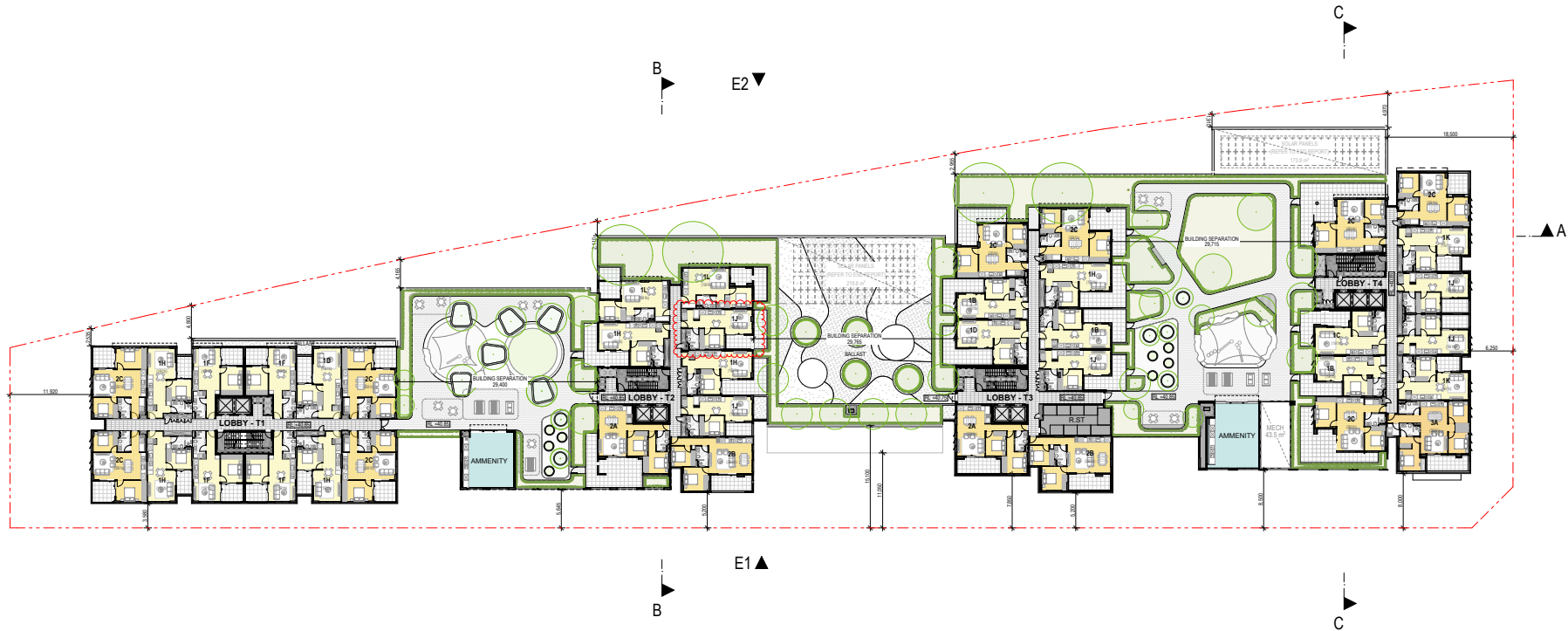


Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: Overall - Level 01
 Drawing Scale: 1:300 @ A1
 DA202
 Revision: B

| LEVEL 02 / TOP OF PODIUM



Rev	Date	By	Check	Description
1	04/04/2018	LIA/LM/TC	COL	Issue for CDR - Issues
2	20/01/2018	LIA/LM/TC	COL	Issue for Development Application
3	20/01/2018	LIA/LM/TC	COL	Issue for CA Draft
4	18/01/2018	LIA/LM/TC	COL	Issue for CA Review
5	12/01/2018	LIA/LM/TC	COL	Issue for Coordination
6	24/11/2017	SS	SP	Issue for Coordination

All works to be in accordance with authority & statutory approvals.
 Plans to be taken for all information relating to existing site conditions.
 Any on site, reference to be confirmed in person before commencing.
 Plans to be taken for all information relating to all information relating to
 site and its surroundings, including all existing and proposed works.
 Changes to be made in consultation with all Stakeholders and Schedules, at
 appropriate intervals throughout the project, unless otherwise stated.
 Plans to be taken for all information relating to all information relating to
 site and its surroundings, including all existing and proposed works.
 Plans to be taken for all information relating to all information relating to
 site and its surroundings, including all existing and proposed works.
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NOTE:
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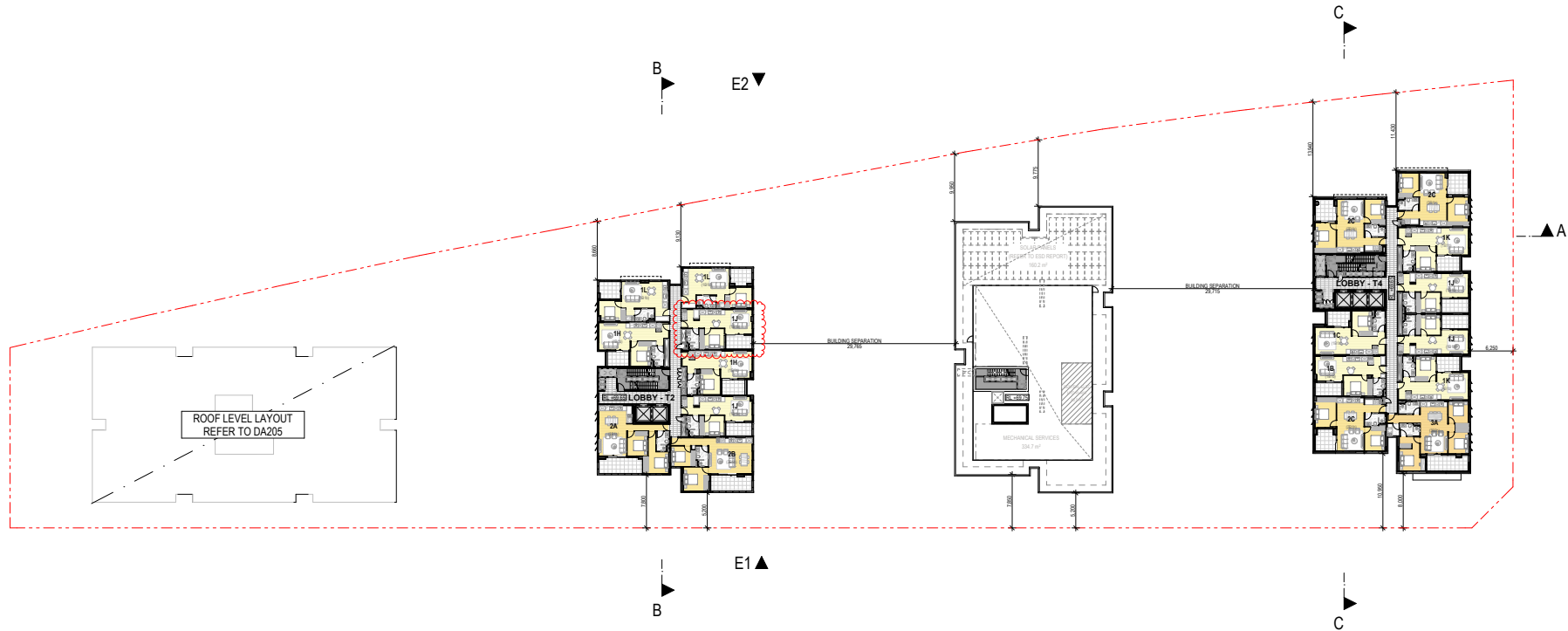


Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: Overall - Level 02 / Podium
 Drawing Scale: 1:500 @ A1
 Drawing No: DA203
 Revision: B

| LEVEL 11 / T3 ROOF SERVICES



Rev	Date	By	Check	Description
1	18/01/2025	LAK/ML/TO	COL	Issue for CA - Issues
2	20/01/2025	LAK/ML/TO	COL	Issue for Development Application
3	20/01/2025	LAK/ML/TO	COL	Issue for CA Panel
4	18/01/2025	LAK/ML/TO	COL	Issue for CA Review
5	18/01/2025	LAK/ML/TO	COL	Issue for Coordination
6	24/11/2025	SD	SP	Issue for Coordination

All works to be in accordance with authority & statutory approvals. Plans to be checked for information relating to existing site conditions. Any use of existing information to be confirmed by registered consultant before commencing work. Final and/or amended drawings and/or documents to be submitted to the relevant authority for approval. Drawings to be used in conjunction with all Specifications and Schedules, all approved and/or amended documents and/or documents to be used. Plans to be checked for compliance with all applicable legislation and/or standards. Plans to be checked for compliance with all applicable legislation and/or standards. Plans to be checked for compliance with all applicable legislation and/or standards.

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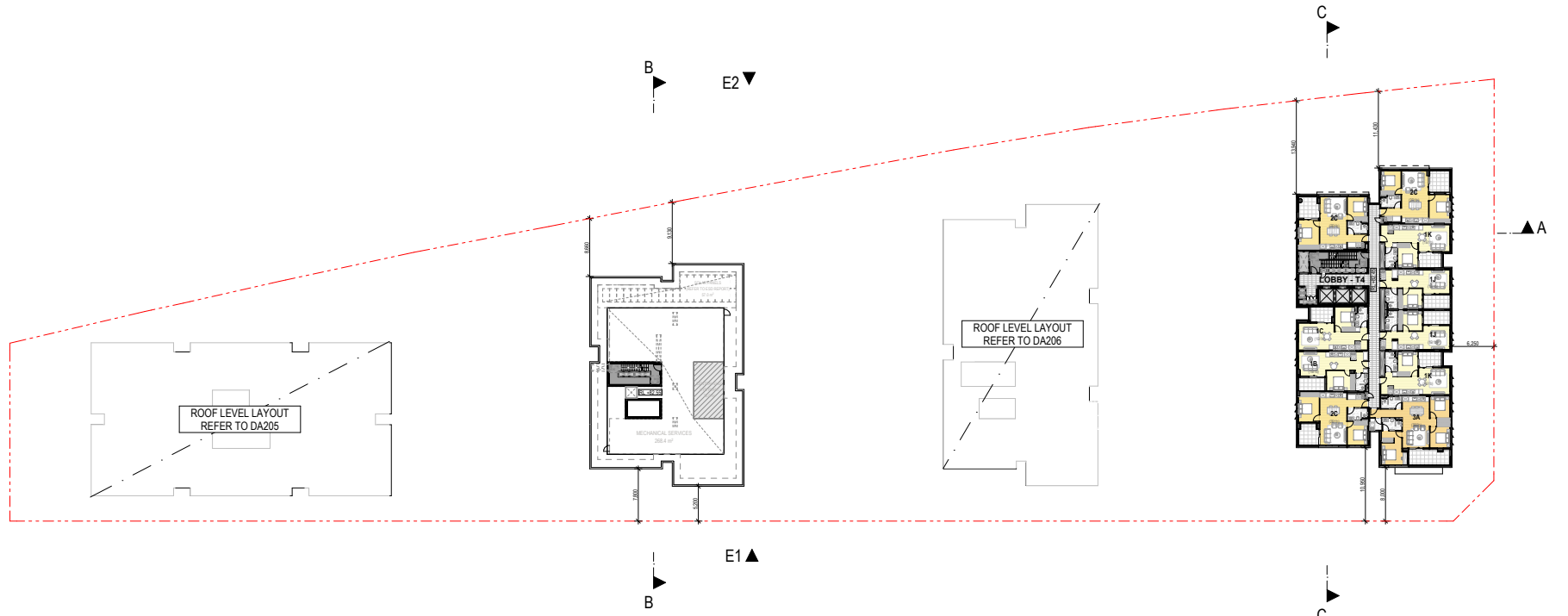
Project Name: Court Place
 Project Number: 00013965
 Project Address: 154 Subiaco Road Subiaco WA 6008

Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: Overall - Level 11 / T3 Roof Services
 Drawing Scale: 1:500 @ A1
 Drawing No: DA206
 Revision: B

| LEVEL 15 / T2 ROOF SERVICES



Rev	Date	By	Chk	Description
1	04/04/2024	LJK, MPT, DCL	DCL	Issue for CA - Update
2	20/01/2024	LJK, MPT, DCL	DCL	Issue for Development Application
3	20/01/2024	LJK, MPT, DCL	DCL	Issue for CA Draft
4	16/01/2024	LJK, MPT, DCL	DCL	Issue for CA Review
5	12/01/2024	LJK, MPT, DCL	DCL	Issue for Coordination
6	24/11/2023	DD	SP	Issue for Coordination

All works to be in accordance with authority & statutory approvals.
 Plans to be drawn for information only to existing site conditions.
 Any use of the information to be confirmed to registered consultant before commencing.
 Plans to be drawn based on available documentation for all information relating to
 site and design information, and all other relevant data.
 Drawings to be used in conjunction with all Specifications and Schedules, all
 approved contract documents and all other relevant documents.
 Plans to be drawn to comply with all applicable laws and regulations.
 Plans to be drawn to comply with all applicable laws and regulations.
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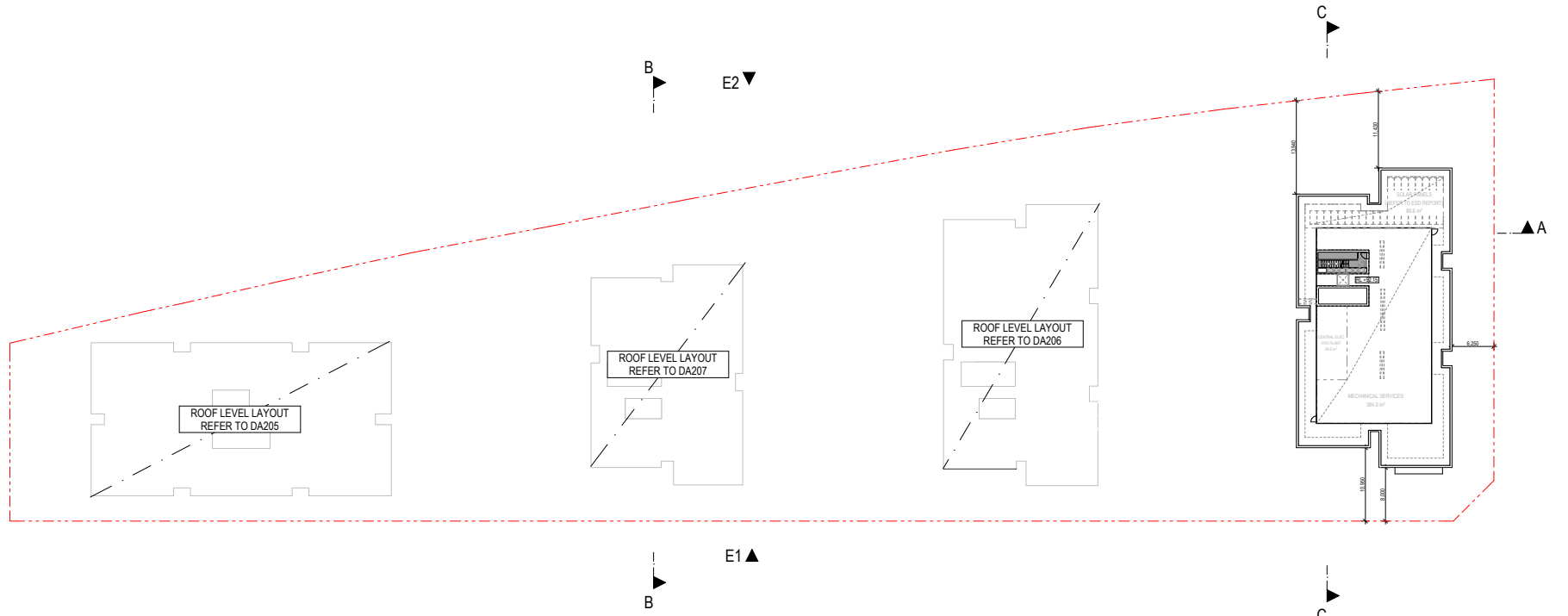
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Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road
 Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: Overall - Level 15 / T2 Roof Services
 Drawing Scale: 1:500 @ A1
 Drawing No: DA207
 Revision: B

T4 ROOF SERVICES



Rev	Date	By	Cha	Description
1	04/01/2020	LIA/LM/TO	COL	Issue for CA - Issues
2	06/01/2020	LIA/LM/TO	COL	Issue for Development Application
3	20/01/2021	LIA/LM/TO	COL	Issue for CA Final
4	16/01/2020	LIA/LM/TO	COL	Issue for CA Review
5	12/01/2021	LIA/LM/TO	COL	Issue for Consultation
6	24/11/2020	SS	ISP	Issue for Consultation

All works to be in accordance with authority & statutory approvals.
 Refer to site notes for all information relating to existing site conditions.
 All structural information to be confirmed to registered engineer before commencing works.
 Refer to relevant Report and Consultant Documents for all information relating to:
 - Site and Design Information, and all other relevant notes.
 - Design to be made in conjunction with all Specifications and Schedules, as approved by the Council.
 - All design to be made in conjunction with the relevant other Development Consent.
 - All works to be made in accordance with the relevant other Development Consent.
 - All works to be made in accordance with the relevant other Development Consent.

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NOTE:
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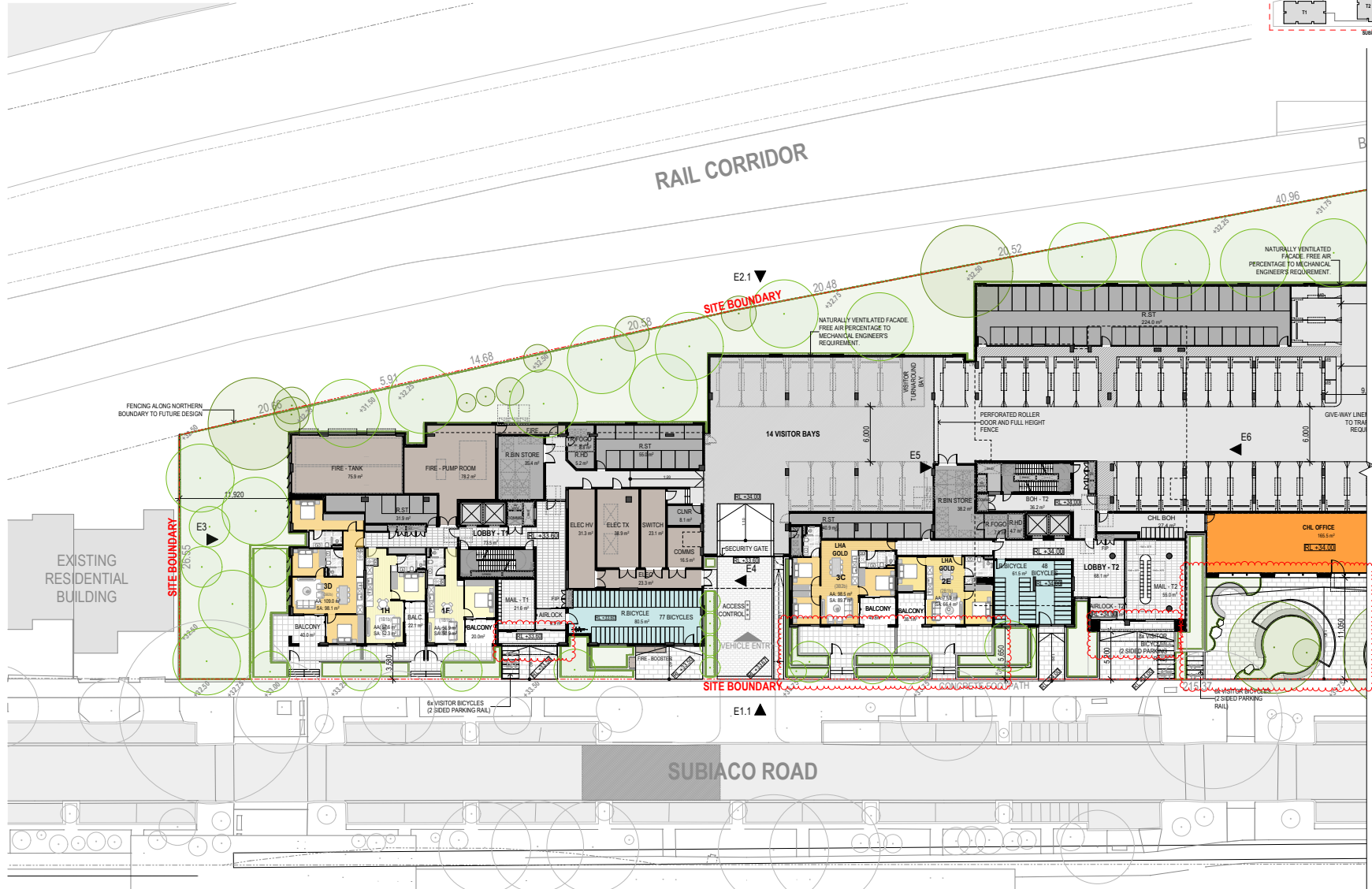
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Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: Overall - T4 Roof Services
 Drawing Scale: 1:500 @ A1
 Drawing No: DA208
 Revision: B

TOWER 1 & 2 - UPPER GROUND



REFER TO DA21 FOR CONTINUATION

Rev	Date	By	Ofc	Description
1	18/01/2024	LJA/MF/TZ	DDL	Issue for Development Application
2	20/01/2024	LJA/MF/TZ	DDL	Issue for DA Review
3	18/01/2024	LJA/MF/TZ	DDL	Issue for Development Application
4	18/01/2024	LJA/MF/TZ	DDL	Issue for Development Application

All works to be in accordance with authority & statutory approvals.
 Plans to be used for information only. No liability is accepted for any errors or omissions.
 Plans to be used for information only. No liability is accepted for any errors or omissions.
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NOTE:
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 REQUIREMENTS. REFER TO ACCESS
 CONSULTANT REPORT FOR DETAIL.

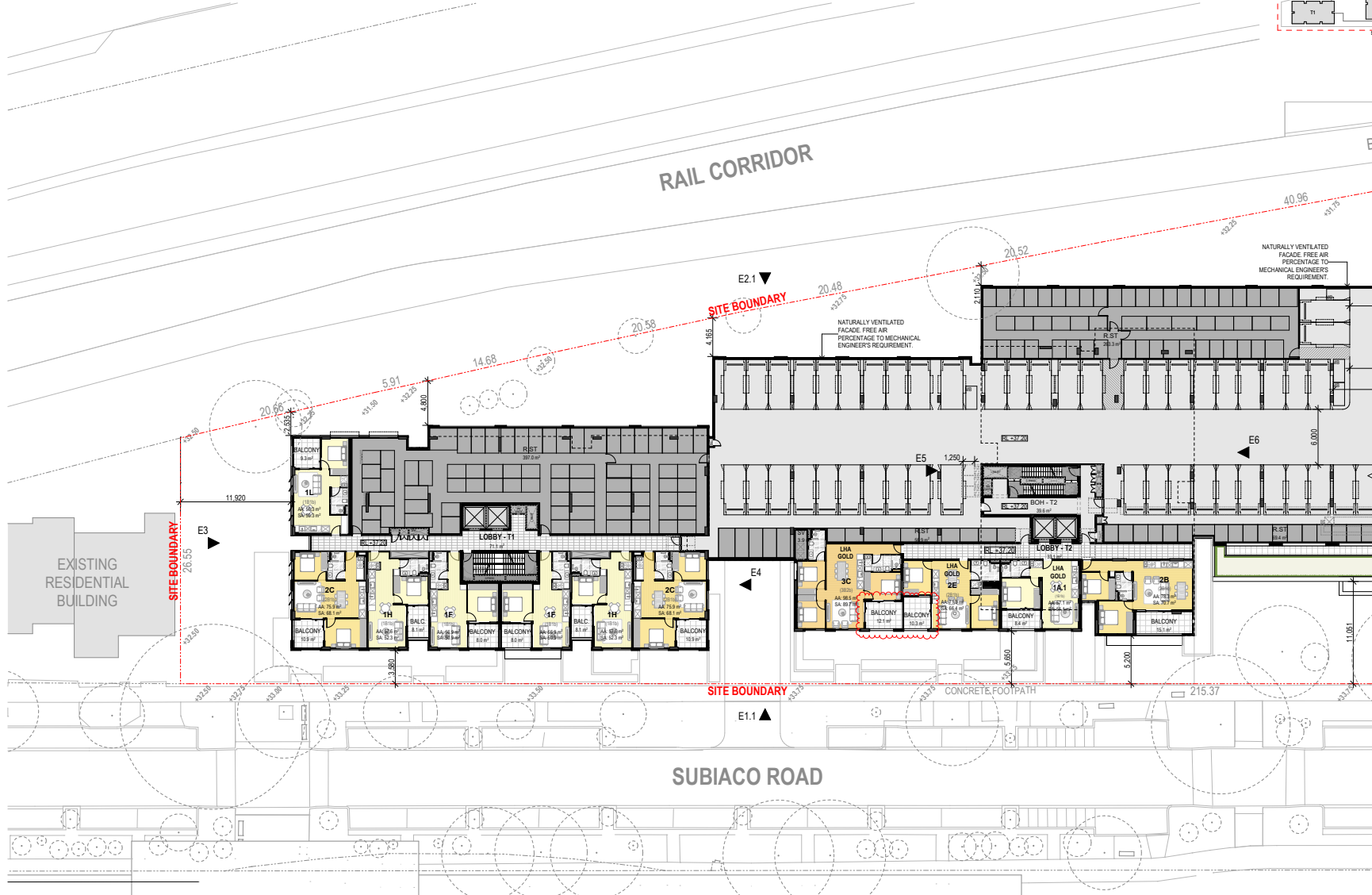
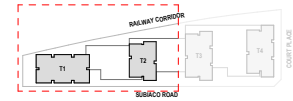


Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T1 & T2 - Upper Ground Plan
 Drawing Scale: 1:200 @ A1
 Drawing No: DA210
 Revision: B

| TOWER 1&2 - LEVEL 01



Rev	Date	By	Ofc	Description
01	15/01/2024	LJA/KP/TZ	DDL	Issue for Development Application
02	20/01/2024	LJA/KP/TZ	DDL	Issue for DA Review
03	15/01/2024	LJA/KP/TZ	DDL	Issue for Development Application
04	15/01/2024	LJA/KP/TZ	DDL	Issue for Development Application

All services to be coordinated with authorities & statutory agencies.
 Refer to site notes for all information relating to existing site conditions.
 All services to be coordinated with authorities & statutory agencies.
 Refer to site notes for all information relating to existing site conditions.
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 Refer to site notes for all information relating to existing site conditions.

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NOTE:
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 REQUIREMENTS. REFER TO ACCESS
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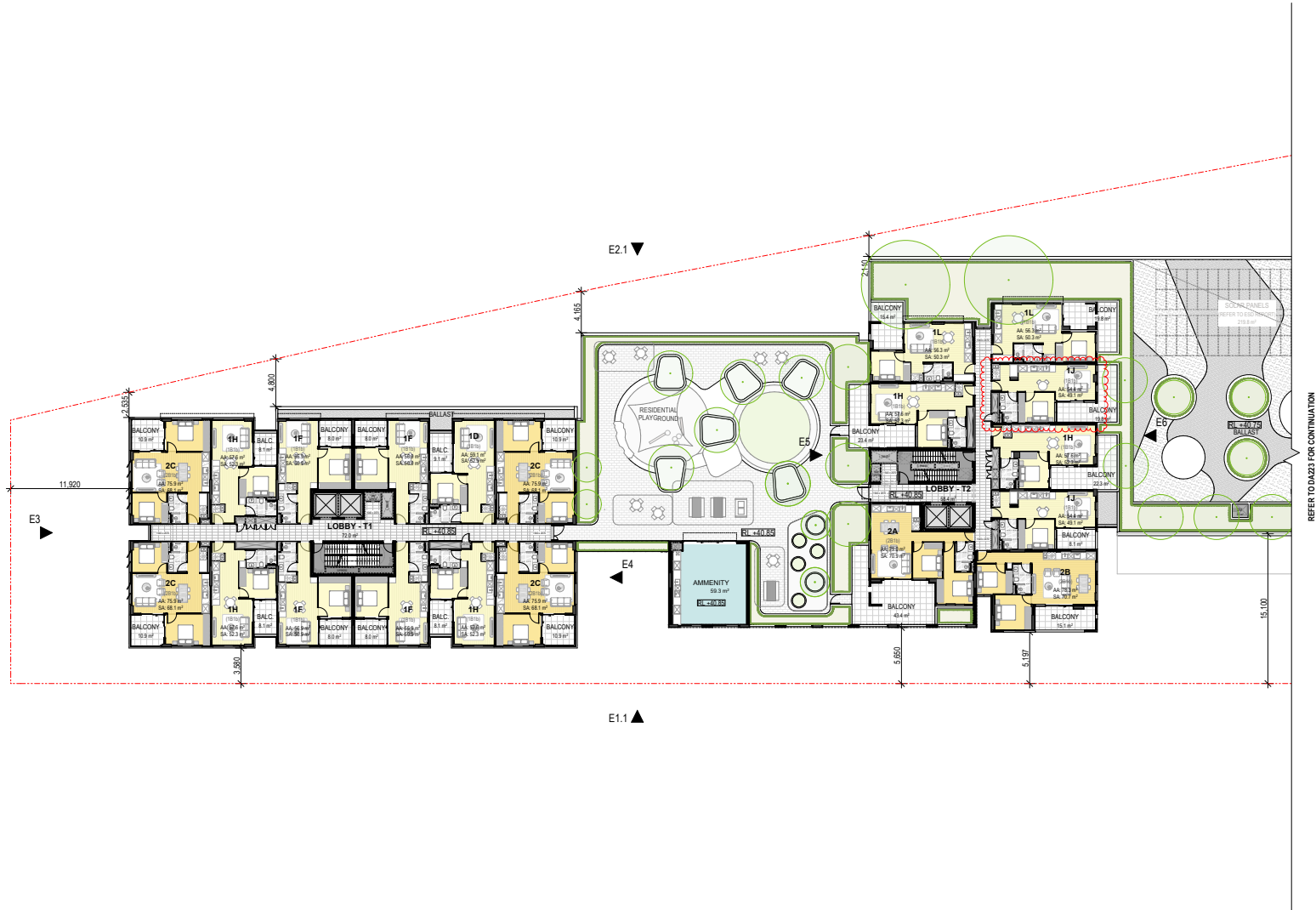


Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T1 & T2 - Level 01
 Drawing Scale: 1:200 @ A1
 Drawing Number: DA211
 Revision: B

| TOWER 1 & 2 - LEVEL 02 / TOP OF PODIUM



REFER TO DA223 FOR CONTINUATION

DEVELOPMENT APPLICATION

Rev	Date	By	Check	Description
1	04/04/2024	LJA, MP, TD	COL	Issue for CA - Issues
2	26/04/2024	LJA, MP, TD	COL	Issue for Development Application
3	20/05/2024	LJA, MP, TD	COL	Issue for CA Draft
4	16/06/2024	LJA, MP, TD	COL	Issue for CA Review
5	16/06/2024	LJA, MP, TD	COL	Issue for Development

All works to be in accordance with authority & statutory approvals.
 Refer to the terms of any information relating to existing site conditions.
 All dimensions shall be confirmed to registered surveyor before commencing.
 Refer to the site plan and landscape documentation for all dimensions relating to
 site and landscape conditions. All dimensions shall be confirmed to registered
 surveyor before commencing.
 Changes to the plan or conditions shall be in accordance with the Specifications and Schedule, as
 approved by the Development Application, unless otherwise stated.
 Refer to the site plan for all dimensions and conditions.
 Dimensions shown in this drawing are for information only and do not constitute a contract.
 The contractor shall verify all dimensions before commencing work.



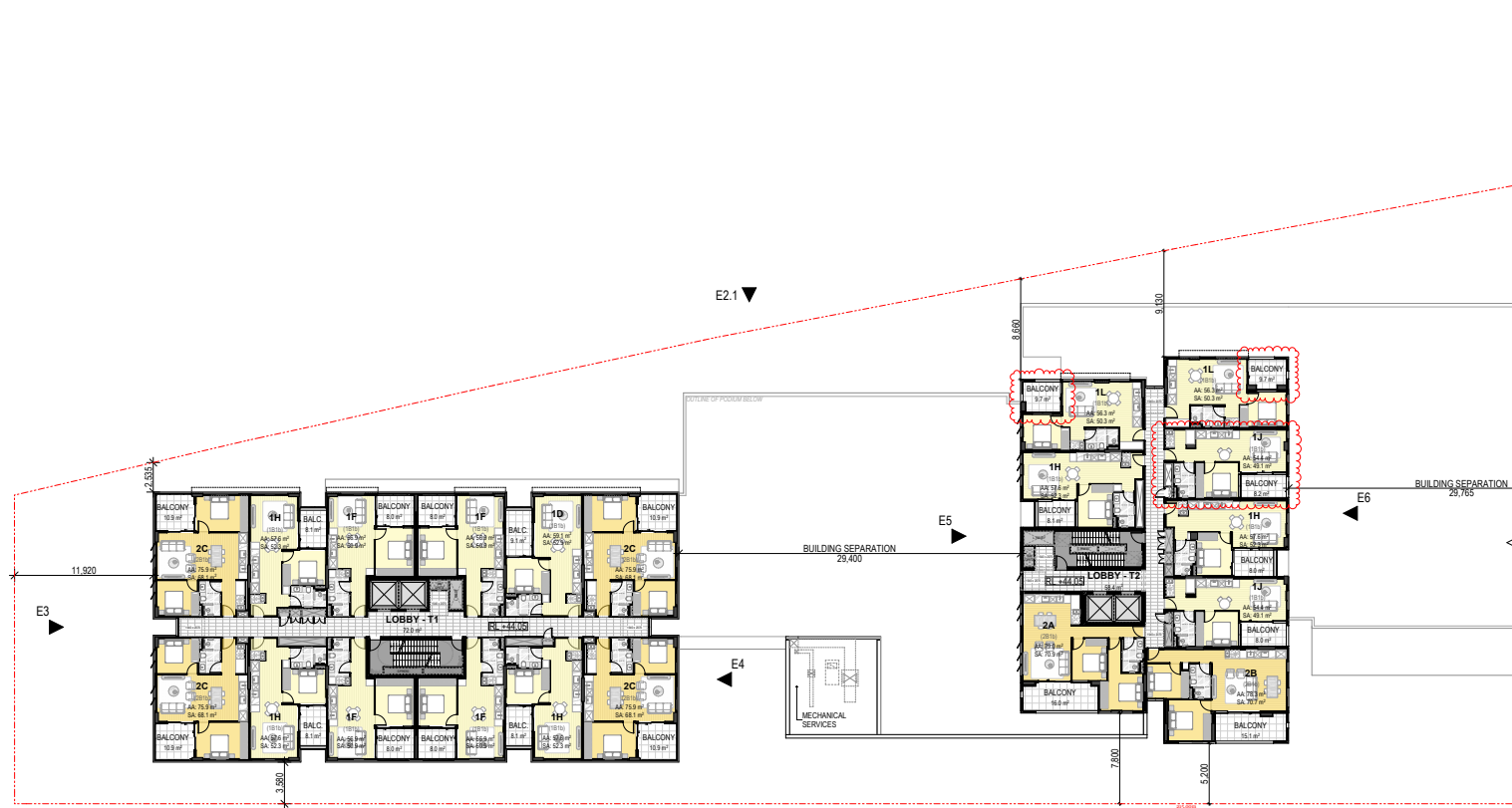
NOTE:
 ALL APARTMENTS DESIGNED TO
 MINIMUM LHA SILVER LEVEL
 REQUIREMENTS. REFER TO ACCESS
 CONSULTANT REPORT FOR DETAIL.



Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

Drawing Name: T1 & T2 - Top of Podium
 Drawing Scale: 1:200 @ A1
 Drawing No: DA212
 Revision: B

| TOWER 1 & 2 - LEVEL 03-14



REFER TO DA224 FOR CONTINUATION

Rev	Date	By	Check	Description
1	04/01/2018	LIA/LM/TD	COL	Issue for CDA - Issues
2	20/01/2018	LIA/LM/TD	COL	Issue for Development Application
3	20/01/2018	LIA/LM/TD	COL	Issue for CA Form
4	18/01/2018	LIA/LM/TD	COL	Issue for CA Review
5	18/01/2018	LIA/LM/TD	COL	Issue for Coordination

All works to be in accordance with authority & statutory approvals.
 Refer to the terms of any information relating to existing site conditions.
 All structural information to be confirmed to registered engineer before commencing.
 Refer to related Report and drawings. Documentation for all information relating to:
 - Base and/or intermediate levels of construction work.
 - Changes to the floor or construction with the Specifications and Schedule, as approved under the Development Application, unless otherwise indicated.
 - Base and/or intermediate levels of construction work to be used after Development Commencement.
 - All work to be done in accordance with the relevant Australian Standards.
 - Building Contractors to verify all dimensions before commencing work.

RECEIVED
 METROPOLITAN DEVELOPMENT AUTHORITY ACT 2011
 18 JAN 2018
NOTE:
 ALL APARTMENTS DESIGNED TO
 MINIMUM LHA SILVER LEVEL
 REQUIREMENTS. REFER TO ACCESS
 CONSULTANT REPORT FOR DETAIL.

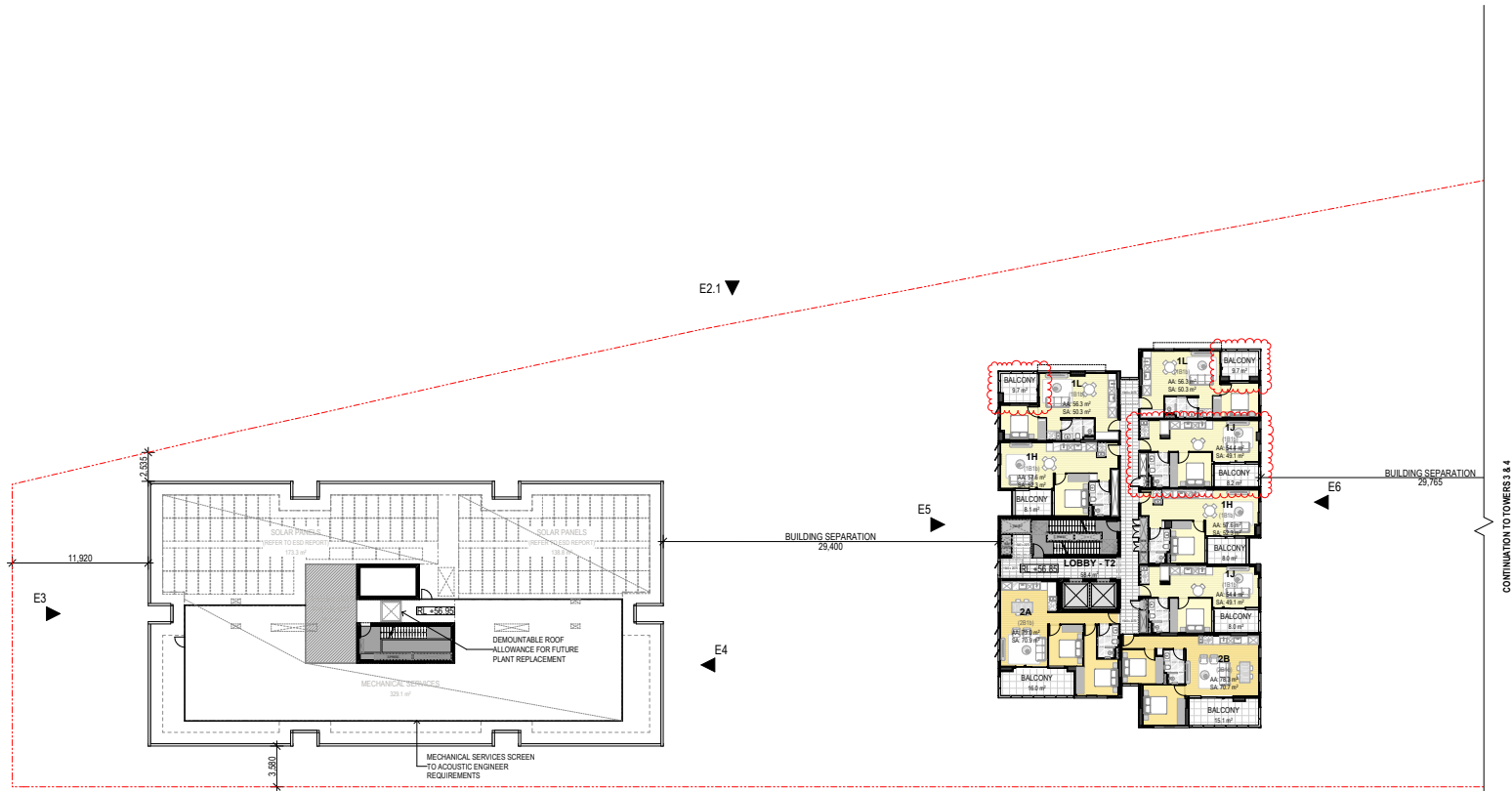


Project Name: Court Place
 Project Number: 00013365
 Project Address: 164 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T1 & T2 - Typical Tower
 Drawing Scale: 1:200 @ A1
 Drawing No: DA213
 Revision: B

TOWER 1 - ROOF SERVICES PLAN



DEVELOPMENT APPLICATION

Drawing Name
T1 - Roof Services Plan
Drawing Scale
DA214
1:200 @ A1
Revision
B

Rev	Date	By	Cha	Description
1	04/01/2024	LJA/LMF/DC	COL	Issue for DA - Issues
2	20/01/2024	LJA/LMF/DC	COL	Issue for Development Application
3	20/01/2024	LJA/LMF/DC	COL	Issue for DA Final
4	18/01/2024	LJA/LMF/DC	COL	Issue for DA Review
5	18/01/2024	LJA/LMF/DC	COL	Issue for Development

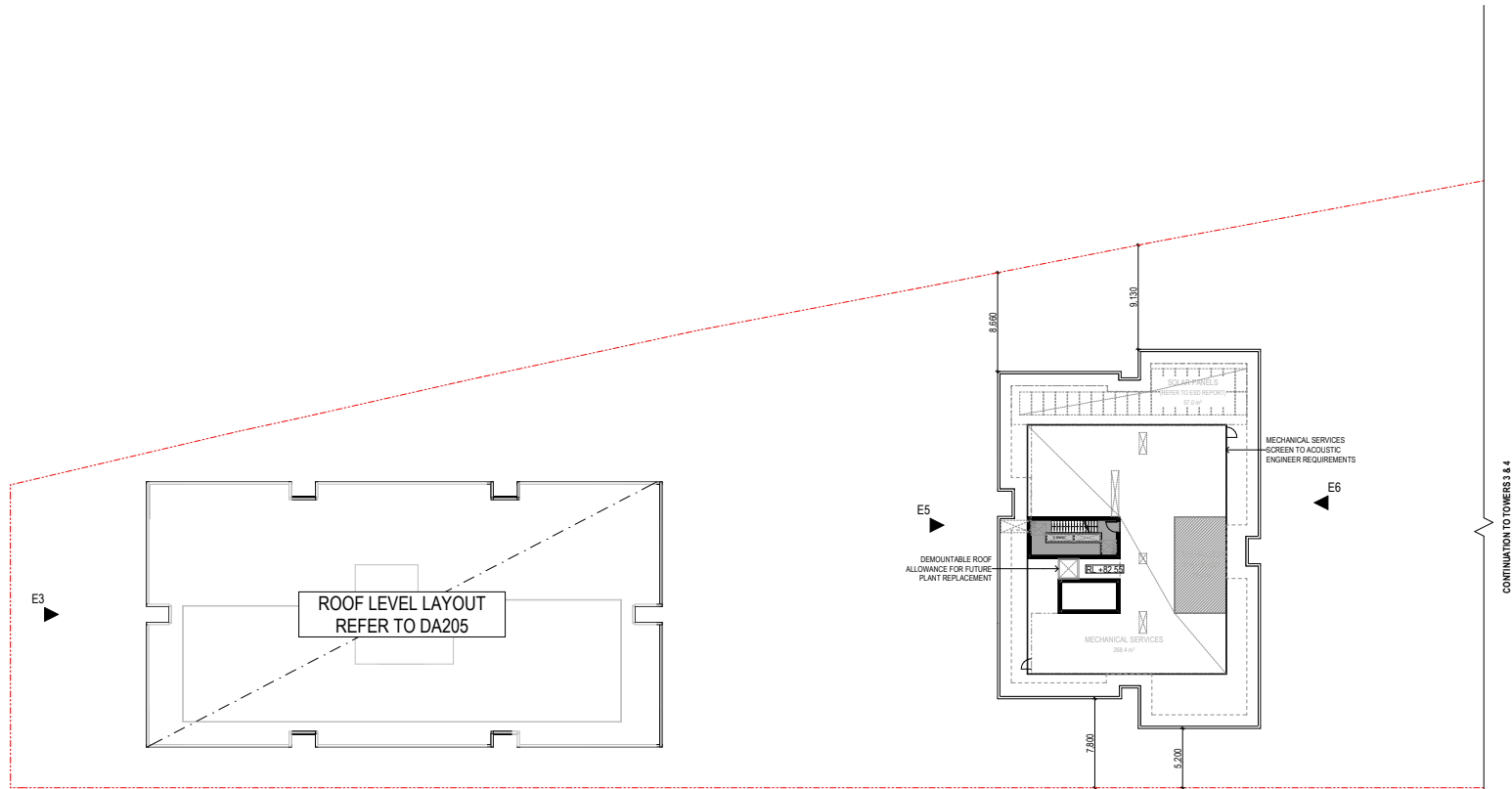
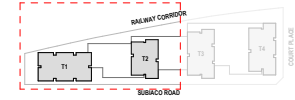
All works to be in accordance with authority & statutory approvals.
 Refer to the terms of any information relating to existing site conditions.
 All drawings, specifications, documents, etc. shall be confirmed to be consistent with the approved development application.
 Plans for related design and construction documents to be submitted to the relevant authority for approval.
 Changes to be made in consultation with the Architectural and Structural Engineer, as approved in writing.
 Plans for related design and construction documents to be submitted to the relevant authority for approval.
 Plans for related design and construction documents to be submitted to the relevant authority for approval.
 Plans for related design and construction documents to be submitted to the relevant authority for approval.
 Plans for related design and construction documents to be submitted to the relevant authority for approval.

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 METROPOLITAN DEVELOPMENT AUTHORITY ACT 2011
NOTE:
 ALL APARTMENTS DESIGNED TO
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 REQUIREMENTS. REFER TO ACCESS
 CONSULTANT REPORT FOR DETAIL.



Project Name
Court Place
Project Number
00013365
Project Address
154 Subiaco Road
Subiaco WA 6008
Country
Aus.

TOWER 2 - ROOF SERVICES PLAN



Rev	Date	By	Chk	Description
1	04/04/2018	LIA/LM/TO	COL	Issue for CDR - Issues
2	26/01/2018	LIA/LM/TO	COL	Issue for Development Application
3	20/01/2018	LIA/LM/TO	COL	Issue for CA Panel
4	18/01/2018	LIA/LM/TO	COL	Issue for CA Review
5	18/01/2018	LIA/LM/TO	COL	Issue for Coordination

All services to be in accordance with authority & statutory approvals.
 Refer to the terms of any information relating to existing site conditions.
 All structural information to be confirmed to registered engineer before commencing.
 Refer to Access Report and Acoustic Documentation for all information relating to
 floor and roof construction, and all structural work.
 Changes to be made in consultation with all Specifications and Schedules, all
 approvals and other relevant documents, and in accordance with the relevant
 Building Code of Australia and other applicable laws.
 Refer to the relevant Building Code of Australia and other applicable laws.
 The information shown on this drawing is for information only and is not to be used
 for any other purpose without the written consent of the relevant authority.
 Building Contractors to verify all dimensions before commencing work.

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 METROPOLITAN DEVELOPMENT
 AUTHORITY ACT 2011

NOTE:
 ALL APARTMENTS DESIGNED TO
 MINIMUM LHA SILVER LEVEL
 REQUIREMENTS. REFER TO ACCESS
 CONSULTANT REPORT FOR DETAIL.

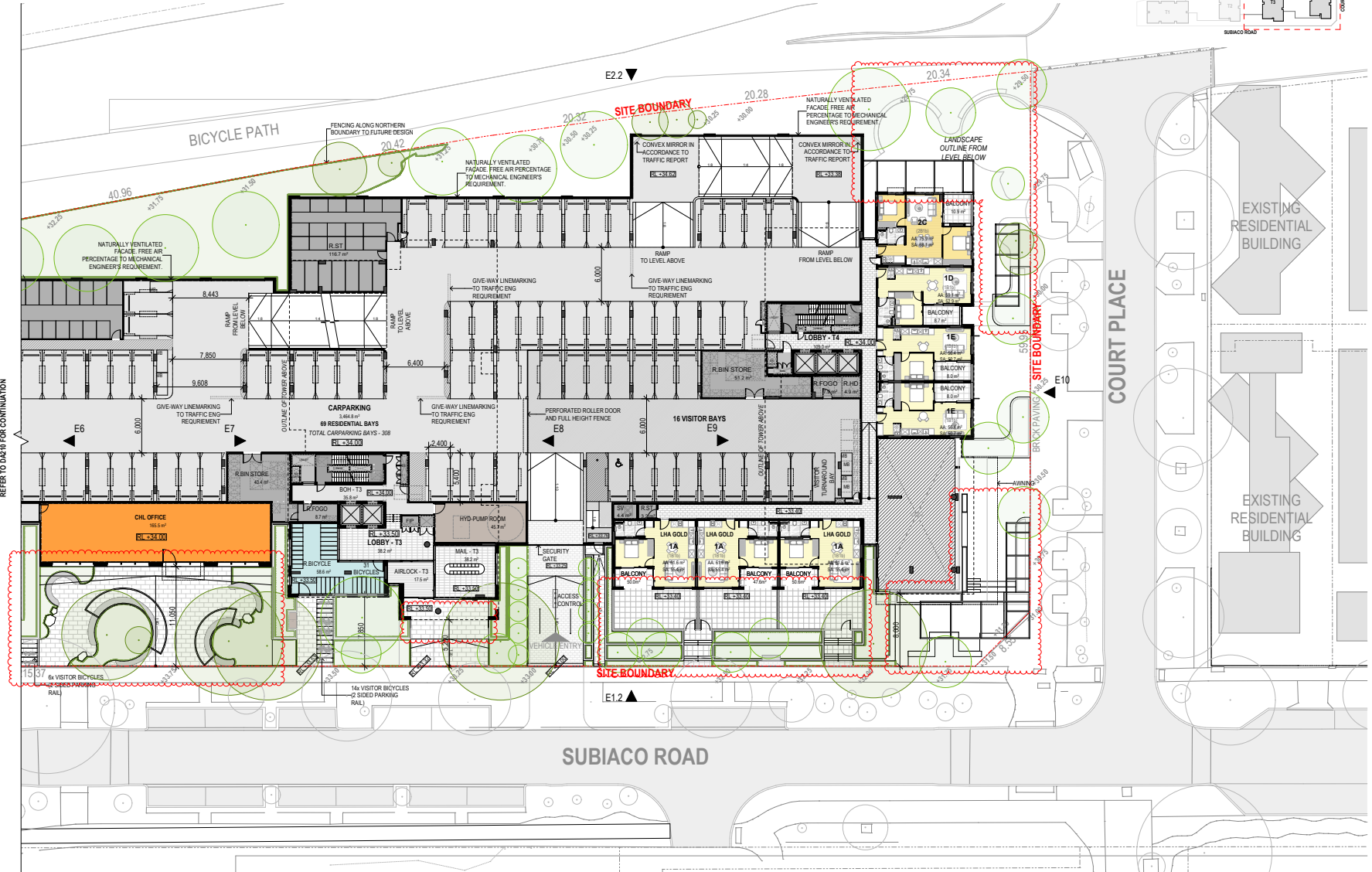


Project Name: Court Place
 Project Number: 00013365
 Project Address: 144 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T2 - Roof Services Plan
 Drawing Scale: DA215
 1:200 @ A1
 Revision: B

TOWER 3 & 4 - UPPER GROUND



REFER TO DA219 FOR CONTINUATION

No.	Date	By	Check	Description
1	28/01/2024	LJA, MF, T2	COL	Initial Design & Issue
2	28/01/2024	LJA, MF, T2	COL	Issue for Development Application
3	28/01/2024	LJA, MF, T2	COL	Issue for DA
4	18/05/2024	LJA, MF, T2	COL	Issue for DA Review
5	18/05/2024	LJA, MF, T2	COL	Issue for Development

All works to be in accordance with authority & statutory approvals.
 Plans to be used for information only & not for construction.
 Refer to DA for details of construction & materials.
 Refer to DA for details of construction & materials.
 Refer to DA for details of construction & materials.
 Refer to DA for details of construction & materials.



NOTE:
 ALL APARTMENTS DESIGNED TO
 MINIMUM LHA SILVER LEVEL
 REQUIREMENTS. REFER TO ACCESS
 CONSULTANT REPORT FOR DETAIL.

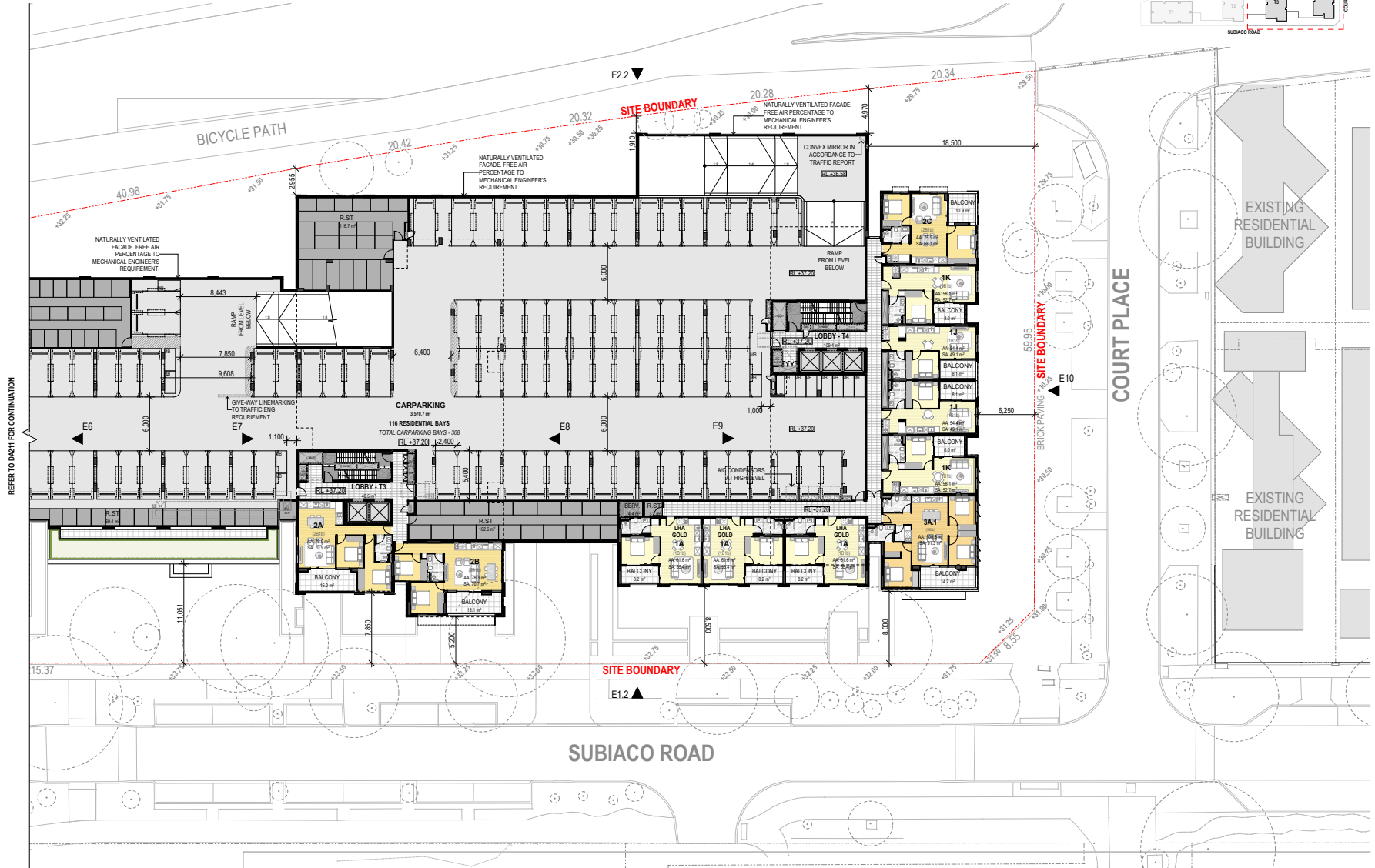


Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road
 Subiaco WA 6008
 Country: Aus.

DEVELOPMENT APPLICATION

Drawing Name: T3 & T4 - Upper Ground Plan
 Drawing Scale: DA221
 1:200 @ A1
 Revision: B

TOWER 3 & 4 - LEVEL 01



REFER TO D&T FOR CONTINUATION

METROPOLITAN DEVELOPMENT AUTHORITY ACT 2011
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NOTE:
ALL APARTMENTS DESIGNED TO MINIMUM LHA SILVER LEVEL REQUIREMENTS. REFER TO ACCESS CONSULTANT REPORT FOR DETAIL.

Rev	Date	By	Check	Description
1	20/01/2024	LJA,MP,TD	DL	Issue for Development Application
2	20/01/2024	LJA,MP,TD	DL	Issue for Development Application
3	20/01/2024	LJA,MP,TD	DL	Issue for Development Application
4	18/03/2024	LJA,MP,TD	DL	Issue for Development Application
5	18/03/2024	LJA,MP,TD	DL	Issue for Development Application

All services to be in accordance with authority & statutory approvals.
 Plans to be used for construction or other purposes only.
 Authority of this plan is limited to the information contained in the accompanying
 report and is not to be used for any other purpose.
 Plans to be used for construction or other purposes only.
 Authority of this plan is limited to the information contained in the accompanying
 report and is not to be used for any other purpose.
 Plans to be used for construction or other purposes only.
 Authority of this plan is limited to the information contained in the accompanying
 report and is not to be used for any other purpose.

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 Ho Chi Minh
 City
 Vietnam
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 Sydney

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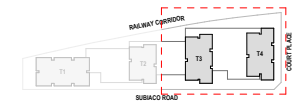
dko.com.au
 67538 Subiaco Way
 T 08 9 6556 0450

Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road
 Subiaco WA 6008

DEVELOPMENT APPLICATION

Drawing Name: T3 & T4 - Level 01
 Drawing Scale: DA222
 1:200 @ A1
 Revision: B

TOWER 3 & 4 - LEVEL 02 / TOP OF PODIUM



Rev	Date	By	Cha	Description
1	20/01/2024	LJA, MP, TO	COL	Issue for Design Application
2	20/01/2024	LJA, MP, TO	COL	Issue for Design Application
3	20/01/2024	LJA, MP, TO	COL	Issue for Design Application
4	20/01/2024	LJA, MP, TO	COL	Issue for Design Application
5	20/01/2024	LJA, MP, TO	COL	Issue for Design Application

All works to be in accordance with authority & statutory approvals.
 Refer to the terms of any information relating to existing site conditions.
 All dimensions are in millimetres unless otherwise stated.
 All dimensions are to the finished surface unless otherwise stated.
 Refer to the relevant Report and Schedule of Documents for all dimensions relating to:
 - Site and site boundaries, including all easements and rights.
 - Changes to the site or completion with all Specifications and Schedules, all
 approved plans for development, including all site and site conditions.
 - All other relevant information.
 The drawings are intended to be used in conjunction with the relevant Development
 Consent.
 The drawings are intended to be used in conjunction with the relevant Development
 Consent.
 The drawings are intended to be used in conjunction with the relevant Development
 Consent.

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 METROPOLITAN RESIDENTIAL DEVELOPMENT
 AUTHORITY ACT 2011

NOTE:
 ALL APARTMENTS DESIGNED TO
 MINIMUM LHA SILVER LEVEL
 REQUIREMENTS. REFER TO ACCESS
 CONSULTANT REPORT FOR DETAIL.

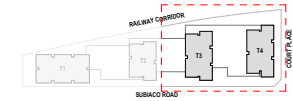


Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T3 & T4 - Top of Podium
 Drawing Scale: 1:200 @ A1
 Drawing No: DA223
 Revision: B

TOWER 3 & 4 - LEVEL 03-17



REFER TO DA23 FOR CONTINUATION

METROPOLITAN RESIDENTIAL DEVELOPMENT AUTHORITY ACT 2011
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NOTE:
ALL APARTMENTS DESIGNED TO
MINIMUM LHA SILVER LEVEL
REQUIREMENTS. REFER TO ACCESS
CONSULTANT REPORT FOR DETAIL.

Rev	Date	By	Check	Description
1	04/04/2024	LJA, MPT, DCL	DCL	Issue for Development Application
2	20/04/2024	LJA, MPT, DCL	DCL	Issue for Development Application
3	20/04/2024	LJA, MPT, DCL	DCL	Issue for Development Application
4	18/05/2024	LJA, MPT, DCL	DCL	Issue for Development Application
5	18/05/2024	LJA, MPT, DCL	DCL	Issue for Development Application

All works to be in accordance with authority & statutory approvals.
 Refer to the notes for all information relating to building site conditions.
 All works to be in accordance with authority & statutory approvals.
 Refer to the notes for all information relating to building site conditions.
 All works to be in accordance with authority & statutory approvals.
 Refer to the notes for all information relating to building site conditions.

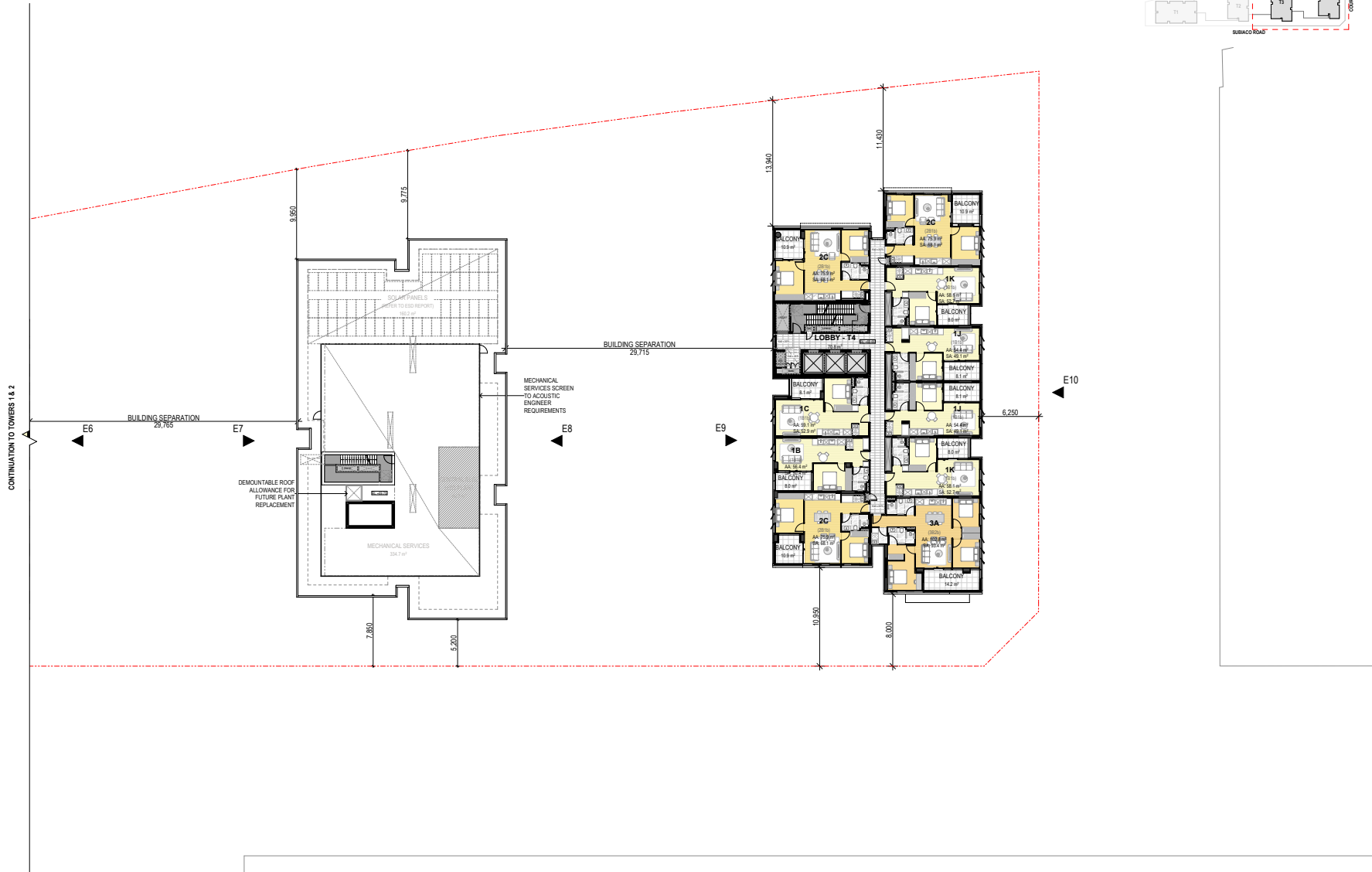
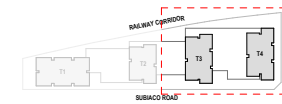


Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T3 & T4 - Typical Tower
 Drawing Scale: 1:200 @ A1
 Revision: B

TOWER 3 - ROOF SERVICES PLAN



Rev	Date	By	Check	Description
A	04/04/2024	LIA/M/TO	COL	Issue for CA Review
B	26/01/2024	LIA/M/TO	COL	Issue for Development Application
C	22/01/2024	LIA/M/TO	COL	Issue for CA Review
D	19/01/2024	LIA/M/TO	COL	Issue for CA Review
E	19/01/2024	LIA/M/TO	COL	Issue for Development

All services to be in accordance with authority & statutory approvals.
 Refer to site plan for all information relating to existing site conditions.
 All services to be confirmed to be confirmed to proposed contract before commencing.
 Refer to Access Report and Acoustic Assessment for all information relating to
 all services.
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 all services.

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 AUTHORITY ACT 2011

NOTE:
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 REQUIREMENTS. REFER TO ACCESS
 CONSULTANT REPORT FOR DETAIL.

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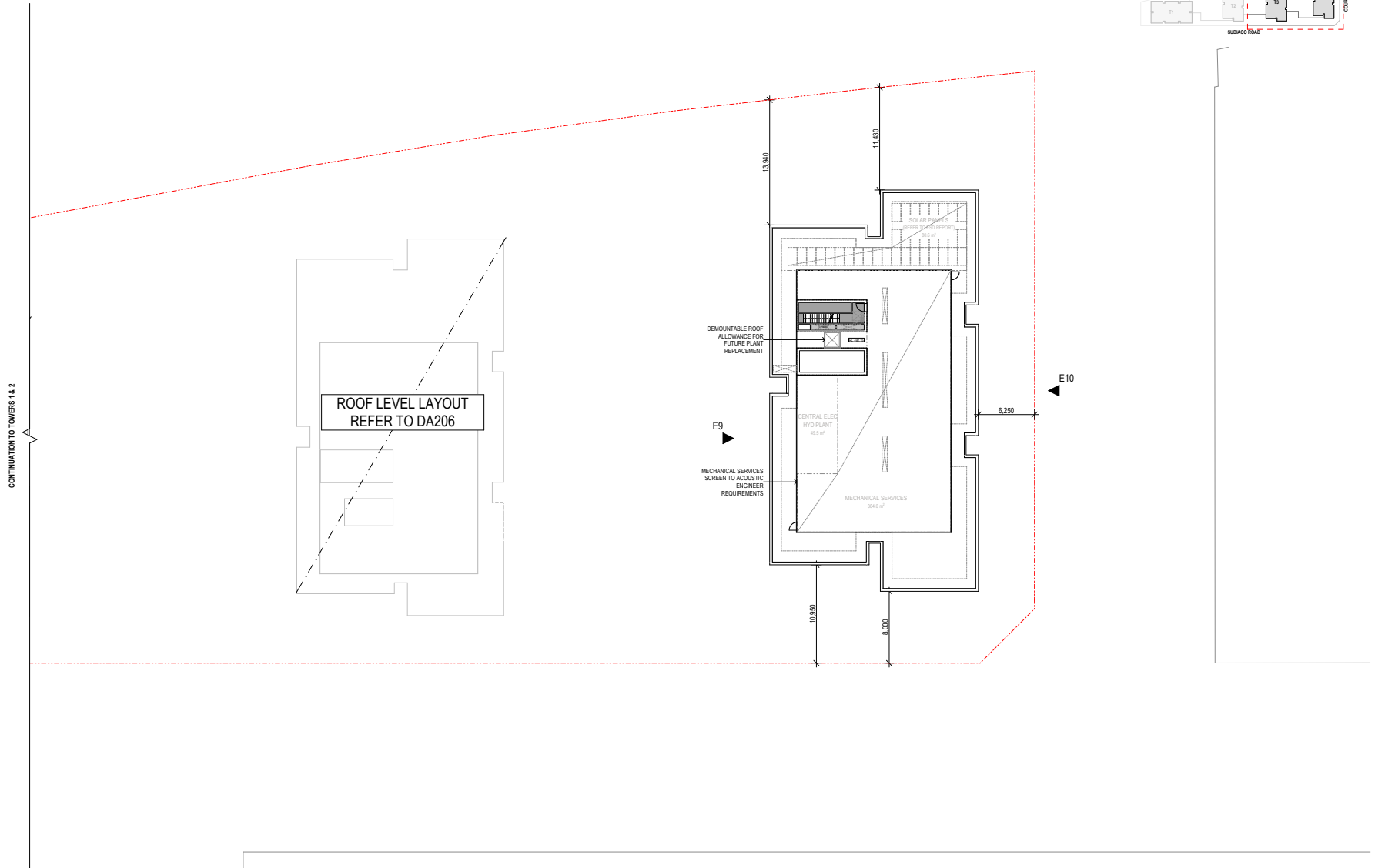
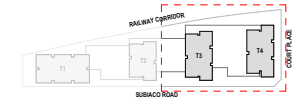
Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008

Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T3 - Roof Services Plan
 Drawing Scale: 1:200 @ A1
 Drawing No: DA225
 Revision: B

| TOWER 4 - ROOF SERVICES PLAN



Rev	Date	By	Obj	Description
1	04/04/2024	LIA/LIA/MD	COL	Issue for Design
2	20/01/2024	LIA/LIA/MD	COL	Issue for Development Application
3	20/01/2024	LIA/LIA/MD	COL	Issue for DA
4	18/01/2024	LIA/LIA/MD	COL	Issue for DA Review
5	18/01/2024	LIA/LIA/MD	COL	Issue for Development

All works to be in accordance with authority & statutory approvals.
 Refer to the terms of any information relating to existing site conditions.
 All drawings, information to be confirmed to registered surveyor before commencing.
 Refer to relevant Report and Planning Documentation for all information relating to:
 - Base and final construction levels and construction notes.
 - Changes to be made in compliance with all Specifications and Schedules, all
 approved and approved documents, and all other relevant documents.
 - Refer to relevant Planning and Construction Plans for all relevant information.
 - Refer to relevant Planning and Construction Plans for all relevant information.
 - Refer to relevant Planning and Construction Plans for all relevant information.
 - Refer to relevant Planning and Construction Plans for all relevant information.

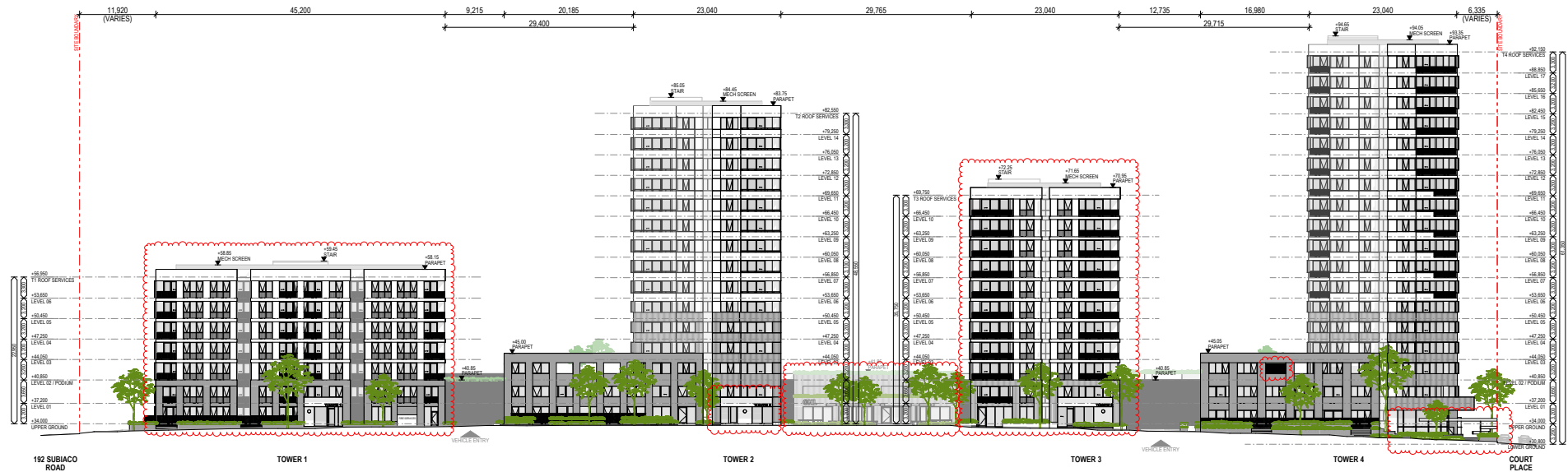
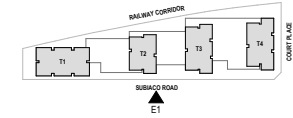
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 METROPOLITAN REDEVELOPMENT AUTHORITY ACT 2011
 NOTE:
 ALL APARTMENTS DESIGNED TO
 MINIMUM LHA SILVER LEVEL
 REQUIREMENTS. REFER TO ACCESS
 CONSULTANT REPORT FOR DETAIL.



Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION
 Drawing Name: T4 - Roof Services Plan
 Drawing Scale: 1:200 @ A1
 Drawing No: DA226
 Revision: B

| SOUTH ELEVATION



E1 SOUTH ELEVATION

Rev	Date	By	Chk	Description
1	08/04/2008	LJ/LK/MP/TZ	COL	Issue for CDA - Issues
2	26/01/2009	LJ/LK/MP/TZ	COL	Issue for Development Application
3	20/03/2009	LJ/LK/MP/TZ	COL	Issue for CDA Final
4	19/04/2009	LJ/LK/MP/TZ	COL	Issue for Construction
5	26/01/2009	TZ/MP	MP	Issue for Construction
6	18/01/2009	TZ/MP	MP	Issue for Planning

METROPOLITAN DEVELOPMENT
AUTHORITY ACT 2016
RECEIVED
08 April 2008

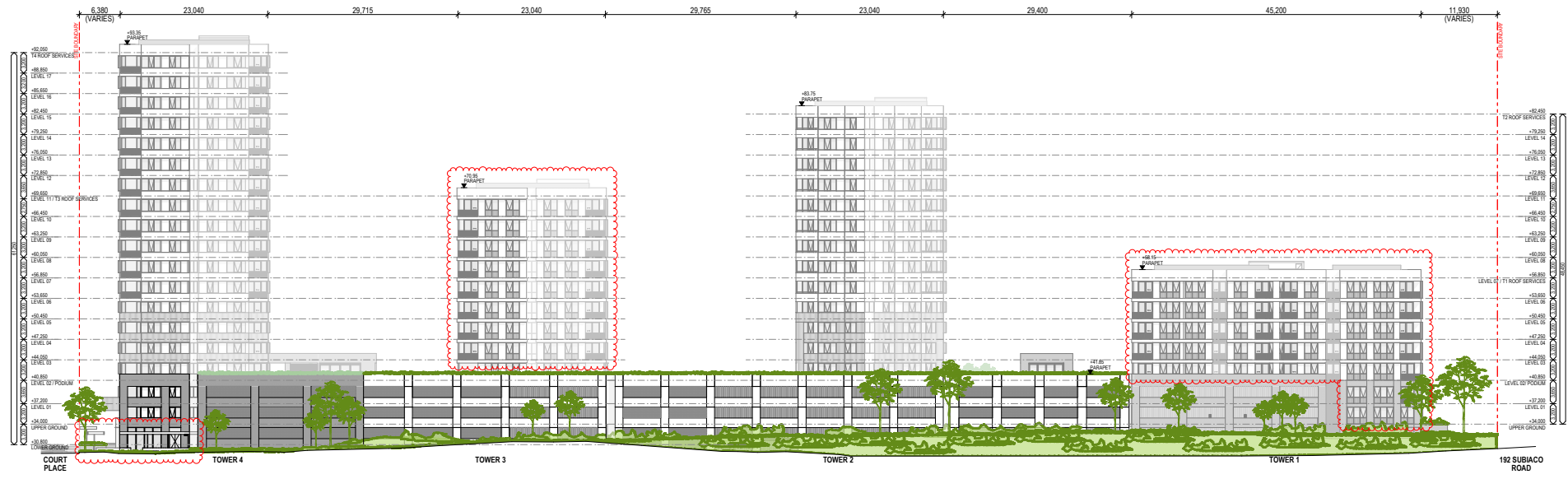
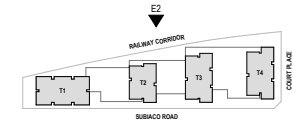
Auckland
Dunedin
Hokitika
Invercargill
Napier
New Plymouth
Palmerston North
Rotorua
Tairāhema
Timaru
Wellington
Whangarei

Project Name: Court Place
Project Number: 00013965
Project Address: 154 Subiaco Road, Subiaco WA 6008
Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: South Elevation
Drawing Scale: 1:500 @ A1
DA300
Revision: B

| NORTH ELEVATION



E2 NORTH ELEVATION

Rev	Date	By	Ofc	Description
01	18/01/2008	LIAK/MP/TC	COL	Issue for OIA - Issues
02	28/01/2008	LIAK/MP/TC	COL	Issue for Development Application
03	20/01/2008	LIAK/MP/TC	COL	Issue for OIA Draft
04	19/01/2008	LIAK/MP/TC	COL	Issue for Construction
05	20/01/2008	TC/MP	LIA	Issue for Construction
06	18/01/2008	TC/MP	LIA	Issue for Planning

All works to be in accordance with authority & statutory approvals.
 Issues to be taken to all information meeting to meeting 200 conditions.
 Authority or OIA permission to be confirmed to implement project before commencing.
 Proceed to obtain Report and Strategic Documentation for all information meeting to
 Issue and Strategic Documentation, and all other works.
 Drawings to be made in compliance with all Specifications and Schedules, at
 appropriate to drawing level in compliance with the relevant after Development
 Commence.
 Do not proceed from drawing, signed dimensions only to be used.
 Do not proceed from drawing, signed dimensions only to be used.
 Issuing Contractors to verify all dimensions before commencing work.



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Project Name: Court Place
 Project Number: 00013956
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: North Elevation
 Drawing Scale: 1:500 @ A1
 Drawing No: DA301
 Revision: B

TOWER 1 & 2 - SOUTH ELEVATION,



E1.1 TOWER 1 & 2 - SOUTH ELEVATION

ALL			TOWER 1			TOWER 2			TOWER 3			TOWER 4		
1. CLEAR GLASS	2. TINTED CLEAR GLASS	3. CHARCOAL POWDERCOAT	6. CREAM BRICK	7. BROWN BRICK	8. BEIGE TEXTURED FINISH	11. TERRACOTTA BRICK	12. RED BRICK	13. TERRACOTTA TEXTURED FINISH	17. LIGHT GREY TEXTURED FINISH	19. RED BROWN BRICK	20. DARK BROWN BRICK	21. LIGHT / DARK RED TEXTURED FINISH	22. DARK RED TILED FINISH	
4. DARK GREY TEXTURED FINISH	5. CHARCOAL PERFORATED METAL		9. BEIGE CORRUGATED METAL	10. BEIGE PERFORATED CORRUGATED METAL		14. TERRACOTTA CORRUGATED METAL	15. TERRACOTTA PERFORATED CORRUGATED METAL	16. TERRACOTTA RIBBED TEXTURED FINISH	18. LIGHT GREY RIBBED TEXTURED FINISH	23. LIGHT RED CORRUGATED METAL	24. LIGHT RED PERFORATED CORRUGATED METAL	25. LIGHT / DARK RED RIBBED TEXTURED FINISH		



Rev	Date	By	Chk	Description
A	28/01/2018	L.A.L.M./T.C.	D.C.L.	Issue for Development Application
B	20/04/2018	L.A.L.M./T.C.	D.C.L.	Issue for Construction
C	18/10/2018	L.A.L.M./T.C.	D.C.L.	Issue for Construction

All works to be in accordance with authority & statutory approvals.
 Refer to the terms of any relevant planning or building consent conditions.
 Any necessary permission to be confirmed by written consent.
 Refer to relevant design and construction documents for all information relating to
 site and construction details. All construction work
 is to be in accordance with all Specifications and Schedules, all
 approved design documents in compliance with the relevant Development
 Consent.
 The contractor must ensure appropriate signage is installed in accordance with
 planning conditions to comply with all relevant planning consent conditions.

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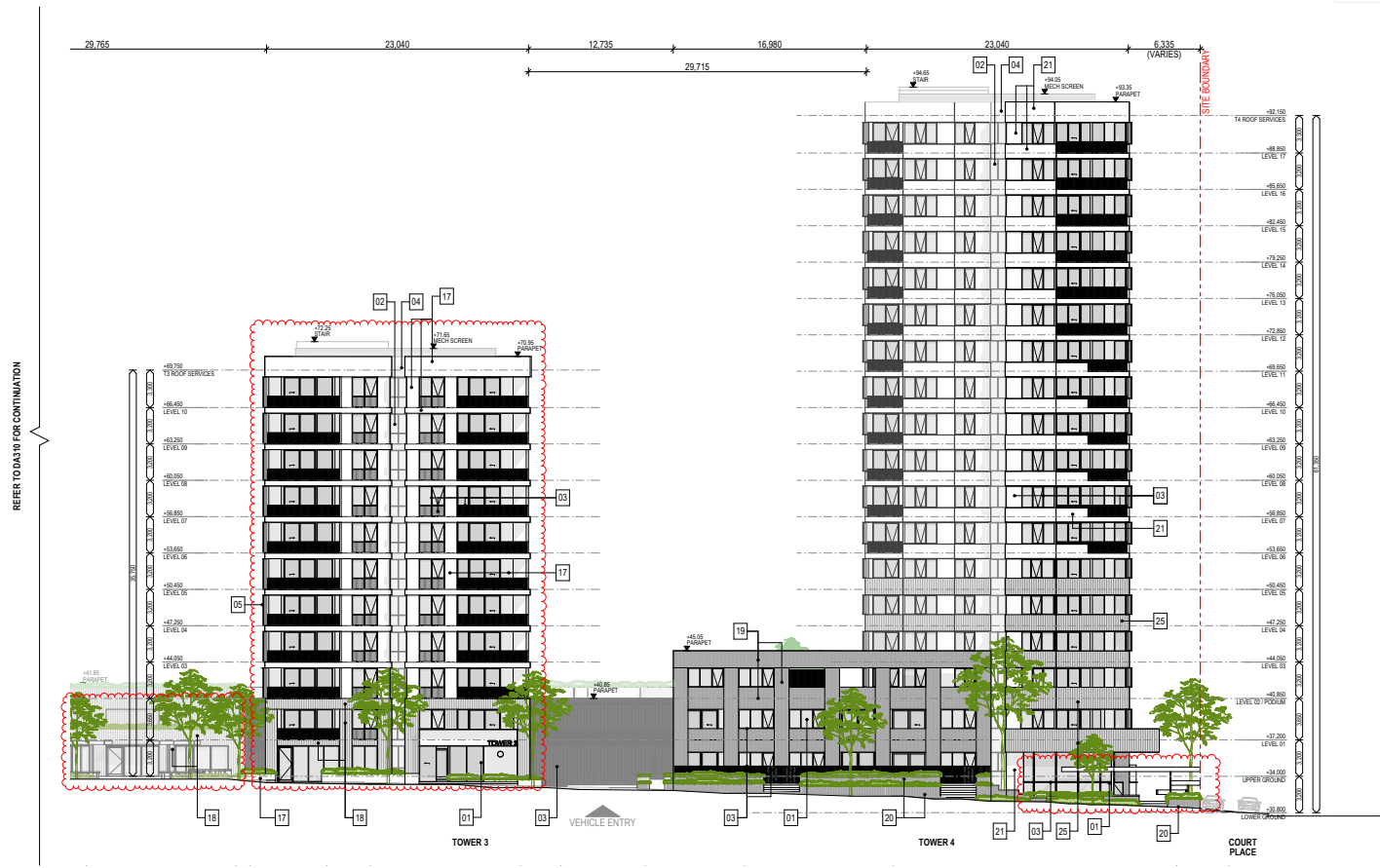
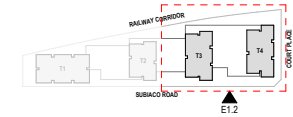
Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008

Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T1 & T2 - South Elevation
 Drawing No: DA310
 Project No: 00013365
 1:200 @ A1
 Revision: B

| TOWER 3 & 4 - SOUTH ELEVATION,



E12 TOWER 3 & 4 - SOUTH ELEVATION

ALL			TOWER 1			TOWER 2			TOWER 3		TOWER 4			
1. CLEAR GLASS	2. TINTED CLEAR GLASS	3. CHARCOAL POWDERCOAT	6. CREAM BRICK	7. BROWN BRICK	8. BEIGE TEXTURED FINISH	11. TERRACOTTA BRICK	12. RED BRICK	13. TERRACOTTA TEXTURED FINISH	17. LIGHT GREY TEXTURED FINISH	19. RED BROWN BRICK	20. DARK BROWN BRICK	21. LIGHT / DARK RED TEXTURED FINISH	22. DARK RED TILED FINISH	
4. DARK GREY TEXTURED FINISH	5. CHARCOAL PERFORATED METAL		9. BEIGE CORRUGATED METAL	10. BEIGE PERFORATED CORRUGATED METAL		14. TERRACOTTA CORRUGATED METAL	15. TERRACOTTA PERFORATED CORRUGATED METAL	16. TERRACOTTA RIBBED TEXTURED FINISH	18. LIGHT GREY RIBBED TEXTURED FINISH	23. LIGHT RED CORRUGATED METAL	24. LIGHT RED PERFORATED CORRUGATED METAL	25. LIGHT / DARK RED RIBBED TEXTURED FINISH		

Rev	Date	By	Check	Description
1	04/04/2008	LIA/LM/TZ	COL	Issue for OIA - Issues
2	20/04/2008	LIA/LM/TZ	COL	Issue for Development Application
3	20/04/2008	LIA/LM/TZ	COL	Issue for OIA Panel
4	16/10/2008	LIA/LM/TZ	COL	Issue for Construction

All services to be in accordance with authority & statutory approvals.
 Refer to the relevant DC or information meeting for existing site conditions.
 All dimensions are in millimetres unless otherwise stated.
 Refer to the relevant Report and/or drawings for all information relating to
 the site and its surroundings. All dimensions are in millimetres unless
 otherwise stated.
 Drawing to be used in conjunction with all Specifications and Schedules, all
 approved drawings and documents. All dimensions are in millimetres unless
 otherwise stated.
 The drawing is for information only and is not to be used for construction.
 The drawing is for information only and is not to be used for construction.
 The drawing is for information only and is not to be used for construction.



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 Christchurch
 Hamilton
 Dunedin
 Tauranga
 Napier-Hastings
 Palmerston North
 Porirua
 Rotorua
 Taranaki
 Timaru
 Upper Meruere
 Whangarei

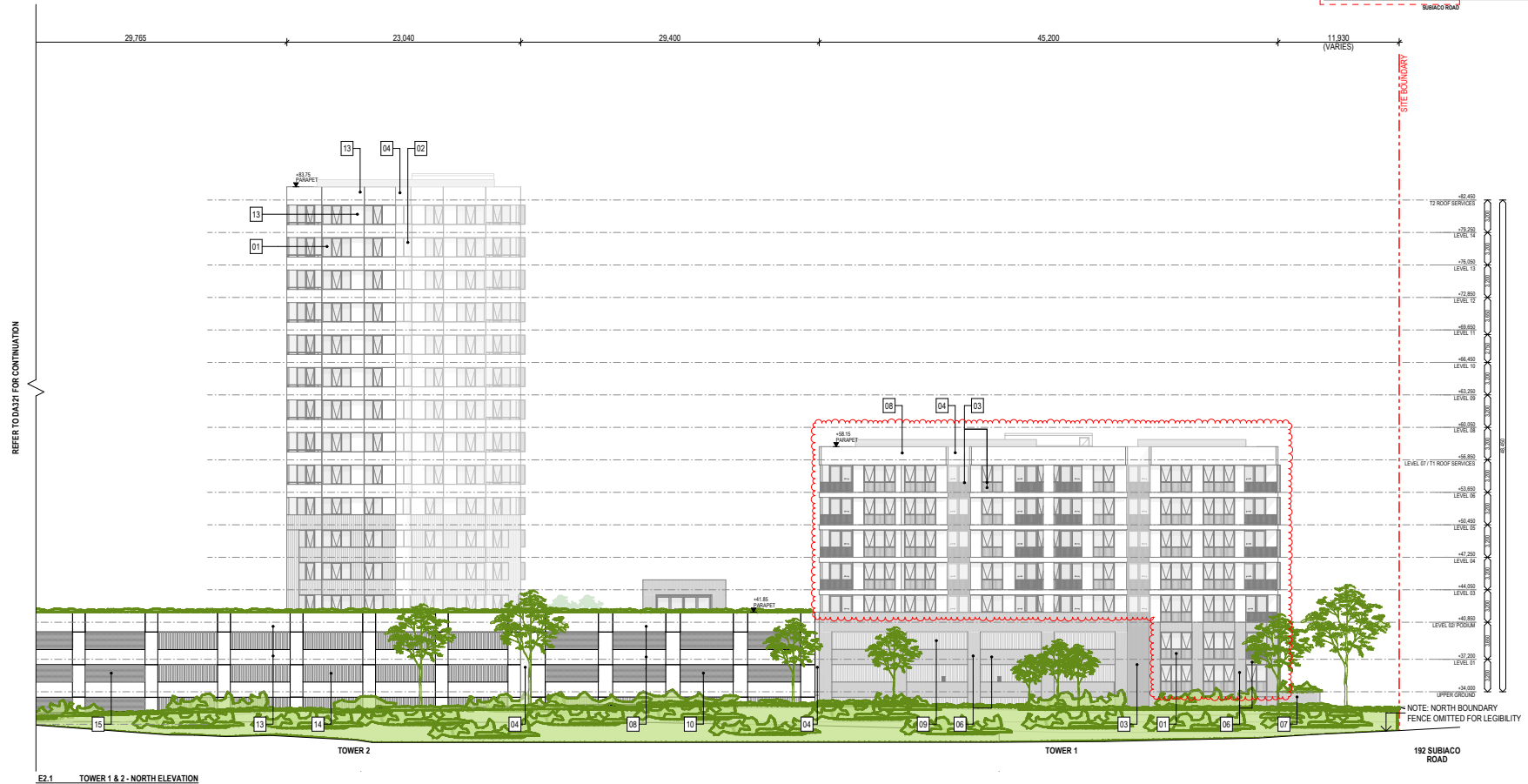
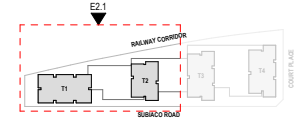
DKO

Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T3 & T4 - South Elevation
 Drawing Scale: 1:200 @ A1
 Drawing No: DA311
 Revision: B

TOWER 1 & 2 - NORTH ELEVATION,



REFER TO DA31 FOR CONTINUATION

E2.1 TOWER 1 & 2 - NORTH ELEVATION

ALL			TOWER 1			TOWER 2			TOWER 3			TOWER 4		
1. CLEAR GLASS	2. TINTED CLEAR GLASS	3. CHARCOAL POWDERCOAT	6. CREAM BRICK	7. BROWN BRICK	8. BEIGE TEXTURED FINISH	11. TERRACOTTA BRICK	12. RED BRICK	13. TERRACOTTA TEXTURED FINISH	17. LIGHT GREY TEXTURED FINISH	19. RED BROWN BRICK	20. DARK BROWN BRICK	21. LIGHT / DARK RED TEXTURED FINISH	22. DARK RED TILED FINISH	
4. DARK GREY TEXTURED FINISH	5. CHARCOAL CORRUGATED METAL		9. BEIGE CORRUGATED METAL	10. BEIGE PERFORATED CORRUGATED METAL		14. TERRACOTTA CORRUGATED METAL	15. TERRACOTTA PERFORATED CORRUGATED METAL	16. TERRACOTTA RIBBED TEXTURED FINISH	18. LIGHT GREY RIBBED TEXTURED FINISH	23. LIGHT RED CORRUGATED METAL	24. LIGHT RED PERFORATED CORRUGATED METAL	25. LIGHT / DARK RED RIBBED TEXTURED FINISH		

DEVELOPMENT APPLICATION

Drawing Name
T1 & T2 - North Elevation
Drawing Scale
DA313
1:200 @ A1
Revision
B

Rev Date By Cx Description
A 20/01/2018 LAL/MF/TJ CCL Issue for Development Application
B 20/01/2018 LAL/MF/TJ CCL Issue for Development Application
C 19/10/2018 LAL/MF/TJ CCL Issue for Coordination

All services to be in accordance with authority & statutory approvals.
Refer to the notes for all information relating to existing site conditions.
All dimensions are to be confirmed by the registered surveyor unless otherwise stated.
Refer to the relevant report and landscape documentation for all information relating to site and existing conditions.
Changes to the plan or completion with all Specifications and Schedules, all approved reports and landscape documentation, may be required after Development Consent.
This drawing is for information only and is not to be used for construction.
Building contractors to verify all dimensions before commencing work.

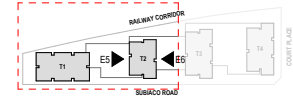


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New Plymouth
Palmerston North
Rotorua
Tairānea
Timaru
Whangarei

Project Name
Court Place
Project Number
00013365
Project Address
154 Subiaco Road
Subiaco WA 6008
Country
Aus.



TOWER 2 - WEST ELEVATION, EAST ELEVATION,



E5 TOWER 2 - WEST ELEVATION

E6 TOWER 2 - EAST ELEVATION

ALL			TOWER 1			TOWER 2			TOWER 3			TOWER 4		
1. CLEAR GLASS	2. TINTED CLEAR GLASS	3. CHARCOAL POWDERCOAT	6. CREAM BRICK	7. BROWN BRICK	8. BEIGE TEXTURED FINISH	11. TERRACOTTA BRICK	12. RED BRICK	13. TERRACOTTA TEXTURED FINISH	17. LIGHT GREY TEXTURED FINISH	19. RED BROWN BRICK	20. DARK BROWN BRICK	21. LIGHT / DARK RED TEXTURED FINISH	22. DARK RED TILED FINISH	
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DEVELOPMENT APPLICATION

Drawing Name
T2 - West & East Elevation
Drawing Scale
DA315
1:200 @ A1
Revision
B

Rev Date By Cda Description
1 04/04/2018 L.A.L./M.F./D.C.L. Issue for Development Application
2 20/04/2018 L.A.L./M.F./D.C.L. Issue for DA Panel
3 19/04/2018 L.A.L./M.F./D.C.L. Issue for Coordination

All works to be in accordance with authority & statutory approvals.
Refer to the terms of any relevant planning or building consent conditions.
No warranty is made by the architect as to the accuracy of information supplied by others.
The architect is not responsible for the construction of any structure or for the safety of any structure.
The architect is not responsible for the construction of any structure or for the safety of any structure.
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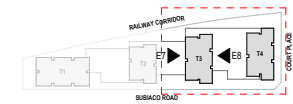


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Whangarei

Project Name
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Project Number
00013365
Project Address
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Subiaco WA 6008
Country
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TOWER 3 - WEST ELEVATION, EAST ELEVATION,



E7 TOWER 3 - WEST ELEVATION

E8 TOWER 3 - EAST ELEVATION

ALL			TOWER 1			TOWER 2			TOWER 3			TOWER 4		
1. CLEAR GLASS	2. TINTED CLEAR GLASS	3. CHARCOAL POWDERCOAT	6. CREAM BRICK	7. BROWN BRICK	8. BEIGE TEXTURED FINISH	11. TERRACOTTA BRICK	12. RED BRICK	13. TERRACOTTA TEXTURED FINISH	17. LIGHT GREY TEXTURED FINISH	19. RED BROWN BRICK	20. DARK BROWN BRICK	21. LIGHT / DARK RED TEXTURED FINISH	22. DARK RED TILED FINISH	
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Rev	Date	By	Check	Description
1	20/10/2018	L.A.N./M.F.T.C.	D.C.L.	Issue for Development Application
2	20/10/2018	L.A.N./M.F.T.C.	D.C.L.	Issue for Construction
3	19/10/2018	L.A.N./M.F.T.C.	D.C.L.	Issue for Construction

All services to be in accordance with authority & statutory approvals.
 Refer to other sheets for information relating to building and construction.
 Refer to other sheets for information relating to building and construction.
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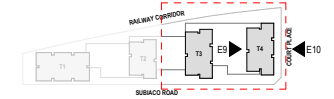
dko.com.au
 67538360.com.au
 T +61 8 9356 0450

Project Name: Court Place
 Project Number: 00013956
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: T3 - West & East Elevation
 Drawing Scale: DA316
 1:200 @ A1
 Revision: B

TOWER 4 - WEST ELEVATION, EAST ELEVATION,



99 TOWER 4 - WEST ELEVATION



E10 TOWER 4 - EAST ELEVATION

ALL			TOWER 1			TOWER 2			TOWER 3			TOWER 4		
1. CLEAR GLASS	2. TINTED CLEAR GLASS	3. CHARCOAL POWDERCOAT	6. CREAM BRICK	7. BROWN BRICK	8. BEIGE TEXTURED FINISH	11. TERRACOTTA BRICK	12. RED BRICK	13. TERRACOTTA TEXTURED FINISH	17. LIGHT GREY TEXTURED FINISH	19. RED BROWN BRICK	20. DARK BROWN BRICK	21. LIGHT / DARK RED TEXTURED FINISH	22. DARK RED TILED FINISH	
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DEVELOPMENT APPLICATION

Rev	Date	By	Check	Description
1	20/01/2018	LAL/MPT/DC	DC	Issue for Development Application
2	20/01/2018	LAL/MPT/DC	DC	Issue for Construction
3	19/04/2018	LAL/MPT/DC	DC	Issue for Construction

All works to be in accordance with authority & statutory approvals.
 Plans to be drawn up in consultation with relevant statutory authorities.
 No warranty is made by the architect as to the accuracy of any information or data supplied by the client or any other person.
 The architect shall not be responsible for any errors or omissions in the drawings or specifications or for any consequences arising therefrom.
 The architect shall not be responsible for any delays or interruptions in the progress of the works or for any consequences arising therefrom.
 The architect shall not be responsible for any damage to property or persons or for any consequences arising therefrom.
 The architect shall not be responsible for any costs or expenses incurred by the client or any other person in connection with the works.
 The architect shall not be responsible for any claims or damages of any kind made against the architect or any other person in connection with the works.
 The architect shall not be responsible for any claims or damages of any kind made against the client or any other person in connection with the works.



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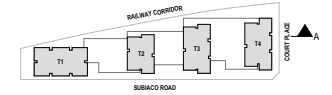
DKO

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 6782863000.au
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Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

Drawing Name: T4 - West & East Elevation
 Drawing Scale: 1:200 @ A1
 Drawing No: DA317
 Revision: B

SECTION A-A



Rev	Date	By	Chk	Description
1	04/04/2016	LIA MFL	DCS	Issue for public consultation
2	26/05/2016	LIA MFL	DCS	Issue for Development Application
3	22/09/2016	LIA MFL	DCS	Issue for CA Panel

All works to be in accordance with authority & statutory approvals.
 Plans to be taken for all information relating to existing and proposed
 works to be confirmed to registered engineer before commencing
 any work.
 Final Design Report and Landmark Documentation for all information relating to
 the works.
 Change to be made in consultation with all Stakeholders and Schedules, as
 approved by the appropriate authority, before the start of construction.
 Final Design Report and Landmark Documentation to be provided after Development
 Consent.
 The works to be taken from existing signed dimensions only to be used.
 The works to be taken from existing signed dimensions only to be used.
 Retaining Contractor to verify all dimensions before commencing work.



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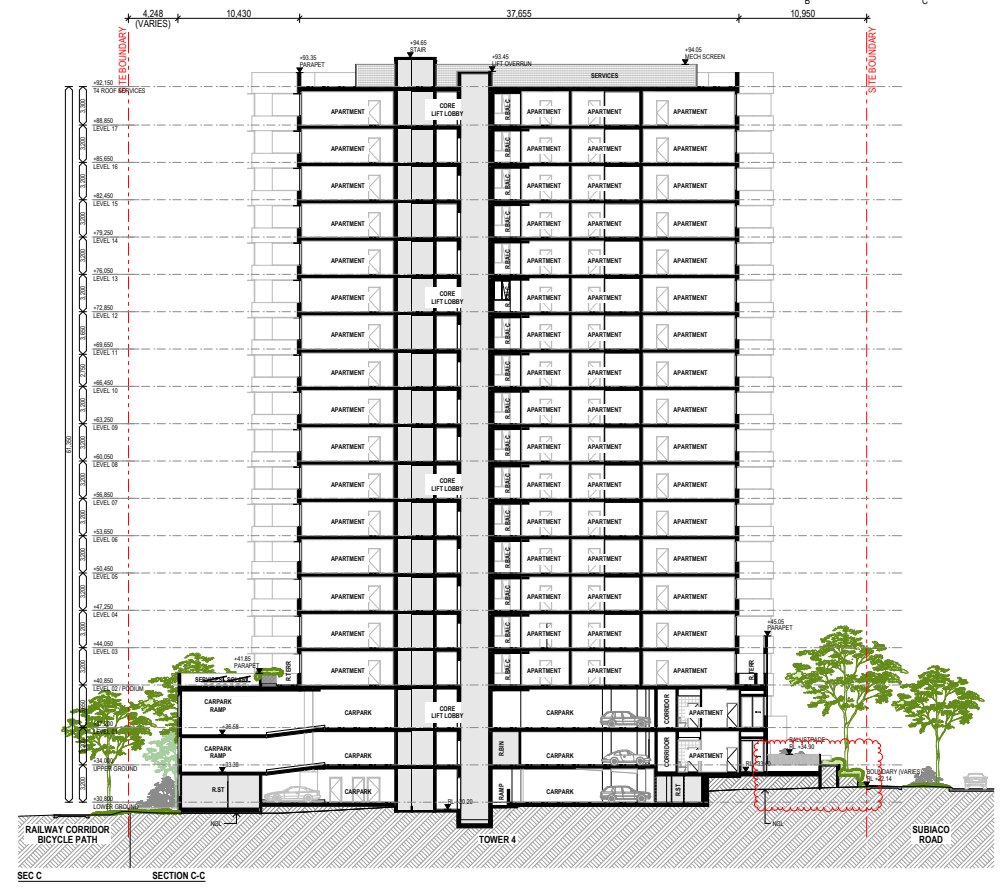
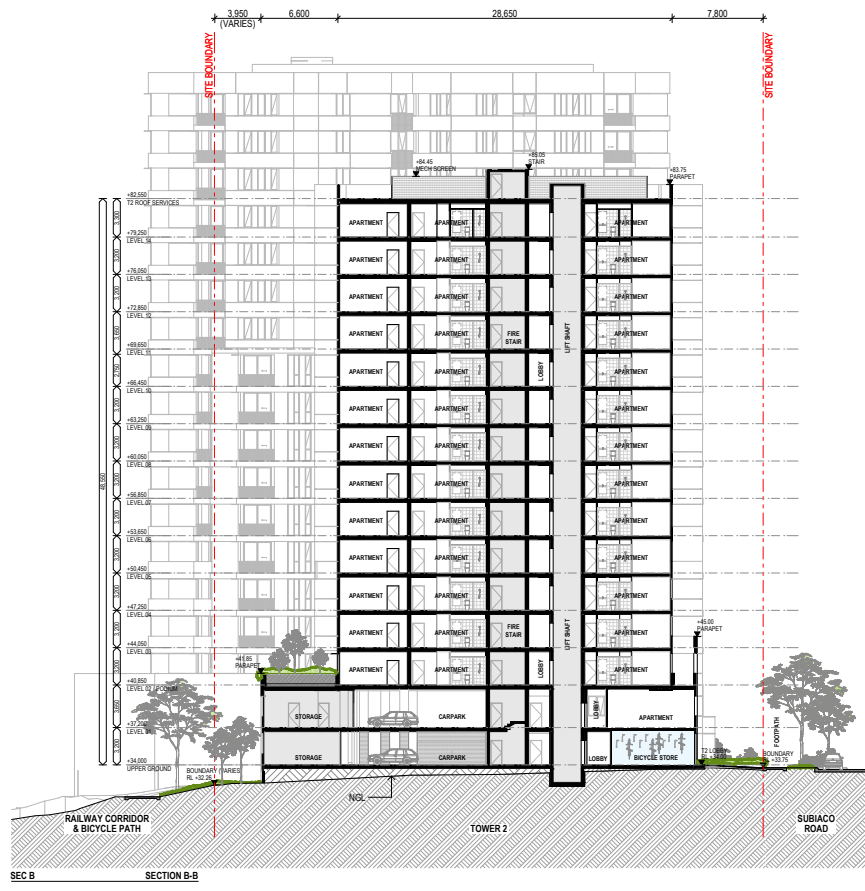
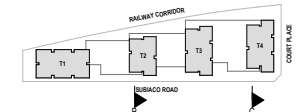
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Project Name: Court Place
 Project Number: 00013365
 Project Address: 154 Subiaco Road, Subiaco WA 6008
 Country: Aust.

DEVELOPMENT APPLICATION

Drawing Name: Section A-A
 Drawing Scale: 1:100 @ A1
 Drawing No: DA320
 Revision: B

| SECTION B-B, SECTION C-C



Rev	Date	By	Chk	Description
A	26/09/2018	L.H. M.P.A.	D.C.L.	Issue for Development Application
B	26/09/2018	L.H. M.P.A.	D.C.L.	Issue for Council

All works to be in accordance with applicable laws and statutory approvals. Plans to be referred to for information relating to existing site conditions. Any use of 'AS SHOWN' is to be confirmed to registered surveyor before commencing works. All other notes and conditions apply. Drawings to be approved by the Council. Changes to be made in consultation with the Surveyor and Schedule of Works. All works to be completed within the time specified in the Contract after Development Commencement. No work to be undertaken until the relevant Development Commencement Date. No work to be undertaken until the relevant Development Commencement Date. No work to be undertaken until the relevant Development Commencement Date.



Architect
D&K O
156 Subiaco Road
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Phone: +61 8 9450 0400
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Project Name
Court Place
Project Number
000113961
Project Address
156 Subiaco Road
Subiaco WA 6008

DEVELOPMENT APPLICATION

Drawing Name
Section B-B & Section C-C
Drawing Scale
DA321
1:200 @ A1
Revision
B

154 SUBIACO ROAD

Landscape Architecture
DRP Comments Document
Community Housing Limited
20.03.2026





Acknowledgment of Country

This project is on Whadjuk Noongar Boodjar.

We pay our respects to their Elders past and present and recognise their ongoing care for and connection to the land that we are working on.

We acknowledge that sovereignty was never ceded.

THE SITE

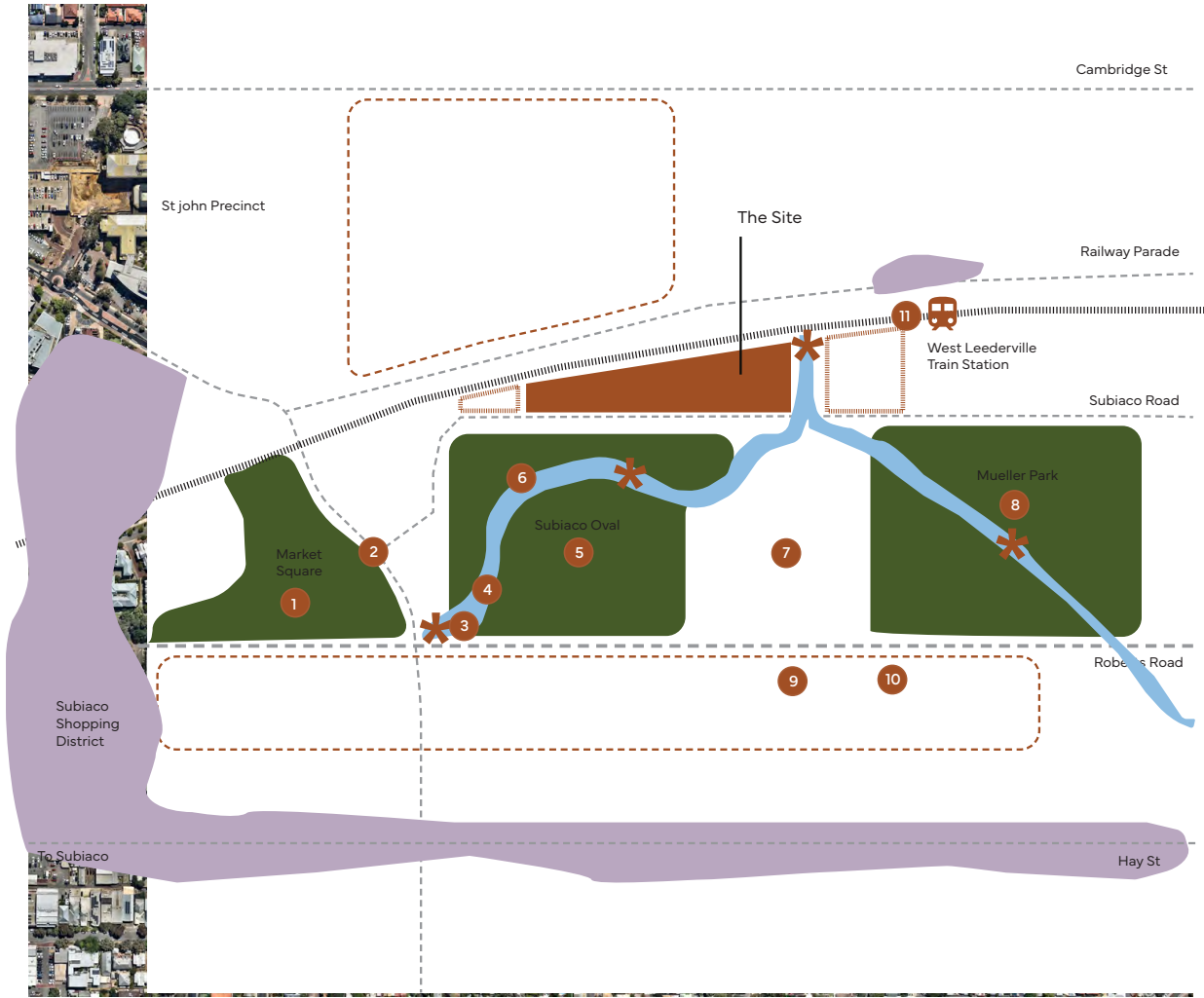
Local Context

Legend

- The site
- Future development
- Parks and Green Space
- Shopping / Food and Beverage Areas
- Bidi Trail
- 1 & 2 Story Residences
- Fremantle Train Line
- Community Spaces / Six Seasons Knowledge Areas
- West Leederville Train Station

Heritage and Cultural Spaces

- 1 Market Square Park
- 2 Haydn Bunton Drive
- 3 Heritage Gates
- 4 Civic square and end of Six Seasons Bidi Trail
- 5 Existing Subiaco Oval
- 6 Retained seating from Subiaco Stadium for viewing
- 7 Bob Hawke College
- 8 Mueller Park
- 9 187 Roberts Road
- 10 Princess Margaret Hospital for Children
- 11 Pedestrian Link to Railway Parade

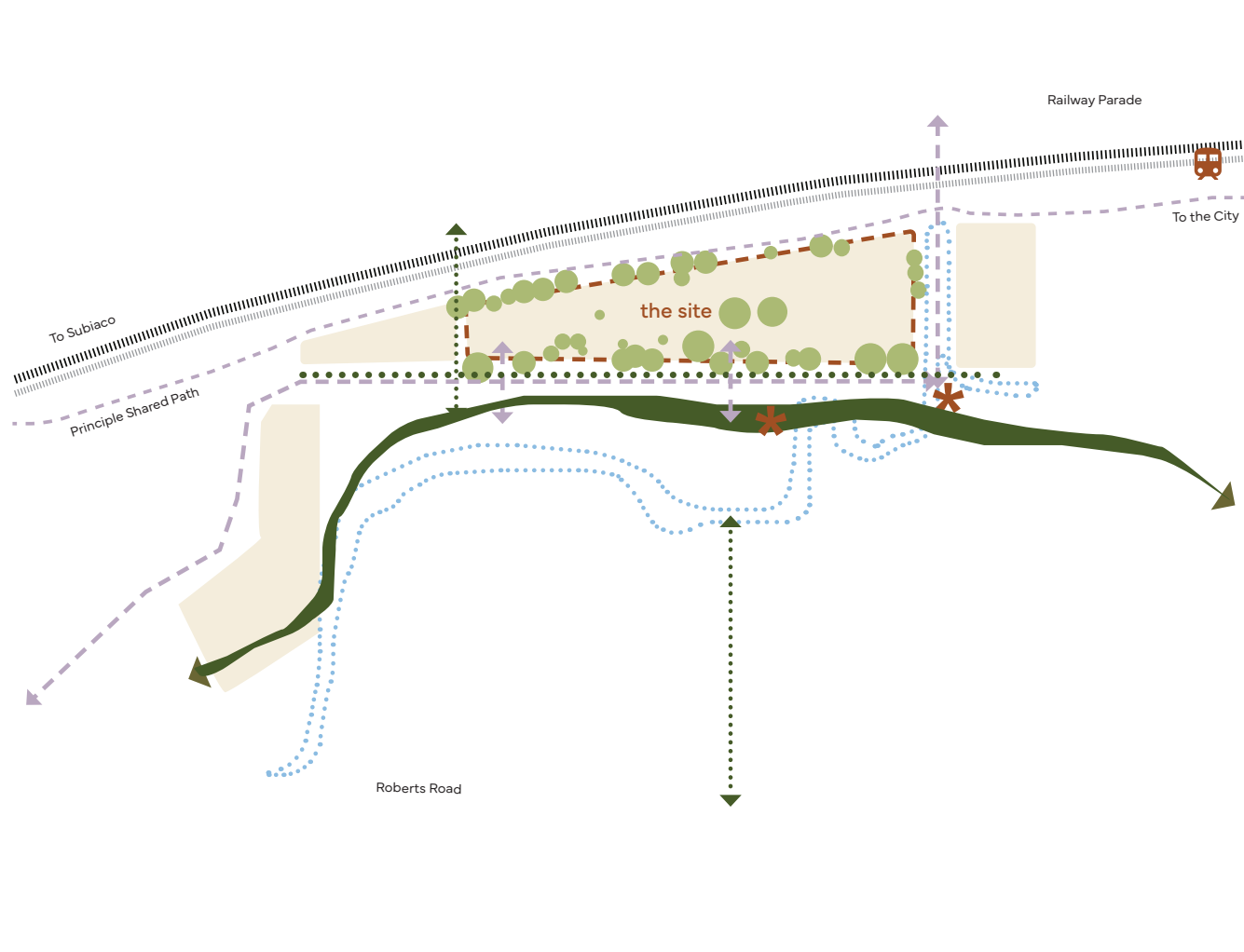


The Site

The site is situated in a location that provides excellent public transport amenity and thoughtful urban realm design. The area has provision for streetscape activation and connection to the surrounding natural and recreational spaces.

Legend

- Activated Streetscape
- Established Tree
- Future Development
- Green Link Opportunity
- ⋯ Bidi Trail Connection
- ↔ Connection to Train station / Recreation
- ◀▶ Opportunity for connection pathways
- ✱ Activation opportunity
- ||||| Noise from train line



Site Condition

The site is predominantly vacant except for a few trees located to the edge and some to the centre of the site.

The existing condition of the streetscape is of high quality, and will serve as a key design driver for the ground plane.

There is good access to the train station, Bob Hawke College, and the Subiaco Oval parkland.

The site has a gentle level change from east to west.



View to Bob Hawke College from Subiaco Road



View to Bob Hawke College from Subiaco Road



View to site from Court Place



Court Place streetscape existing condition



View to Bob Hawke College from cycle path



Subiaco Road streetscape existing condition

Neighborhood Walkability



154 Subiaco Road is located in a walkable neighborhood with cafes, shopping and parks close by. As a vibrant inner city suburb, the topography of the area is relatively flat allowing for easy accessibility to access paths. Pathways are lined with mature deciduous trees creating shade during summer and beauty all year.

Nestled between both Subiaco and West Leederville train stations, and just a short walk to the Subiaco shopping district, the site offers great neighbourhood walkability and connection.

Walkable neighborhoods create healthy livable communities that are well connected to schools, community, meeting places and have easy access to nature. Exchanging short car trips for active transport (walking or cycling) has huge benefits for individuals and at a broader community level.

A successful walkable neighborhood typically has a range of community destinations within a 5-10 minute walk.

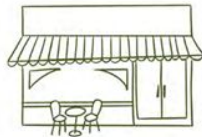
References:

- <https://www.planning.vic.gov.au/guides-and-resources/strategies-and-initiatives/20-minute-neighborhoods>
- Western Australian Planning Commission, Livable Neighbourhoods, 2015

Characteristics of a Walkable Neighbourhood:



Safe and accessible



Strong local economy



Design for climate



Destinations and community services



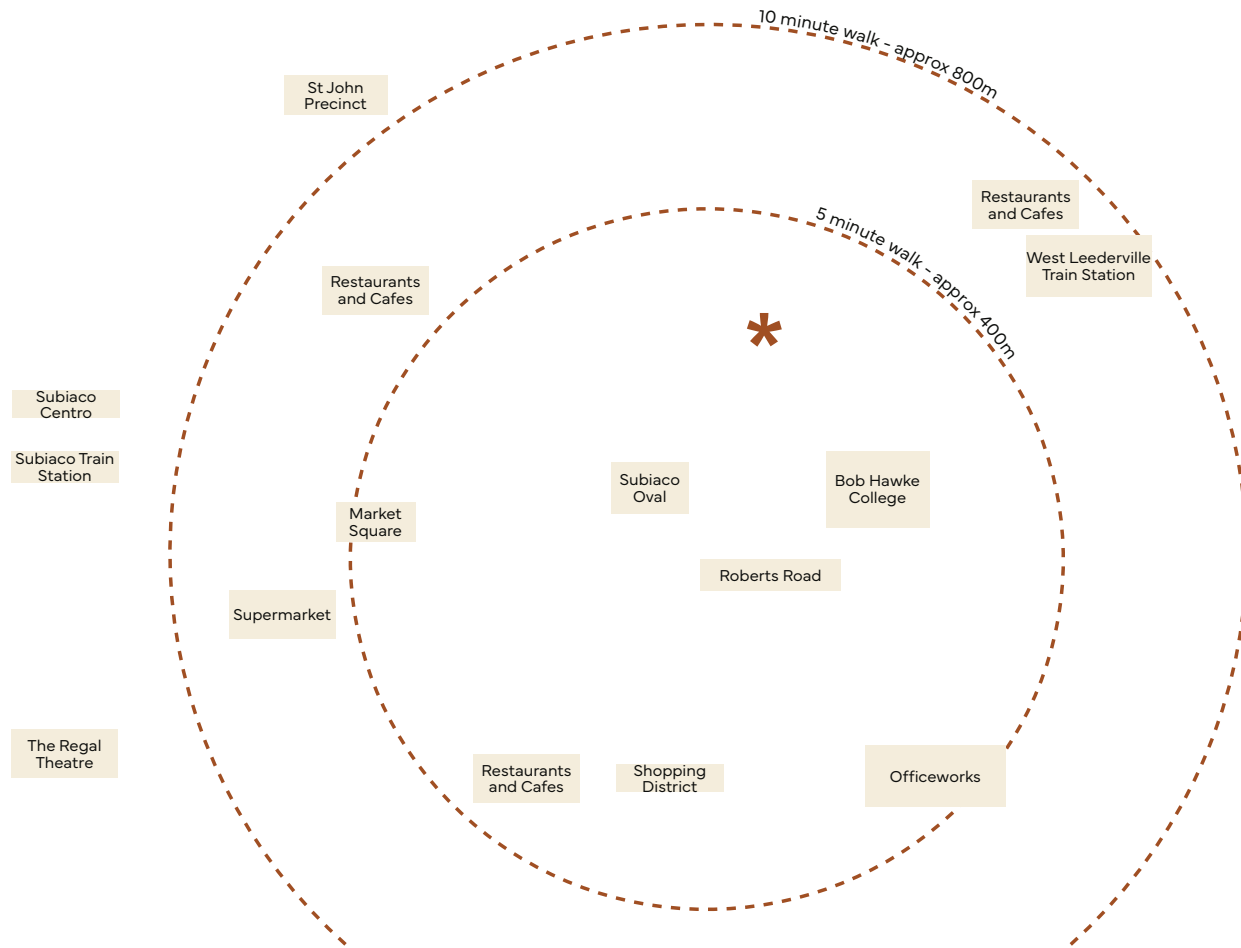
Public realm



Increased housing density

Neighborhood Walkability

This diagram outlines the location of the site in its broader context and walkability. The diagram outlines a 400m radius (5 minute walk) and 800m radius (10 minute walk) from the site.



CONCEPT DRIVERS

Noongar Boodja

Project Site Country: Whadjuk Noongar Boodja

The Noongar people of the Swan River and Swan Coastal Plains inhabited the area now known as the City of Subiaco before and during the early days of European settlement. They are the traditional landowners, keepers of knowledge, custodians and carers of the land and waterways. (1)

The Six Seasons Bidi Trail celebrates the Noongar seasonal calendar, weaving through the Subi East Redevelopment and connecting people to culture, knowledge, and boodjar (land).

Encouraging self-guided learning of Whadjuk Noongar cultural knowledge (katidjin), the trail integrates landscape design and public art to convey the importance of knowledge-sharing through the life cycle of the Noongar six seasons. (2)

Source:

1. <https://www.subiaco.wa.gov.au/subiacowebiste/media/media/Communications%20and%20Engagement/City-of-Subiaco-Info-sheet-History-of-Subiaco.pdf>

2. Subi East Elders Group and Karrda. <https://developmentwa.com.au/documents/1737-noongar-six-seasons-bidi-trail-flyer/viewdocument/1737>



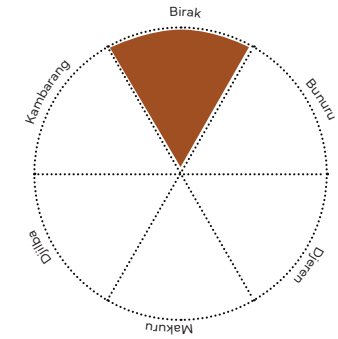
Birak → Bunuru

The site is part of the Noongar six seasons Bidi trail that winds its way through the Subi East precinct connecting people to culture, knowledge and boodja (land). The development sits within the Birak and Bunuru seasons, which will be referenced within the landscape design.

Birak

*First Summer - Season of the young
December - January*

Burning time of year for fuel reduction,
increase grazing land for animals, seed
germination and ease of travel across country



Baby frogs will be transforming into adulthood



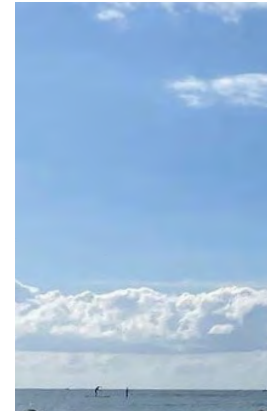
Yellow - orange flowers will start to bloom, including the striking Nuytsia (WA native Christmas Tree)



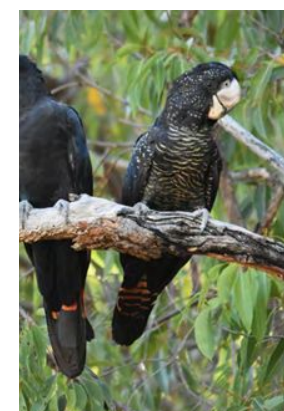
Reptiles start shedding their skin



Birak is burning time of the year. The Noongar people would burn the country in mosaic patterns.



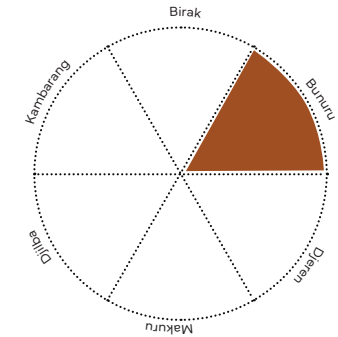
Hot and sunny, hot morning easterlies and cool afternoon sea breeze



Fledglings leave their nests

Bunuru

Second Summer - Season of the adolescence
February - March



Banksias such as the Acorn Banksia in flower



Acacias in flower



White flowering trees: Marri, Jarrah, Ghost Gums



Diet was mainly fish, crab and mussels. Tailor and mullet became easier to catch as they were found in shallower waters. Marron and gilgies caught in waterways



Hottest time of year with hot morning easterlies and cool afternoon sea breeze



The female *Macrozamia riedlei* in flower. The cone contains a cotton like substance called *kundy!* which was used for fire tinder or for hygienic purposes.

Thematic landscape planning

West Towers —→ Bunuru

Second Summer - Season of the adolescence



Feature macrozamia plantings and key driver in forms within the landscape



Expressions of the coast and water movement in the forms



Bunuru flowering plants throughout the landscape

East Towers —→ Birak

First Summer - Season of the young



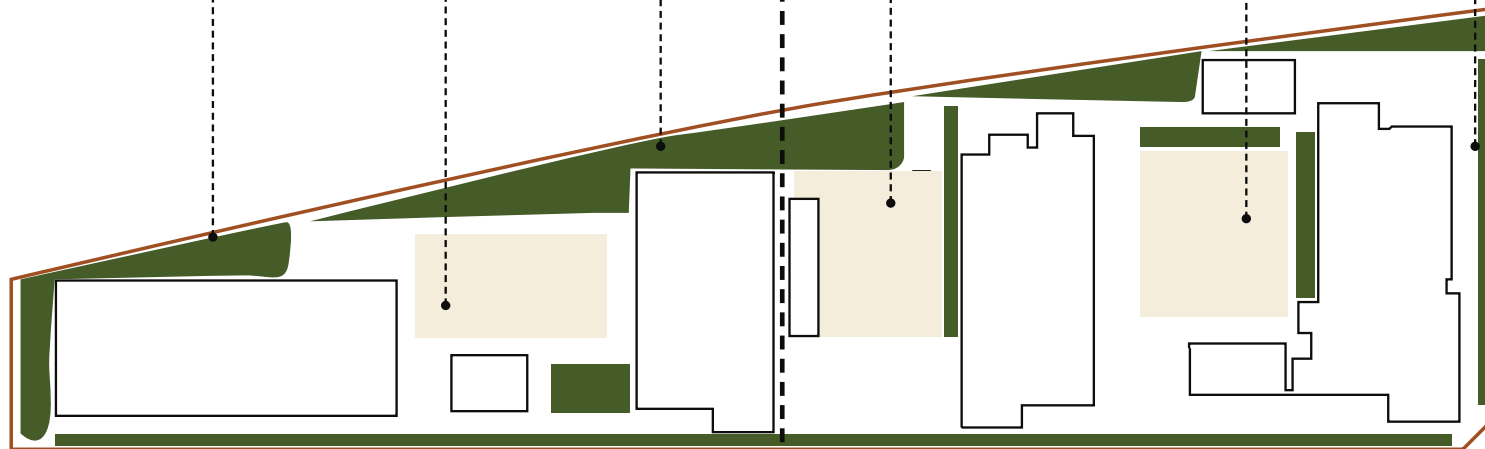
Reptile skin reference in paving



Mosaic burning pattern inspiration. Key driver in forms in the landscape



Patterns inspired by feathers incorporated into details



Landscape Zoning Diagram

Level 02

Zone 1

Zone 1 combines a children's playspace with resident amenities, including a covered shade structure, picnic tables, and barbeque's. These spaces have been designed to be the active pulse of each podium.

Zone 2

This zone consists of a series of circular planters for herbs, vegetable growing, and citrus trees. Small in scale, these areas allow residents to grow produce and use in the resident amenity areas whilst they cook meals.

Zone 3

These areas are strategically positioned across the podiums to provide pleasant gathering spaces for individuals or smaller groups. Consisting of arbors, loose and fixed furniture, and softening through

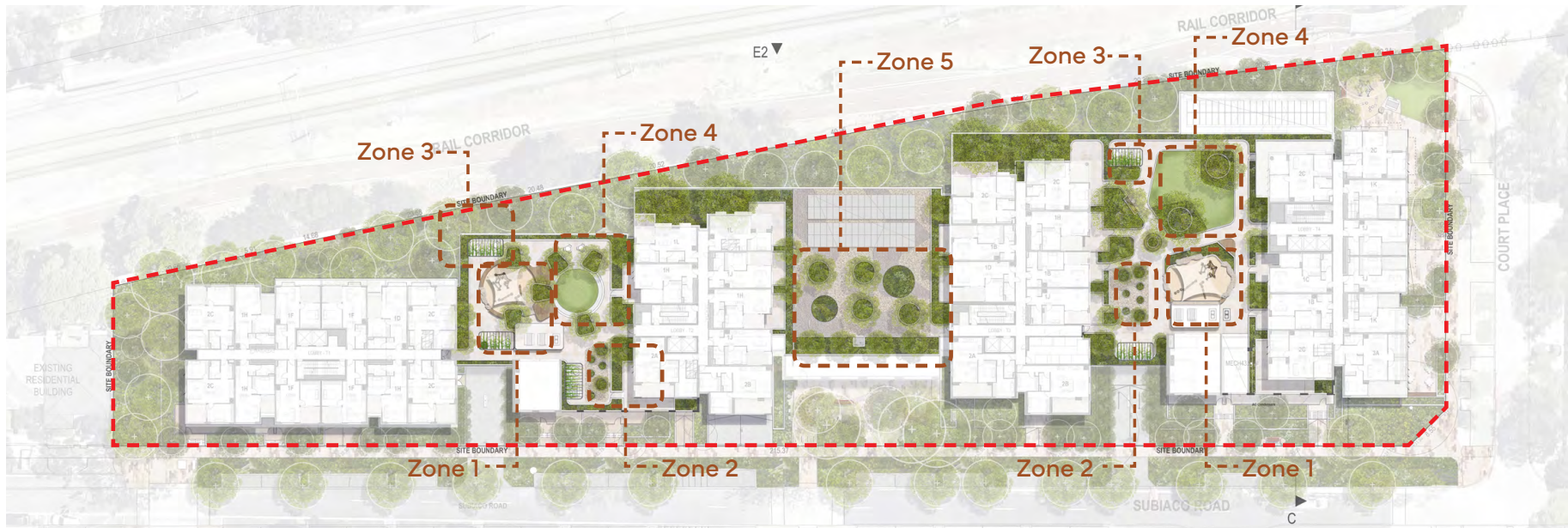
planting beds, these zones offer residents the opportunity to engage in the gardens in a peaceful manner.

Zone 4

Zone 4 provides an elevated grass area designed for flexibility and is connected to the children's play space. These grass areas have been strategically located near the play spaces to extend the active zones on the podium.

Zone 5

A series of simple circular planters with small trees in gravel have been positioned to reduce visual impact from residents looking down onto the space, and to reduce the impact of urban heat island effect.



Landscape Zoning Diagram

Level 02

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Zone 1 combines a children's playspace with resident amenities, including a covered shade structure, picnic tables, and barbeque's. These spaces have been designed to be the active pulse of each podium.

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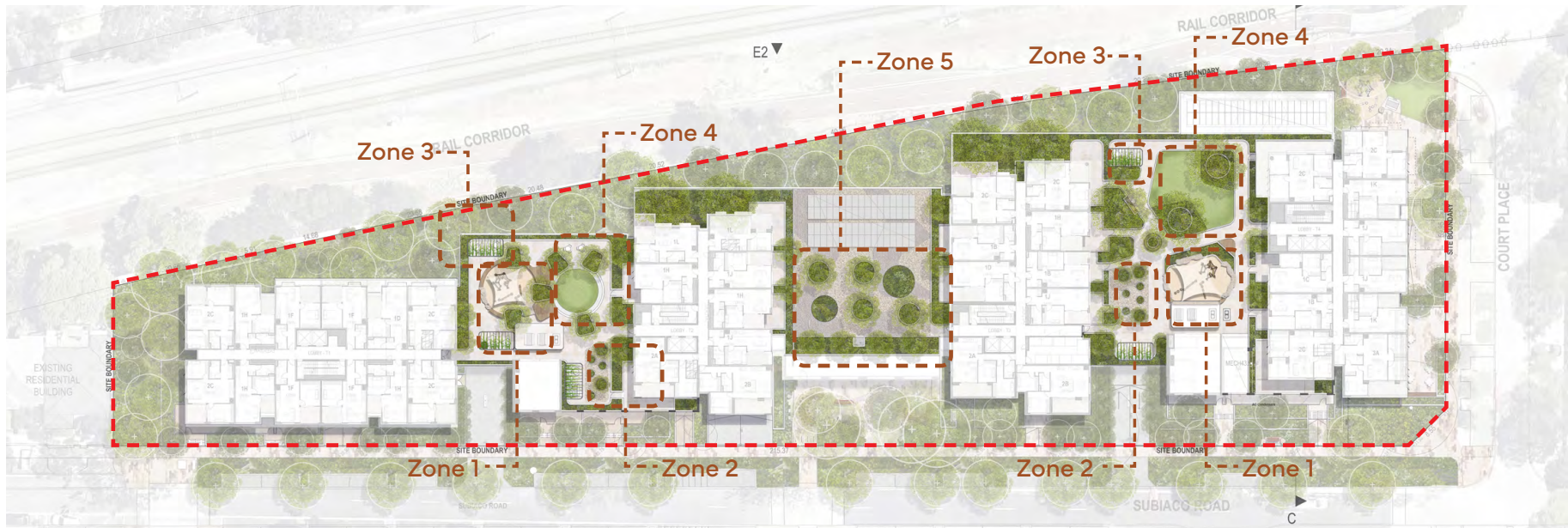
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Landscape Design Principles

Design for People

A community first approach.

The diagrams below outline how the landscape design facilitates positive urban living outcomes for residents and visitors to the site.



Welcoming, inclusive and accessible spaces.



Spaces for social connection and gathering.



Well connected and walkable spaces to create a healthy, active landscape.



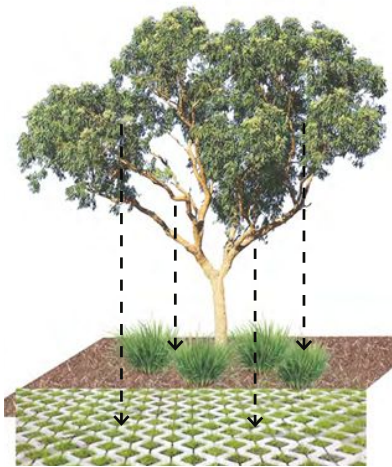
People first design. Minimise the impact of vehicles and parking.

Landscape Design Principles

Design for the Environment

Positive environmental contributions

The diagrams below outline the intent for the landscape to create positive environmental ecological outcomes, further enhancing the development as a desirable place to live and work.



Permeability and enhanced water filtration.



Create habitat and improve ecology.



Healthy trees with generous deep



Use of robust, sustainable materials.

DRP Comments



Key Comments:

Following a positive response to DRP#3, the panel identified three key areas for development of the landscape design and public realm.

Subiaco Road Forecourt

1. 'Kit of Parts' comprising the landscape design are appropriate but consider realigning to make the forecourt more usable and welcoming to the public.
2. Consider how the forecourt addresses the street and assists with street front activation.
3. Consider wayfinding to CHL office/front door

Subiaco Road + Court Place corner

1. Consider how the public realm can interface with Subiaco Road in a more positive way, and help with activating the corner
2. Consider articulating the space with overhead canopies/arbors
3. Consider creation of 'outdoor rooms' within the public realm that provide additional amenity to residents and visitors to the site

Court Place Micro Park

1. Consider interface with building and accessibility.
2. Consider the opportunity to continue overhead canopies/arbors to north side of building to create a consistent aesthetic along Court Place, and make more usable/comfortable

CONCEPT DESIGN

Ground Floor Landscape Plan



Subiaco Road Forecourt Detail Plan

LEGEND

- 1. Deep soil planting zone
- 2. Bicycle rack location
- 3. CHL outdoor forecourt
- 4. Community seat/table
- 5. Shade structure
- 6. Existing verge



Location Plan

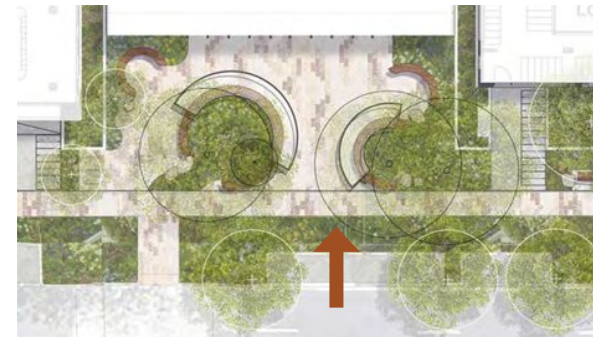


DRP#3 Plan



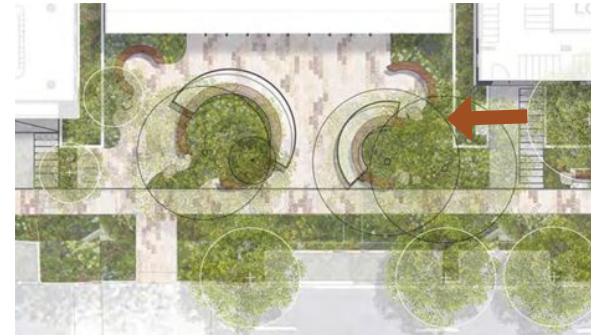
Revised Plan

Subiaco Road Forecourt Renders



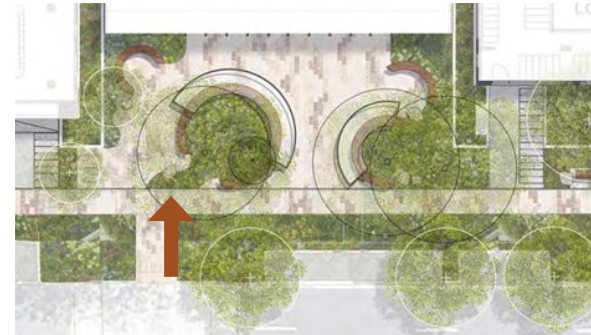
Location Plan

Subiaco Road Forecourt Renders



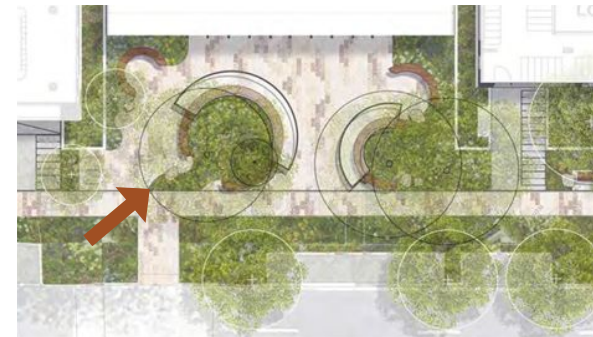
Location Plan

Subiaco Road Forecourt Renders



Location Plan

Subiaco Road Forecourt Renders



Location Plan

Subiaco Road + Court Place Detail Plan

LEGEND

- 1. Deep soil planting zone
- 2. Arbor
- 3. Outdoor decking rooms with built in seating
- 4. Timber bleachers
- 5. Alfresco area
- 6. Existing verge



Location Plan

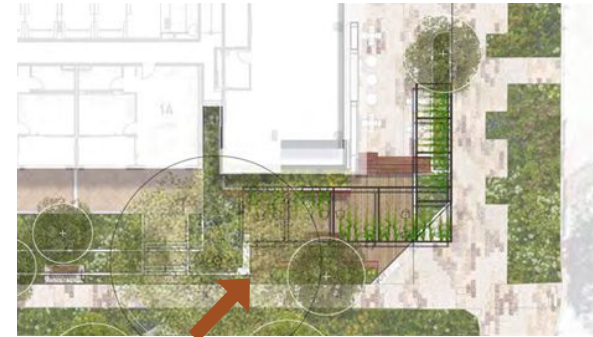


DRP#3 Plan



Revised Plan

Subiaco Road + Court Place Renders



Location Plan

Subiaco Road + Court Place Renders



See Design Studio

154 Subiaco Road



Location Plan

Subiaco Road Forecourt Renders

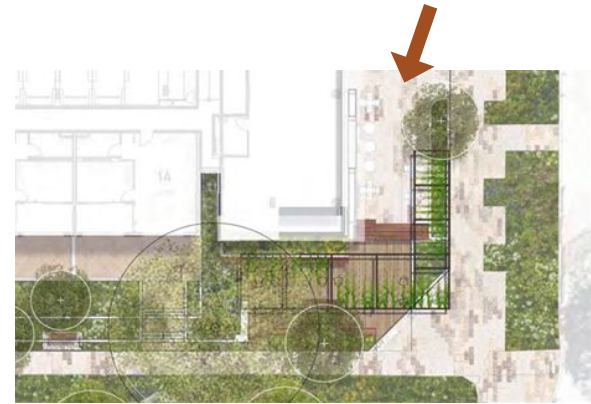


Location Plan



See Design Studio

154 Subiaco Road



Location Plan

Court Place Pocket Park Detail Plan

LEGEND

- 1. Deep soil planting zone
- 2. Lawn
- 3. Bicycle rack location
- 4. Alfresco dining area + arbor
- 5. Nature play pocket park with shade
- 6. Bench



Location Plan



DRP#3 Plan



Revised Plan



Level 02 Detail Plan 01

LEGEND

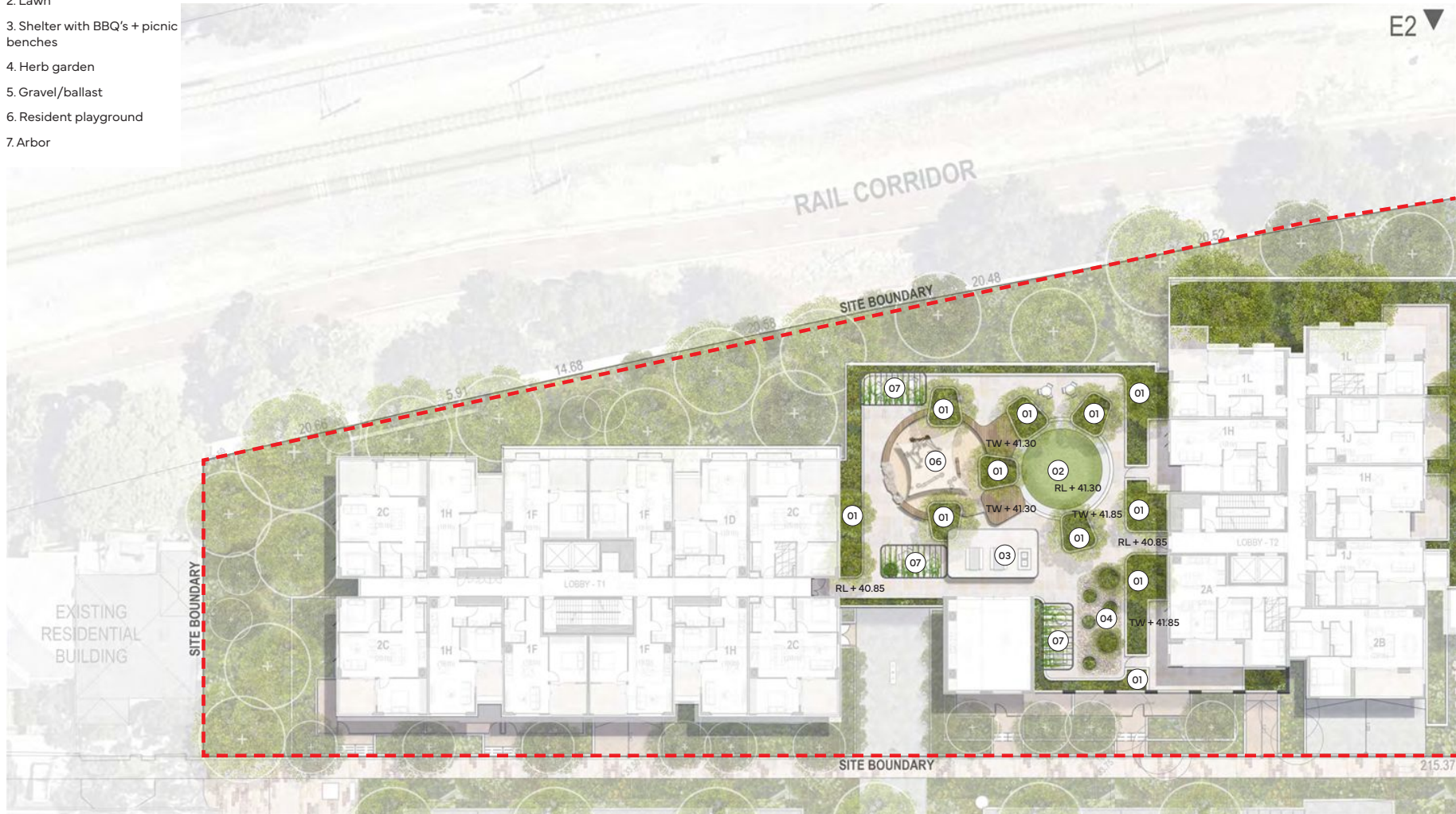
1. Raised planting bed
2. Lawn
3. Shelter with BBQ's + picnic benches
4. Herb garden
5. Gravel/ballast
6. Resident playground
7. Arbor



Level 02 Detail Plan 02

LEGEND

- 1. Raised planting bed
- 2. Lawn
- 3. Shelter with BBQ's + picnic benches
- 4. Herb garden
- 5. Gravel/ballast
- 6. Resident playground
- 7. Arbor



1:200 @ A3



Look and Feel

The plant selections on the level 2 podium will focus on climate appropriate species that will create verdant, welcoming garden for the residents.

A community productive garden will provide a valuable space for connection, learning, and wellbeing within the community.

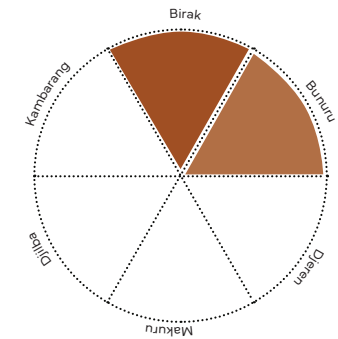


SELECTIONS

Planting Design

The plant palette comprises exclusively Australian native species, with priority given to locally endemic varieties that are inherently adapted to Perth's climate. All selections are water-wise and climate-resilient, requiring minimal maintenance once established.

The diverse mix of species has been carefully curated to provide essential habitat for native birds and beneficial insects, strengthening local ecological networks. To maintain clear sight lines and enable effective passive surveillance throughout the site, all species have been selected for their naturally low-growing habit, reaching a maximum height of one meter. Seasonal flowers will celebrate Bunuru and Birak. This palette achieves a balance between environmental sustainability, biodiversity enhancement, and practical urban design considerations.



Diverse species for habitat



Native and endemic



Celebrate Bunuru and Birak seasons



Robust and low maintenance

Plant Palettes

Low native shrubs



Acacia saligna 'prostrate'



banksia nivea



Callistemon viminalis Little John



Calothamnus quadrifidus prostrate



Westringia Mundi



Casuarina 'Green Wave'



Eremophila glabra Kalbarri Carpet



Grevillea Gingin Gem



Hibbertia Ocean Reef



Leucophyta brownii



Hardenbergia comptoniana



Myoporum parvifolium



Chamelaucium uncinatum 'Beach'



Grevillea olivacea red



Olearia Little Smokie



Westringia 'Low Horizon'



Adenanthos cuneata

Medium native shrubs



Grevillea olivacea red



Adenanthos sericea select



Chamelaucium Seabird White

See Design Studio

154 Subiaco Road

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Strappy leaf plants



Lomandra longifolia 'Nyalla'



Lepidosperma gladiatum



Dianella 'Cassa Blue'



Dianella Little Jess



Dianella streetscape



Liriope Just Right'



Conostylis candicans



Lomandra longifolia 'Verday'

Birak Seasonal Colour



Dianella sp.



Callistemon 'Little John'



Agonis flexuosa



Anigozanthos flavidus



Banksia grandis 'dwarf'

Bunuru Seasonal Colour



Macrozamia riedlei



Corymbia calophylla



Eucalyptus marginata



Eucalyptus gomphocephala



Banksia prionotes dwarf



Hemiandra pungens alba

Tree Species

Large Trees



Corymbia calophylla



Eucalyptus marginata



Eucalyptus gomphocephala

Medium Trees



Agonis flexuosa



Aucalyptus erthyrocorys

Small Trees



Melaleuca raphiophylla
Size: 8m H x 4m W

See Design Studio



Eucalyptus foecunda
Size: 6m H x 4m W



Eucalyptus victrix
Size: 8m H x 4m W

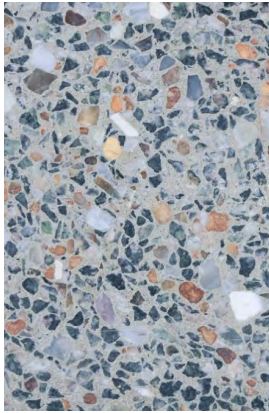


Callistomen Kings Park Special
Size: 4m H x 4m W

154 Subiaco Road

Material Selections_ Ground Floor

Surface Treatments



Material: Hydropave engineered concrete permeable paver
Colour: Casino Grey
Finish: Milled
Unit size: 300 x 200 x 70mm
Supplier: Urbanstone



Material: Hydropave engineered concrete permeable paver
Colour: River Topaz
Finish: Shot Blast
Unit size: 300 x 200 x 70mm
Supplier: Urbanstone

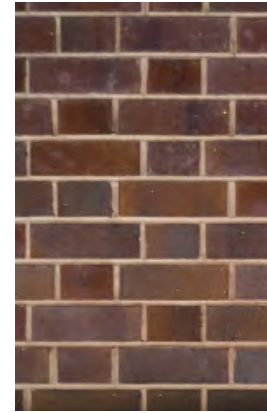


Material: Hydropave engineered concrete permeable paver
Colour: Fleece
Finish: Shot Blast
Unit size: 300 x 200 x 70mm
Supplier: Urbanstone

Wall Treatments



Material: Rendered masonry wall
Colour: Off white



Material: Brick
Colour: Subiaco Red
Unit size: 230 x 110 x 76mm
Supplier: Midland Brick

Material Selections_ Level 02

Surface Treatments



Material: Porcelain paver
Colour: Durango Acero
Unit size: 596 x 596 x 20mm
Supplier: Earp Bros



Material: Timber look battens
Colour: Coppered Oak
Unit size: 126 x 32mm
Supplier: Millboard

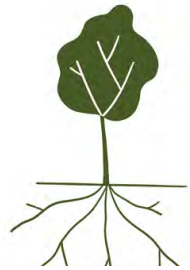
Wall Treatments



Material: Rendered masonry wall
Colour: Off white

TECHNICAL INFORMATION

Deep Soil Zone Benefits



Healthy roots = healthy tree



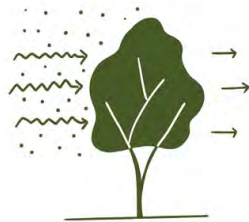
Enhance water filtration



Comfortable Micro-Climate +
Reduction of Urban Heat Island
Effect



Create Habitat



Improve Air Quality



Community Health
through increased canopy
coverage



Trees and gardens make a significant contribution to the ecology, character and amenity of neighbourhoods. They provide habitat for fauna, shade, storm water management and micro-climate benefits, as well as improve apartment outlook and privacy.

The provision of deep soil areas to support and sustain the development of tree canopy can also make a major contribution to the retention of existing trees. A deep soil area is an area of soil that is free of built structure and has sufficient area and depth to support tree growth and infiltrate rainwater. Site planning should seek to co-locate deep soil areas with existing trees on and adjacent to the site, and in locations best suited to the development of a viable tree canopy and landscaping.

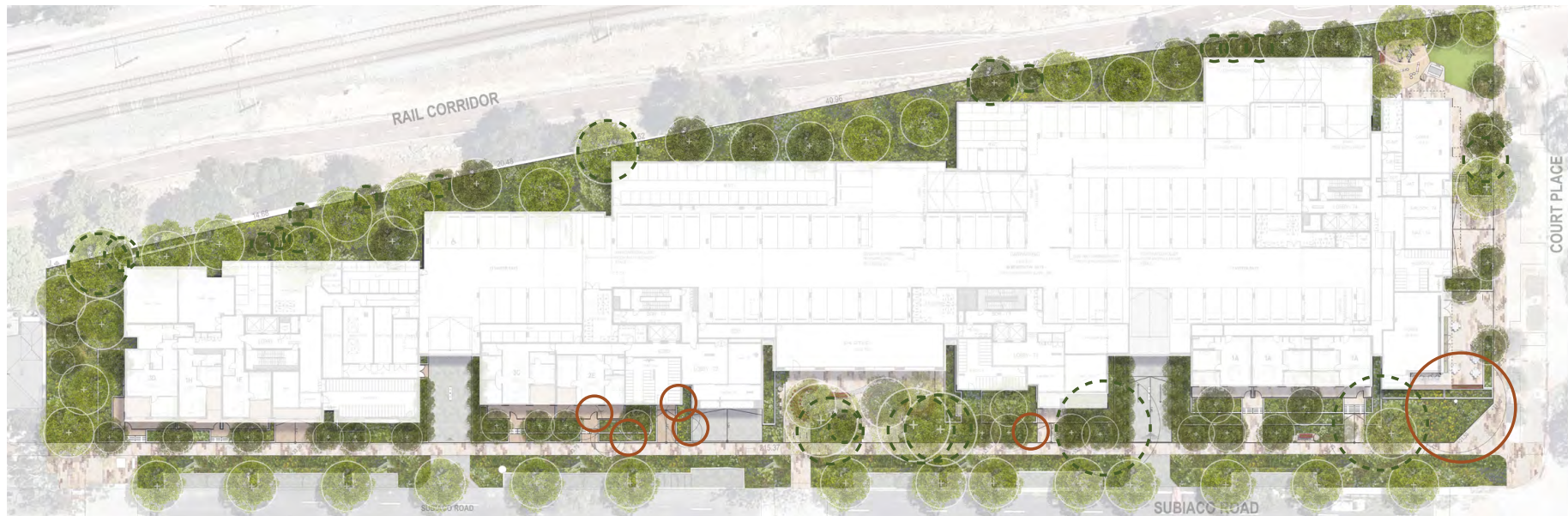
Tree retention plan

Summary

Existing trees	Total
Trees to be retained	21
Trees to be removed	6

 Tree Retention

 Tree Removal



Deep Soil Requirements

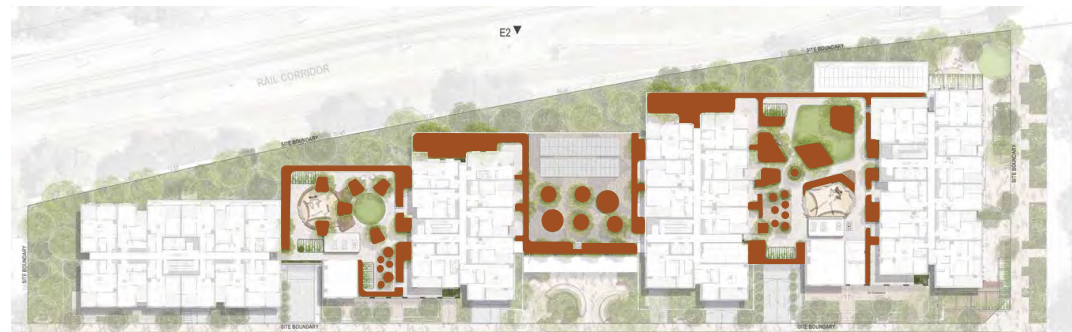
Summary

DEEP SOIL REQUIREMENTS	AREA
Total Site Area	10,786 sqm
Min site wide deep soil zone required	1,078.6 sqm (10%)
Ground floor deep soil area	2,365sqm (21.9%)
Level 2 Podium planting (min 1m wide x 1m deep)	825 sqm (7.6%)
Deep soil and planting on podium total	3,190sqm (or 29.5 %)

- Deep Soil Planting
- Planting on Podium



Ground Floor



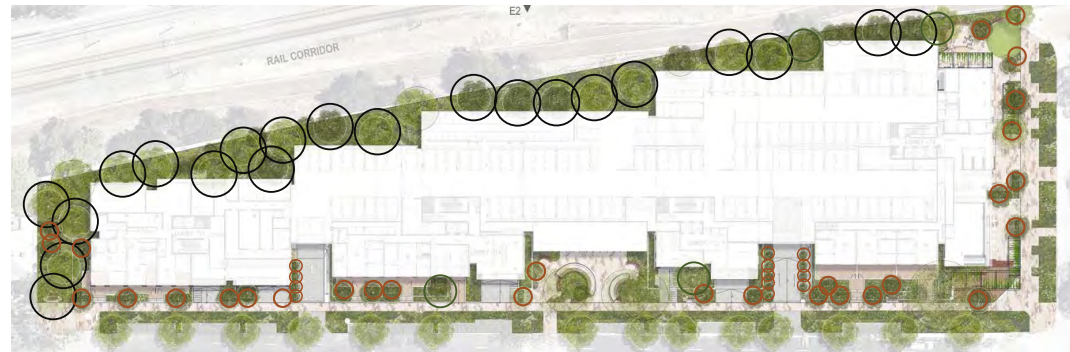
Level 02

Tree Requirements

NOTE:
All trees indicated meet the minimum deep soil zone requirement as per State Planning Policy 7.3 Residential Design Codes - Apartments

Summary

Tree Requirements	Area
Total Site Area	10,786 sqm
Site wide tree requirements (700 to 1,000m2)	1 large tree and small trees to suit
Additional tree requirements (based on 10,081 m2)	10 large trees and small trees to suit
TOTAL TREE NUMBERS	Large trees = 22 total
	Medium trees = 9 total
	Small trees = 81 total



Ground Floor

- 'Small Tree' location
- 'Medium Tree' location
- 'Large Tree' location



Level 02

See Design Studio
Landscape Architects

Please feel free to contact us with any inquiries.

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January 2026

Court Place Subiaco

Sustainable Design Assessment Report

Graham Agar
FULL CIRCLE DESIGN SERVICES

COURT PLACE SUBIACO | Sustainable Design Assessment Report



Full Circle Design Services	
Telephone: 0412 475 819	Report N°: 2025_066_ESD_REP_02_SDAR
E-mail: graham.agar@fcds.com.au	Original Date of Issue: 6 th January 2026
ACN: 163 742 890	
ABN: 84 163 742 890	

Title:	Court Place Subiaco – Sustainable Design Assessment Report
Author :	Graham Agar
Client:	Corbel Property / CHL
Contact:	David Klein
Description:	<p>This report provides a brief summary of the proposed sustainable design strategy for the proposed new multi-unit residential development on Court Place in Subiaco.</p> <p>The report outlines overall intent and sustainable design features to be included within the design, as well as an overall assessment of the expected outcome for the project.</p> <p>Rev A is an initial draft for coordination and comment by the design team.</p>

Revision	Date	Checked by	Transmitted by
A	6 th January 2026		GEA

Distribution	Revision								
Receiver	A								
CHL									
Corbel Property	X								
DKO	X								
Consultant Team	X								
Builder									



Executive Summary

FCDS have been commissioned to provide general sustainable design advice and briefing services for the proposed new affordable and socially responsible residential development for CHL on Court Place in Subiaco.

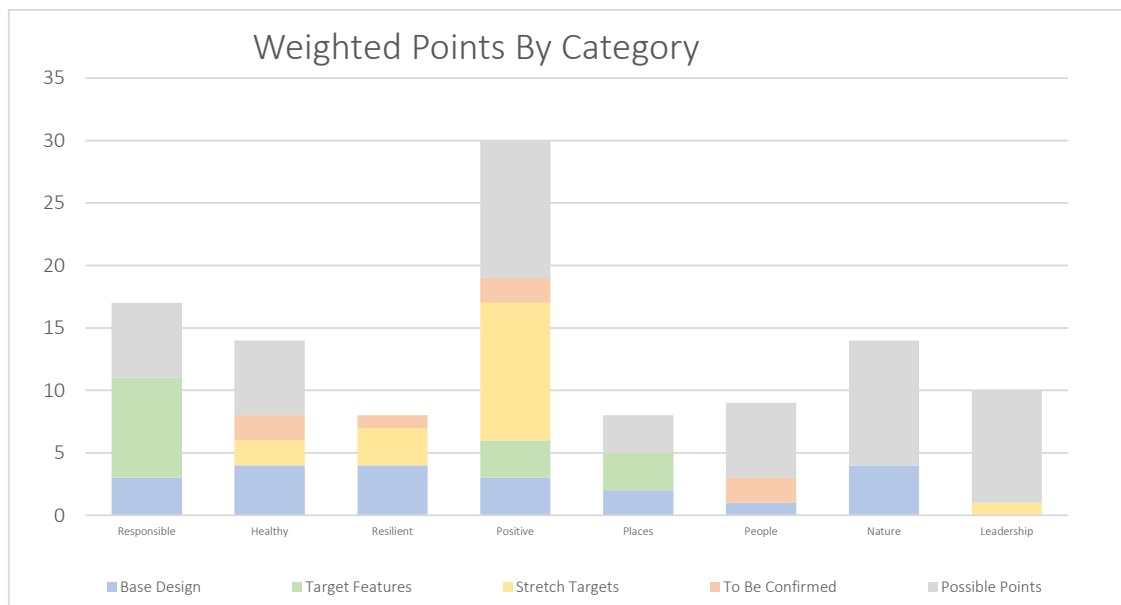
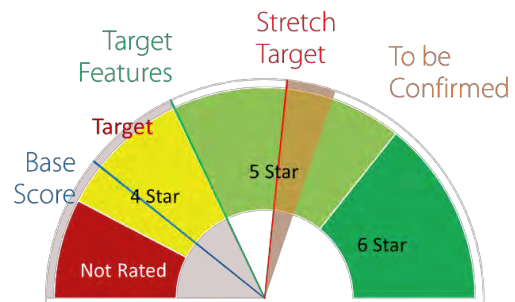
At completing, the project will have around 450 dwellings (447 currently), serving to provide accommodation for around 620 occupants. The built form will include four separate towers, stemming from commercial tenancies and car parking and common / amenity areas on a podium.

The project is committed to an outstanding sustainable design outcome and has elected to demonstrate this commitment through achieving the intent of at least 35 credits under the Green Star Buildings V1b rating system.

Whilst local planning controls require sustainable design excellence for a portion of the site, the project team are targeting the same outcome for all four residential towers.

Sustainable design performance will be self-assessed as formal certification is considered a poor cost/benefit outcome for the project and self-assessment is in alignment with other, recently approved developments in the area.

FCDS have reviewed the existing documentation and, in conjunction with the developer and project team, have identified 35 Green Star Buildings design credits which we believe can be achieved within the project planning with a further 7 pending ongoing design development to confirm:



The project, when complete, will be run by CHL (Community Housing Limited), with the project strategic requirements including; Low operational costs and carbon emissions, High durability; Simplicity of operation and maintenance and Occupant Health and wellbeing.

Further information with respect Green Star Buildings approach is available in the attached scorecard.



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1. Introduction

FCDS have been commissioned to provide general sustainable design advice and briefing services for the proposed new affordable and socially responsible residential development for CHL on Court Place in Subiaco.

At completing, the project will have around 450 dwellings (447 currently), serving to provide accommodation for around 620 occupants. The built form will include four separate towers, stemming from commercial tenancies and car parking and common / amenity areas on a podium.

The project is committed to an outstanding sustainable design outcome and has elected to demonstrate this commitment through achieving the intent of at least 35 credits under the Green Star Buildings V1b rating system.

1.1 Site Description

The proposed project is a residential development in Subiaco, comprising some 447 social and affordable housing apartments to be operated by CHL as a Build to Rent scheme.

The design is targeting high durability, minimising carbon emissions and keeping operational and rental costs as low as possible.

The project includes four towers extending from a podium level including internal and external amenity spaces, with CHL offices and commercial tenancies at street level.



1.2 Sustainability Targets

The project is targeting 'Australian Excellence' in Sustainable Design, as measured by a 5-Star self-assessed outcome. This equates to compliance with all minimum requirements as well as thirty five (35) optimisations or credits.

Category	Target	Initial Targets
Energy Consumption	15% Improvement over BCA – Envelope	<ul style="list-style-type: none"> - Roof > R6.0 Solar Absorbance < 0.35 - Walls – R2.2 batt insulation + Thermal break - Glass – Low-E Double Glazing - Floors – R1.9 Board insulation to any exposed floors
	30% Improvement over BCA – Overall	<ul style="list-style-type: none"> - All electric building - 40% reduction in lighting power density - 35% improvement in minimum efficiency for HVAC - CoP > 3.5 – All domestic hot water systems - Energy efficient appliances
	Façade leakage < 10m ³ /hr @ 50Pa	Design will include detailing to control air leakage and commissioning will include building pressurisation tests. Pressure Testing ~20 Apartments each construction phase
	Renewable Energy	The project should include a large solar photovoltaic array, comprising ~200kW of panels and inverters, to produce at least 350MWhrs of electricity per annum.
Water Consumption	Low Flow Tapware	Utilise low flow tapware throughout building.
	Irrigation	Utilise low water use planting and water efficient irrigation on site.
	Stormwater Management	Minimise stormwater pollutant outflow via infiltration
Waste Targets	75%+ Recycling in operation	Design to facilitate capture of recyclable goods and use of comingled recycling. Facilitate collection through nominated store with easy bin access.
	>90% Recycling in construction	Careful consideration of construction materials to avoid waste. Use of high efficiency resource recovery facility to sort waste in construction.
Material Selections	Ethical Certification	Prioritise materials which are: <ul style="list-style-type: none"> - Locally sourced - Ethically sourced - Renewable - Certified – GECA / FSC / Green Tag - Recycled – preferably from on site. - Durable – 10 year+ Design Life for finishes
	Embodied Carbon	Select natural material for finishes – such as timber. Minimise steel, cement and aluminium
Durability	>10 Years	Internal finishes and flooring shall target >10 year life spans, with minimal repair and maintenance rather than regular replacement.
Indoor Environment Quality	CO ₂ Levels < 800ppm	Through increased outside air rates or provision of CO ₂ control, internal CO ₂ concentrations should be maintained at low levels. Tenancies to be provided with louvre and electrical connections to cater for increased Outside Air rates.
	Low VOC's	TVOC < 0.27ppm Formaldehyde < 0.02ppm
General Sustainability	Green Star	35 Optimisation Credits
	Energy Use	Energy Use <4MWhrs p.a. per apartment
	Water Use	Water Use <250kL/m ² p.a. per apartment

During design phase, the project is aiming to undertake a self-assessment only, using the Green Star Buildings tool as a benchmark. The SDAR will be issued, along with a scorecard, justifying credits claimed, with supporting evidence as noted above.



2. Responsible Design Features

The Responsible category refers to credits which are intended to minimise ecological footprint by control of the design, construction and commissioning process. The features also include elements to optimise operational performance through design of effective spaces and measuring consumption.

The following section outlines FCDS expectation with respect the Responsible design elements intended to be included by the project team:

2.1 Marketing Excellence

The design team will produce documentation to describe sustainable design features of the development for key stakeholders – including prospective occupants of each dwelling and the commercial tenancies.

2.2 Environmentally Responsible Construction

The main contractor will be expected to implement an environmental management system, using ISO 14001 practices to monitor its implementation on site.

The plan will include waste management and minimisation, targeting a minimum of 80% of construction and demolition waste diversion from landfill.

Contractors visiting site for more than 3 days will be required to undertake site familiarisation and sustainable design training covering design features for this development, as well as a wider overview of sustainability issues.

2.3 Commissioning, Verification and Handover

The design team have been provided clear design targets for environmental performance – refer to Section 1.2 above.

FCDS have been appointed to undertake the Independent Commissioning Agent role and will assist in documenting the project's approach to the Soft Landings framework.

Commissioning will be in accordance with best practice international standards, including CIBSE, ASHRA and Airah. The building envelope will be tested for air leakage prior to practical completion.

Common area services will be closely monitored over the first 12 months of operation to minimise performance issues and optimise operational efficiency against design targets. This tuning phase will be part of the Performance period.

The designers will include meters for each dwelling and tenancy, utilising utility meter reading systems to monitor and report on performance. Occupants will be able to review their performance and, hopefully, be encouraged to reduce their footprint.

Detailed handover documentation will be provided to building stakeholders in electronic format, including As Built drawings and functional control descriptions.



2.4 Operational Waste

Tallis have been appointed to the design team to ensure be ensuring that the project is effectively designed to manage operational waste across metrics including sizing, segregation of waste streams and accessibility for waste contractors.

The design includes separate commercial and residential stores at ground level in close proximity to the lifts and stairs, as well as with back of house access for tenancies.

There is a dedicated pathway from the store to the bin collection area, with excellent access for collection vehicles.

Space has been allowed for two waste chutes (general waste and mixed recyclables) as well as separate FOGO (Food Organics Garden Organics) to reduce landfill.



2.5 Sustainable Design Professional

The project team have included sustainable design considerations from schematic design phase and will continue through to practical completion and beyond.

2.6 Waste Minimisation

Beyond the minimum requirement to divert 80% of construction and demolition waste, the contractor will be expected to control materials brought to site and the waste disposal process to achieve a net 90% diversion of all construction and demolition waste, by mass, by practical completion.





2.7 Responsible Products

This category review products selected for building structure, envelope, systems and finishes and provides points for projects which can demonstrate performance improvement over standard practice.

Key elements supporting these features are:

- Design and Construction procurement processes to align with ISO 20400 – Sustainable Procurement Guidance
- Mechanical, electrical and hydraulic systems are to include at least 20% (by cost) achieving a Responsible Products Value, greater than 6, as per the table below:
- Finishes to follow similar sustainable requirements, with a net score of at least 9 for more than 20% of products

Metric	Score
• Industry specific environmental product declarations (EPD)	2 Points
• Product specific environmental product declarations (EPD)	4 Points
• ISO14001 certification	3 Points
• Reused Product	15 Points
• FSC Certified	10 Points
• Best Practice PVC certification	5 Points

Based on our understanding of the project and recent experience in this space, FCDS expect the project team will deliver sustainable material selections in building structure and finishes. Final compliance will be reviewed as finishes selections are finalised and verified during construction.

3. Healthy Design Features

The Healthy credit category is about ensuring the building provides a strong response to occupant health and wellbeing. Features supporting air quality, views, access to light and noise contribute to point scoring within this category:

3.1 Ventilation System Attributes

Outside air and natural ventilation systems are to comply with the prescriptive requirements of AS 1668.2 (mechanical ventilation) and AS 1668.4 (natural ventilation) for air quantity, intake location and exhaust separation.

Local exhaust systems are to be provided to isolate occupied spaces from contamination such as kitchens and toilets.

GBCA compliance for residences can be achieved with AS 1668.4 compliant openings, tenancy spaces will have louvre allocation and space to exceed BCA minimums for outside air provision.

3.2 Lighting Systems

Lighting systems are to be flicker free and provide a minimum Colour Rendering Index (CRI) >85 with a maximum of 3 MacAdam Ellipses.

The design will meet best practice illuminance levels for each task within each space type with a maintained illuminance values must achieve a uniformity of no less than that specified in Table 3.2 of AS/NZS 1680.1:2006



3.3 Glare Control

External shading systems will provide some coverage from direct solar penetration, however, internal areas will be provided with blinds and window treatments to eliminate direct solar penetration.

Lighting systems are to be provided with diffusers or other design features which maintain direct glare from the luminaries below the UGR (Unified Glare Rating) limit within AS 1680.1

3.4 Access to Daylight

All occupants have excellent access to natural lighting through strong passive design (north/south facing windows with overhangs) and relatively shallow plans.

3.5 Noise Levels

The use of high-quality mechanical plant and good architectural detailing will result in comfortable internal noise levels, generally matching AS 2107.

The acoustic design will consider noise carry over between levels as a means to improve occupant amenity and privacy.

3.6 Connection to Nature, Amenity and Comfort

At least 60% (close to 100% for this project) of spaces are within 8m of a view to outside. The design also includes a large, landscaped area for occupant amenities on level 1.

FCDS recommend the project team target the use of natural finishes and motifs to further promote the connection of users to nature.

Occupants are provided with more than 1,800m² of external amenity with views to nature and the adjacent Subiaco Oval at level 1:

3.7 Low Toxicity Products

The design team are expected to select finishes and composite wood products with low Volatile Organic Compound (VOC) and low formaldehyde content. This includes joinery, carpets, adhesives and sealants. Wall and ceiling paints will target a level <5 g/L for VOC content.

Performance is to be verified at practical completion through air quality tests on selected typical apartments.

4. Resilient Design Features

The Resilient category of credits highlight the need for projects to be ready for the imminent impacts of climate change and to provide a level of support to the surrounding community. Shocks to power infrastructure, ongoing weather pattern adjustment and the urban heat island effect are considered within the category.

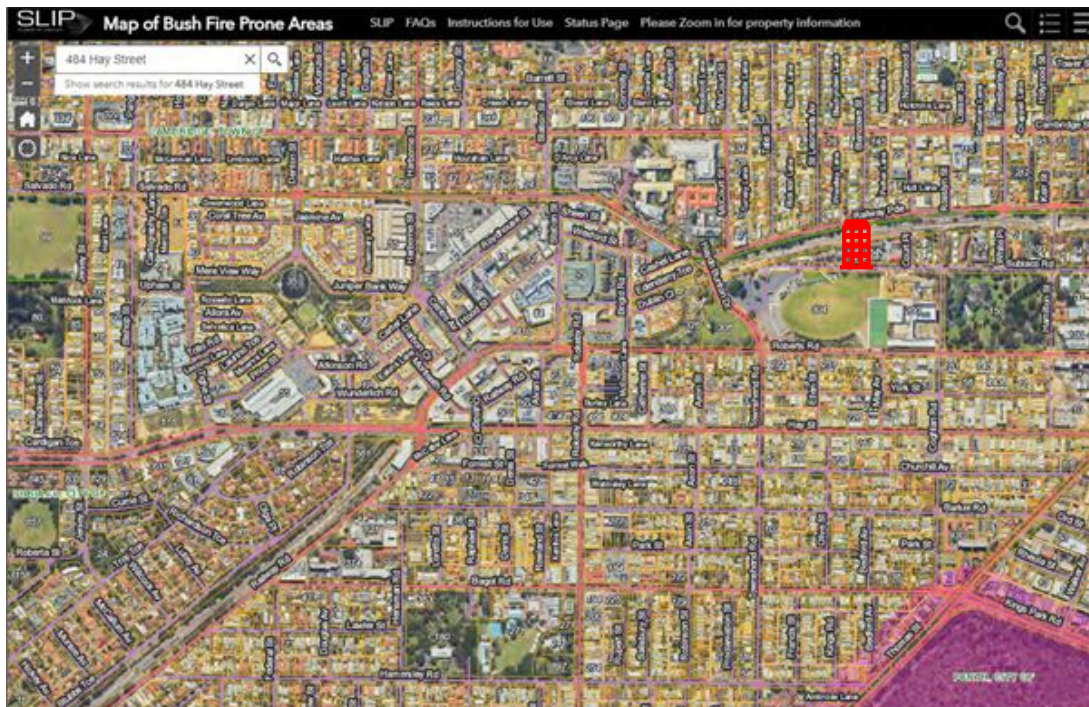
4.1 Climate Change Resilience

FCDS have undertaken a base review of the project and the potential likely impacts of climate change on the site, based on the following likely impacts of climate change in South West WA:

Variable	Current	Predicted	Expected Change	Possible Range
Annual Average Temperature (°C)	25.8°C	26.6°C	+0.8°C	26.4 – 27°C
Number of days over 35°C	28	35.3	+7.3	33.1-38.7
Annual Average Rainfall (mm)	835.1mm	785mm	-6%	726.5 – 827mm
Annual Average Potential Evaporation	1800mm	1836mm	+2%	1818-1872mm
Annual Relative Humidity	55%	54.4%	-0.6%	53.7 – 55%

The site is not at risk of floor, under even a 10m sea level rise.

Similarly there is no localised bushfire risk, with the closes 'prone' area at Kings Park to the south east:





4.2 Climate Change Risk Management

Following the base risk review above, the design team have included the following features to mitigate risks and provide an improved outcome for occupants and the local community:

Climate Change Impact	Risk	Proposed Response
Increased temperatures lead to increased bushfire risk and intensity.	Low due to current classification of site.	Consider provision of air filtration on air conditioning systems and dual aspect ventilation.
Rising sea levels and increased flood risk.	Very low.	Avoid critical construction below ground level. Ensure structure can adapt to changing water levels.
Reduced rainfall	Increasing requirement for irrigation, increased cost of scheme water.	Utilise smart irrigation, including moisture detection and prioritise drought tolerant planting.
Increased temperatures lead to increased reliance on air conditioning.	Building is unable to provide comfortable environment for extended periods.	Provide high efficiency air conditioning systems with automatic controls. Upgrade building envelope in excess of BCA minimum requirements.
Increased temperatures lead to increased power demand.	Operating cost increases as electrical prices increase. Power security becomes questionable.	Good control systems and energy efficient design. Installation of solar photovoltaics for renewable generation.

4.3 Operations Resilience

Whilst the project team have identified no significant risks in terms of operation under the loss of power, the design includes features which improve operational resilience under major shocks to electrical infrastructure including:

- Strong solar passive design
- Building envelope upgrade above minimum compliance
- Onsite renewable generation – consideration for future battery connection

The site embedded network is to be complete with an energy management dashboard, allowing management to monitor building demand and implement management strategies in future.

4.4 Grid Resilience:





The building will be designed to reduce its electricity peak demand by 10% of the annual peak electricity demand for at least a one-hour period through a variety of features, some of which may include:

- Improved building envelope
- High efficiency systems
- On site renewable generation.



4.5 Heat Resilience

The project will include the selection of light colours to external finishes as well as shaded area (from fixed building shading and planting) and landscape integrated into the site to minimise the impacts of urban heat island on the site. The following table shows the recommended colour palettes with respect colours and solar absorbance:

Usage	Initial SRI	3 Year SRI	Sample Product
Metal Roof > 15° Pitch	>34	>39	 Shale Grey ⁺ SA = 0.43 SRI = 66 Dunes ⁺ SA = 0.47 SRI = 67 Windsprayer ⁺ SA = 0.58 SRI = 48 Pale Eucalypt ⁺ SA = 0.60 SRI = 43
Hardscape			Bamboo (35-48)  Grey Limestone (54) 
Metal Roof < 15° Pitch	>82	>64	 Classic Cream ⁺ SA = 0.32 SRI = 82 Surfmeister ⁺ SA = 0.32 SRI = 82 White Concrete (86) 

Final compliance will be confirmed following finishes selection and calculation at design finalisation.



5. Positive Design Features

The Positive category refers to design elements which contribute positively to the environment. Buildings must actively reduce their harm, but also act as a restorative force for good in order to achieve credits. The category assesses energy use, energy source, water consumption and refrigerant emissions.

5.1 Upfront Carbon Emissions

The design team will target material selections which reduce the embodied energy by a minimum of 20%. Features to support this include the reuse of structures and civil infrastructure on site, as well as the use of natural finishes, waste diversion and high durability as part of the proposed solution. Final compliance will be confirmed with a detailed LCA moel as the design is completed.

5.2 Energy Use

The design team will ensure low energy use by improving performance against BCA Section J minimum, across building envelope, air conditioning and ventilation systems and lighting.

BCA compliance for apartments will be certified by a Cert IV NatHERS assessor.

Appliances will be selected to reduce operational costs and carbon.

Performance improvements will be justified based on Section J compliance calculations and benchmarking for typical apartment energy consumption.

5.3 Energy Source

The design will omit all fossil fuels from site. In addition, solar arrays will be provided to produce energy on site and reduce operational carbon and costs.

5.4 Water Use

The development is targeting a minimum 15% reduction in water consumption against benchmarks, including the provision of low flow fixtures are being provided for sanitary uses in accordance with the table below:

Fixture Type	WELS rating	Maximum Flow Allowable
Taps	5 stars	4.5-6 L/min
Urinals	5 stars	1.0 L / Flush + Smart demand flush device
Toilets	4 stars	<3.5 L average flush <4.7L full flush <3.2 half flush
Showers	3 stars	6.0 – 7.5 L/min
Clothes Washing Machines	4 stars	
Dishwashers	5 stars	

5.5 Net Zero Energy and Emissions Reduction

The design team are confident that the proposed energy efficient design features and onsite renewable generation will represent an annual energy consumption reduction of at least 30% against BCA minimum requirements.

5.6 Overall Footprint Reduction

The design team are confident that the proposed design features will result in a net life cycle emissions reduction of at least 30% if assessed by an LCA practitioner.

Final compliance will be confirmed by a peer reviewed LCA as the design is completed.

Material	PER embodied energy MJ/kg
Air dried sawn hardwood	0.3
Stabilised earth	0.7
Concrete blocks	1.5
In situ concrete	1.9
Precast tilt-up concrete	1.9
Kiln dried sawn hardwood	2
Precast steam-cured concrete	2
Clay bricks	2.5
Gypsum plaster	2.9
Kiln dried sawn softwood	3.4
Autoclaved aerated concrete (AAC)	3.6
Plasterboard	4.4
Fibre cement	4.8
Cement	5.6
Local dimensioned granite	5.9
Particleboard	8
Plywood	10.4
Glue-laminated timber	11
Laminated veneer lumber	11
MDF (medium density fibreboard)	11.3
Glass	12.7
Imported Dimensioned Granite	13.9
Hardboard	24.2
Galvanised steel	38
Acrylic paint	61.5
PVC (polyvinyl chloride)	80
Plastics — general	90
Copper	100
Synthetic rubber	110
Aluminium	170



6. Places Design Features

The places category reflects outcomes that are linked to the location and nature of the development. Design features which reduce the impacts of transport – on the environment and occupant health – are rewarded with credits. In addition, proximity of the development to local amenity and public transport which can promote walking and reduce reliance on vehicles is also recognised.

Within the site, the category considers the provision of communal spaces and their potential beneficial impacts on the occupants. Externally, project teams are encouraged to engage with, consult and justify their design to the local community as well as their shareholders.

The project team includes a recognised transport planner to assist in identifying requirements to reduce carbon footprint associated with travel.

6.1 Active Transport

The design will include bike parking for visitors and occupants. Each residential tower has a dedicated resident cycle parking with good connection to lift core and pedestrian infrastructure.

6.2 Walkability

The site has good local transport options and is considered very walkable with numerous and varied amenities within 400m of the site:

1 Court Place

Subiaco, Perth, 6008

Commute to **Downtown Perth**

5 min
 11 min
 11 min
 36 min
 [View Routes](#)

Favorite

Map

Nearby Apartments

Walk Score

83

Very Walkable

Most errands can be accomplished on foot.

Transit Score

67

Good Transit

Many nearby public transportation options.

[About your score](#)



7. People Design Features

The People category of credits provides an increased emphasis on social sustainability outcomes within the Green Star system. Projects are required to consider gender inclusivity and provide staff support around issues such as mental and physical health as part of their impact on the people building the project as well as the people who will use it long term.

The category also rewards projects that deliver strong outcomes for Indigenous or disadvantaged and under-represented social groups.

7.1 Minimum Requirements

The main contractor will be required to provide gender specific bathrooms and PPE on site and provide policies and training on discrimination, racism, bullying, drug and alcohol awareness and mental health. This will include introducing programs and solutions to address at least five current health issues such as suicide prevention, healthy eating and depression.

7.2 Needs Analysis

The contractor will be required to complete a needs analysis of site workers and contractors to inform the programs and policies implemented.

8. Nature Design Features

The Nature category is based on providing design solutions which prioritise and restore the natural environment around prospective developments. Features consider biodiversity, previous site usage, site emissions and waterway protection. Projects are required to demonstrate best practice performance across the range of local impact areas considered.

8.1 Minimum Requirements

- The site is not an old growth forest, prime agricultural land or within 100m of a nationally significant wetland.
- The external lighting will comply with AS 4282 – Control of the obtrusive effects of Outdoor Lighting
- No external light fitting will have an Upward Light Output Ratio (ULOR) of more than 5%.

8.2 Ecological Value

The current and future ecological value of the site will be retained through the use of primarily native planting, with green spaces across the site.

8.3 Stormwater

The design is to infiltrate a proportion of stormwater into local ground water, aiming to minimise outflow and achieve pollution reduction matching the performance below:

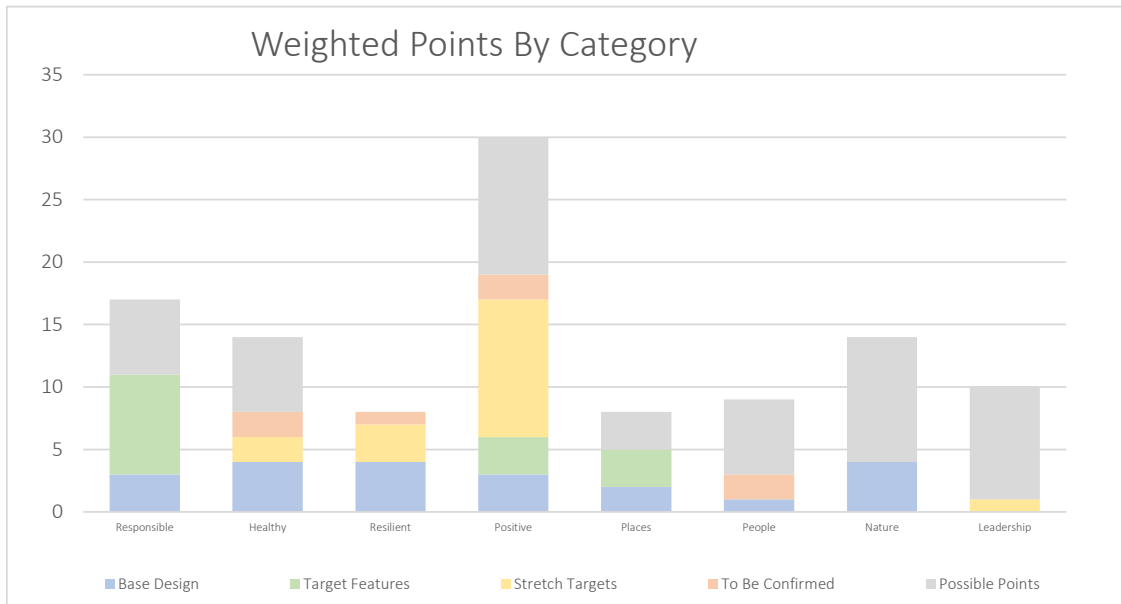
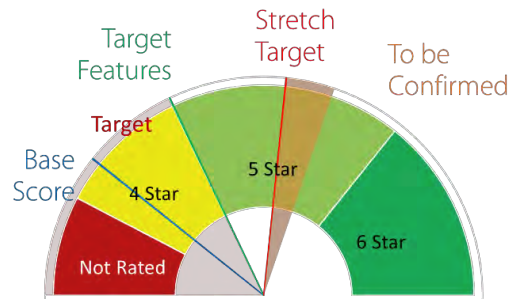
Pollutant	Contaminant Reduction
Total Suspended Solids (TSS)	90%
Gross Pollutants	95%
Total Nitrogen	60%
Total Phosphorus	70%



9. Assessment Review

FCDS have reviewed the existing documentation and, in conjunction with the developer and project team, have identified 35 Green Star Buildings design credits which we believe can be achieved within the project planning with a further 7 pending ongoing design development to confirm.

There are a further 17 points which could be added – if required – to ensure the project meets its target performance.



The design remains at an early stage and, therefore, is subject to change, however, the project notes the following key features expected to be included to support an Australian Excellence outcome include:

- Solar Array (nominal offset of more than 300 tonnes of CO_{2(e)} per annum)
- Sustainable and low-toxicity material selections
- Avoidance of fossil fuels within apartments
- Dedicated exhaust systems to manage indoor air quality.
- High efficiency services and appliances and a high-performance building envelope
- Acoustic, air quality and envelope performance verification at completion
- LCA and envelope modelling certification

Further information with respect Green Star Buildings approach is available in the attached scorecard.



Planning | Design | Delivery | Performance

www.fullcircledesign.com.au

Please Contact: Graham Agar

0412475819

Graham.agar@fcds.com.au

Subiaco LRC Meeting - Development Management



Project Name	Court Place		
Project Number	2024_099		
Version	Briefing V3.1		
Target Rating	5 Stars	35 Points	
Buffer	20%	Formal Certification?	Yes
Client	City of Subiaco		
Local Council	City of Subiaco		
Contract Value	~\$10M		
Building Owner	City		
Building Tenant	Various		

ESD Consultant	FCDS	GFA (m²)	\$2,000.0
Project Manager	Corbell	Site Area (m²)	5,000
Architect	BKD	Type	Residential
Quantity Surveyor	QS	UFA (m2)	29,432.6
ICA	FCDS	Car Bays	312
Building Surveyor	Resolve	Occupants	1252

Project Team		
Main Contractor	Main Contractor	
Mechanical	Consultant	Contractor
Electrical	Electrical	Electrical
Fire	Fire	Fire Contractor
Hydraulic	Plumber	Plumber
Civil	Structural	Civil
Structural	Structure	Structure
Facade	Facade	Facade
Landscape	Landscape	Landscape
Specialists		
Acoustic Engineer	Herring Storer	
Water Consultant	D&S	
Threat Planner	FCG Consulting	
Life Cycle Modeller	FCDS	
Climate Change	FCDS	

Credit	Cost	Cumulative Points	Cumulative Cost
W1.1 Separation of Waste Streams	\$0	21	\$752,000
W1.2 Discoloured Waste Storage	\$0	21	\$752,000
W1.3 Access to Waste Storage Area	\$0	21	\$752,000
W1.4 Qualified Waste Auditor	\$0	21	\$752,000
W1.5 Responsible Structure	\$45,000	24	\$797,000
W1.6 Responsible Structure - Exceptional	\$30,000	26	\$827,000
W1.7 Sustainable Transport	\$69,000	29	\$896,000
W1.8 Soft Landings Approach	\$70,000	30	\$966,000
W1.9 Responsible Finishes	\$105,000	31	\$1,071,000
W1.10 Responsible Finishes - Exceptional	\$105,000	32	\$1,176,000
W2.1 Emissions Reduction	\$133,333	35	\$1,309,333
W2.2 Low-carbon	\$0	36	\$1,309,333
W2.3 Showers	\$5,000	35	\$1,314,333
W2.4 Services and Maintainability Showers	\$20,000	35	\$1,334,333
W2.5 Minimum lighting context	\$30,000	35	\$1,364,333
W2.6 Glass	\$30,000	35	\$1,394,333
W2.7 Building Commissioning	\$0,000	35	\$1,394,333
W2.8 Measuring and Monitoring	\$0,000	35	\$1,394,333
W2.9 Provision of Outdoor Air	\$60,000	35	\$1,454,333
W2.10 100% Renewable Electricity	\$30,000	38	\$1,484,333
W2.11 100% Renewable Energy	\$30,000	41	\$1,514,333
W2.12 On-Site Toxicity Testing	\$37,500	43	\$1,551,833
W2.13 Net Zero Path	\$75,000	46	\$1,626,833
W2.14 Active Generation and Storage Systems	\$85,000	49	\$1,711,833
W2.15 Net Zero Path	\$120,000	51	\$1,831,833

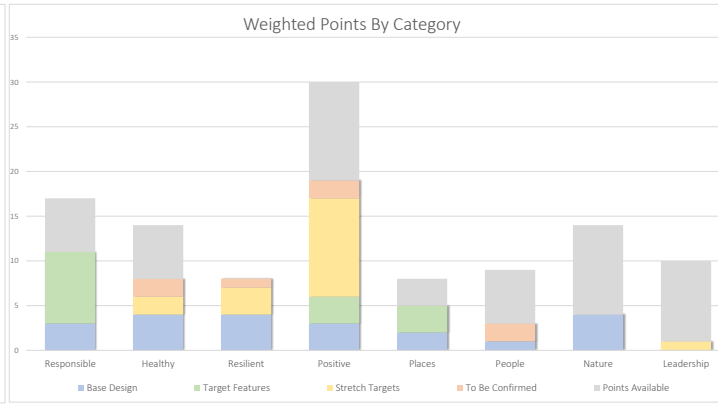
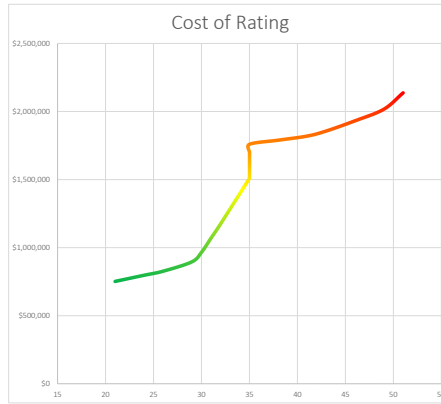
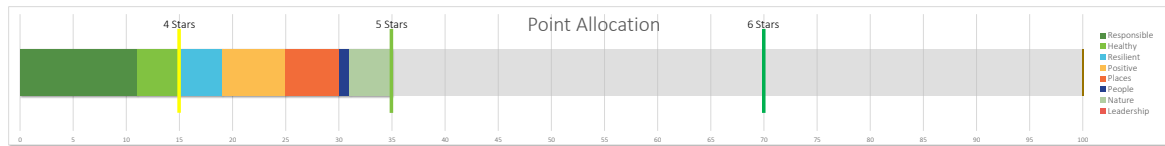
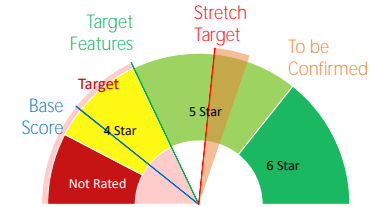
Green Star Credit List

This is FCDS self-assessment of the proposed redevelopment of Court Place. This scorecard is in no way equivalent to a formal review or certification by the Green Building Council and is intended to demonstrate how the design team intends to meet their target of 'Australian Excellence' in sustainable design.

Credits are classified as follows:
Base Design refers to features inherent in the current planning.
Target Initiatives are features which are considered to align with project goals and are part of the briefing to the project team.
Stretch Targets are features which are not currently intended to be included but could be added to replace or supplement other features as the design progresses.
 Scorecard is provided for information only. Refer to Green Star manual and latest published rulings for compliance requirements.

Category	Available Points	Base Design	Target Features	Stretch Targets	To Be Confirmed	Not Achieved	Total Cost
Responsible	17	3	8	0	0	6	\$475,000
Healthy	14	4	0	2	2	6	\$367,000
Resilient	8	4	0	3	1	0	\$85,000
Positive	30	3	3	11	2	11	\$1,133,333
Places	8	2	3	0	0	3	\$86,000
People	9	11	0	0	2	6	\$0
Nature	14	4	0	0	0	10	\$0
Leadership	10	0	0	1	0	9	\$0
Sector Specific	15	0	0	0	0	15	\$0
Total	125	210	14	17	7	66	\$2,134,833

Revision	Date	Reason for Issue
A	09 January 2024	GA Report



Subiaco LRC Meeting - Development Management



Ref No.	Title	Aim of Credit	Sub-Element	Credit Criteria Summary	Design Team Actions	Key Stakeholder	PTS Compliance	Points Available	Minimum Compliance?	Net Zero Pathway	Base Design	Target Features	Stretch Targets	To Be Confirmed	Unlikely Points	Comments			
Responsible	Industry Development	The development facilitates industry transformation through partnerships, collaboration and data sharing	Green Star Accredited Professional	The building owner or developer appoints a Green Star Accredited Professional, discloses the cost of sustainable building practices to the GBCA, and markets the building's sustainability achievements.	Appoint a GSAP as part of the project team.	FCDs			No							FCDs are completing this scope. Sustainable design information will be available through FCDs reporting and development marketing material.			
			Financial Transparency		The project team must complete and include in the submission, the Green Star Financial Transparency Disclosure Template. The template assists the project team to submit the cost of sustainable building practices of the project including design, construction and documentation to the GBCA.	FCDs		1	No		1					FCDs will complete this scope.			
			Marketing Excellence		* Complete Green Star Case Study * Disclose Sustainability Achievements to Stakeholders * Display Green Star Certification Prominently	FCDs, DNO, CH & Corbell			No								FCDs will need assistance from project marketing teams and project architect.		
	Responsible Construction	The builder's construction practices promote the reduction of impacts and opportunities for improved environmental outcomes.	Environmental Management System	The builder's construction practices reduce impacts and promote opportunities for improved environmental and social outcomes.	The builder or head contractor (responsible party) must have a formalised systematic and methodical approach to planning, implementing and auditing in place during construction. From the start of construction the builder or head contractor must implement an Environmental Management Plan (EMP) which must be project specific and cover the entire project scope including all construction activities.	Main Contractor		0	Yes		Met						Main contractor will be compliant.		
			Construction and Demolition Waste Diversion		Projects must divert at least 90% of construction and demolition waste from landfill.	Main Contractor		0	Yes		Met							Contractor to use instant waste or similar.	
			ESD Training		The head contractor must provide the following training to 95% of all contractors and subcontractors present on site for at least three days.	Main Contractor		0	Yes		Met								Contractor to add ESD training to site induction
			Construction and Demolition Waste Diversion - Exceptional		Projects must divert at least 90% of construction and demolition waste from landfill.	Main Contractor		1	No		1								Consider of standard practice when specified.
	Verification and Handover	The building has been optimised and handed over to deliver a high level of performance in operation.	Environmental Performance Targets	The building has been commissioned and will be tuned. The building was set up for optimum ongoing management due to appropriate metering and monitoring systems. The project team create and deliver operations and maintenance information to the facilities management team at the time of handover. Information is available to building users on how to use the building.	Set environmental performance targets prior to construction and document them within an OPR (Owners Project Requirements)	FCDs		0	Yes		Met						FCDs will generate OPR as part of ESD consultant scope.		
			Services and Sustainability Review		Complete a services and sustainability review prior to construction.	FCDs		0	Yes			Target						Independent commissioning agent is to complete this scope of work.	
			Building Commissioning		Commission the building in accordance with recognised best practice international standards, including a building envelope test. A specific building envelope pressure test plan and schematic are to be developed.	FCDs & EMF Griffiths		0	Yes				Target					FCDs recommend appointing specialist consultant for assistance with risk review.	
			Building Systems Tuning		Engage a building tuning service provider and tune the building for at least 12 months.	CH, FCDs & Corbell		0	Yes			Met						Building tuning to be complete within FCDs scope of works.	
			Metering and Monitoring		Provide accessible energy and water metering for all common user, major user, and major storages, connected to a monitoring system capable of capturing and displaying the data required for the OPR.	FCDs, EMF Griffiths, Ione & Aurum		0	Yes				Target						Electrical design includes meters. Sophisticated meter monitoring system to be added. Relatively few additional electrical meters. Multiple additional water meters. User guide.
			Operation and Maintenance Information		Provide operations and maintenance information for all nominated building systems to the building owner (or designated representative).	FCDs, EMF Griffiths, Ione & Aurum		0	Yes				Met						O&M Documentation is not problematic.
Independent Commissioning Agent		Building Log Book	An independent level of verification is provided to the commissioning and tuning activities through the involvement of independent commissioning agent, or through a soft landings approach that involves the future facilities management team. For large projects (>\$20M) both must occur.	Develop a building log book to present to the building owner (or designated representative) before practical completion of the project.	FCDs, EMF Griffiths, Ione & Aurum		0	Yes		Met						FCDs will generate the log book with assistance from project team and contractor.			
		Building User Information		All building user information must be available to the building owner and facilities management team at the time of practical completion.	Main Contractor, FCDs, EMF Griffiths, Ione & Aurum		0	Yes			Met						O&M Documentation is not problematic.		
		Soft Landings Approach		Deliver Soft Landings Approach for the project, including involving the FM team and/or Owners Representative across commissioning and handover, development of the Operations and Maintenance Manuals, Sign off on Operations and Maintenance Manuals and be trained before handover.	CH, FCDs & Corbell		1	No					1				Project requires the use of an ICA or the completion of the soft landings approach.		
		Independent Commissioning Agent		Appoint an independent commissioning agent (ICA) to act an advocate for and reports directly to the project owner.	CH, FCDs & Corbell				No										

Subiaco LRC Meeting - Development Management



Ref No.	Title	Aim of Credit	Sub-Element	Credit Criteria Summary	Design Team Actions	Key Stakeholder	PTS Compliance	Points Available	Minimum Compliance?	Net Zero Pathway	Base Design	Target Features	Stretch Targets	To Be Confirmed	Unlikely Points	Comments	
R4.1 - R4.4	Operational Waste	Operational waste can be separated and recovered in a safe and easy manner.	Separation of Waste Streams	The project team must demonstrate the building is designed to allow effective management of operational waste by: <ul style="list-style-type: none"> Separating waste streams; Providing a dedicated and adequately sized waste storage area; and Ensuring easy and safe access to waste storage areas for both occupants and waste collection contractors. 	The building must provide labelled and accessible bins or storage containers to building occupants to enable them to separate their waste. At least four waste streams are to be collected, each at 1% of annual operational waste, with any waste stream over 5% (other than food) of the annual volume provided with dedicated collection points.	CH, Taha & DKO		0	Yes			Target				Falls are providing a Waste Management plan including identification of relevant streams.	
			Dedicated Waste Storage		A dedicated area, or areas, for the storage and collection of the applicable waste streams must be provided. The storage area must be sized to accommodate all bins or containers, for all applicable waste streams, for at least one collection cycle.	Taha & DKO		0	Yes			Target				Design includes large waste store at ground level. Spatial allowances for additional streams are included in planned areas. Consider separate waste stores for commercial tenants.	
			Access to Waste Storage Area		The storage area(s) must have easy and safe access by collection vehicles and cleaning staff.	CH, Taha & DKO		0	Yes			Target					Waste storage area is convenient at ground level. Ample space for multiple waste streams.
			Qualified Waste Auditor		A waste specialist and/or contractor must sign-off on the designs to confirm they are adequately sized and located for the safe and convenient storage and collection of the waste streams identified.	CH, Taha		0	Yes			Target					Waste management plan to developed with project team.
R5.1	Responsible Procurement	The procurement process for all products, materials, and services for the building's design and construction follows best practice environmental and social principles.	Risk and Opportunity Assessment	The building's design and construction procurement process follows ISO 20400 Sustainable Procurement - Guidance and at least one identified supply chain risk and opportunity is addressed.	Undertake a risk and opportunities assessment of its supply chain to identify environmental and social risks and opportunities and	Main Contractor, FCDS & DKO		1	No		1					FCDS will generate a sustainable procurement plan. Social considerations to be shared by client group.	
R5.2	Responsible Procurement	The procurement process for all products, materials, and services for the building's design and construction follows best practice environmental and social principles.	Responsible Procurement Plan	The building's design and construction procurement process follows ISO 20400 Sustainable Procurement - Guidance and at least one identified supply chain risk and opportunity is addressed.	Develop and implement a plan to mitigate and manage identified risks and drive implementation of identified opportunities.	Main Contractor, FCDS & DKO		1	No								
R6.1	Responsible Structure	The building's structure is comprised of responsibly manufactured products.	Responsible Structure	50% of all structural components (by cost) meet a Responsible Products Value score of at least 10	<ul style="list-style-type: none"> Industry specific environmental product declarations (EPD) - 2 Points Product specific environmental product declarations (EPD) - 4 Points ISO14001 certification - 3 Points Recycled Product - 15 Points FSC Certified - 10 Points Best Practice PVC Certified - 5 Points Living Product Challenge Declare 	Hera & DKO		3	No			3				Potential to achieve credit if sourcing sustainable steel - I Steel or similar - and concrete - Borala Enviro or Holcim EcoQuat.	
Responsible Structure - Exceptional			In addition, one of the following is met: <ul style="list-style-type: none"> 10% of all products in the structure (by cost) meet a Responsible Products Value score of at least 12, OR 40% of all products on the structure (by cost) have an average Responsible Products Value score of at least 10. 	Hera & DKO		2	No			2							
R7.1	Responsible Envelope	The building's envelope is comprised of responsibly manufactured products.	Responsible Envelope	80% of all building envelope components (by cost) meet a Responsible Products Value score of at least 10.	<ul style="list-style-type: none"> Industry specific environmental product declarations (EPD) - 2 Points Product specific environmental product declarations (EPD) - 4 Points ISO14001 certification - 3 Points Recycled Product - 15 Points FSC Certified - 10 Points Best Practice PVC Certified - 5 Points 	Hera, BMO & DKO		2	No								
Responsible Envelope - Exceptional			In addition, one of the following is met: <ul style="list-style-type: none"> 10% of all products on building envelope (by cost) meet a Responsible Products Value score of at least 12, OR 40% of all products in the building envelope (by cost) have an average Responsible Products Value score of at least 10. 	Hera, BMO & DKO		2	No									Considered unlikely.	
R8.1	Responsible Systems	The building's mechanical, hydraulic, transportation and electrical systems are comprised of responsibly manufactured products.	Responsible Systems	30% of all active building systems (by cost) meet a Responsible Products Value score of at least 6.	<ul style="list-style-type: none"> Industry specific environmental product declarations (EPD) - 2 Points Product specific environmental product declarations (EPD) - 4 Points ISO14001 certification - 3 Points Recycled Product - 15 Points FSC Certified - 10 Points Best Practice PVC Certified - 5 Points Climate Active Carbon Neutral Certification 	EMF Griffiths, Aurum, Alphastra, SNC, Callers & Aurum		1	No							Considered unlikely	
Responsible Systems - Exceptional			In addition, one of the following is met: <ul style="list-style-type: none"> 1% of all active building systems (by cost) meet a Responsible Products Value score of at least 11, OR 30% of all active building systems (by cost) have an average Responsible Products Value score of at least 6. 	EMF Griffiths, Aurum, Alphastra, SNC, Callers & Aurum		1	No										
R9.1	Responsible Finishes	The building's internal finishes (flooring, plasterboard, paints, ceilings, partitions, doors, internal windows etc.) are comprised of responsibly manufactured products and materials.	Responsible Finishes	60% of all internal building finishes (by area) meet a Responsible Products Value score of at least 7.	<ul style="list-style-type: none"> Industry specific environmental product declarations (EPD) - 2 Points Product specific environmental product declarations (EPD) - 4 Points ISO14001 certification - 3 Points Recycled Product - 15 Points FSC Certified - 10 Points Best Practice PVC Certified - 5 Points 	DKO & FCDS		1	No			1					
Responsible Finishes - Exceptional			In addition, one of the following is met: <ul style="list-style-type: none"> 10% of all internal building finishes (by area) meet a Responsible Products Value score of at least 12, OR 40% of all internal building finishes (by area) have an average Responsible Products Value score of at least 7. 	DKO & FCDS		1	No				1					Design will target responsible finishes within occupied areas.	

Subiaco LRC Meeting - Development Management



Ref No.	Title	Aim of Credit	Sub-Element	Credit Criteria Summary	Design Team Actions	Key Stakeholder	PTS Compliance	Points Available	Minimum Compliance?	Net Zero Pathway	Base Design	Target Features	Stretch Targets	To Be Confirmed	Unlikely Points	Comments		
H10.0	Clean Air	Pollutants entering the building are minimised, and a high level of fresh air is provided to ensure levels of indoor pollutants are maintained at acceptable levels. Compliance is to be demonstrated for > of regularly occupied area	Ventilation System Attributes	Ventilation systems are to comply with AS 1688.2 2012 and ASHRAE Standard 62.1:2013 for minimum separation between openings, outdoor intakes and sources of pollution. All new and existing ductwork is to be cleaned prior to occupation.	EMF Griffith			0	Yes		Met					Basic requirement for mechanical design. No issues with this requirement.		
H10.1			Provision of Outdoor Air	Pollutants entering the building are minimised, and a high level of fresh air is provided to ensure levels of indoor pollutants are maintained at acceptable levels. Compliance is to be demonstrated for > of regularly occupied area	Provide outdoor air to each space in the nominated area at a rate greater than the minimum required by AS 1688.2:2012 by 50% or Monitor and control CO ₂ levels <100ppm during design occupancy or Residential Buildings demonstrate best practice approach to outside air and intervention of mould.	EMF Griffith		0	Yes			Target					Residential spaces to comply with AS1688.4 / or provide outside air to make up exhaust rates.	
H10.2			Exhaust or Elimination of Pollutants	Exhaust or Elimination of Pollutants	Select and utilise low emissions equipment, printers, stoves, vehicles etc. and/or Exhaust sources of pollutants directly to outside and physically separate them from occupants.	CHK, Corbett, DGD & EMF Griffith		0	Yes			Met						Provision of exhaust systems within units and to contaminated areas are expected.
H10.3			Ventilation System Attributes	The building's ventilation systems allow for easy maintenance, and high levels of outdoor air are provided.	Provide adequate access to both sides of all moisture and debris-carrying components for maintenance within the air distribution system and	EMF Griffith			2	No								Ducted fan coil units with HEPA filtration and shallow coils can achieve this outcome. May difficult - depending on mechanical design intent
H10.3			Provision of Outdoor Air	Provide outdoor air to each space in the nominated area at a rate greater than the minimum required by AS 1688.2:2012 by 100% or Monitor and control CO ₂ levels <700ppm during design occupancy or Provide 0.04m ³ ventilation per apartment + 0.015m ³ per habitable room.	EMF Griffith				2	No								AS1688.4 / make up air rates should achieve this target.
H11.1	Light Quality	The building provides good daylight and its lighting is of high quality.	Minimum lighting comfort	<ul style="list-style-type: none"> All lighting must be flicker-free. Light sources must have a minimum Colour Rendering Index (CRI) 85 or higher, in all internal and external applications. Light sources must meet best practice illuminance levels for each task within each space type. The maintained illuminance values must achieve a uniformity of no less than that specified in Table 5.2 of AS/NZS 1680.1:2006. All light sources must have a maximum of 3 MacAdam Ellipses. 	Aurum		0	Yes			Target					May restrict lighting selections. Budget of \$12 per m ² for compliance.		
H11.2			Glare	The building provides adequate levels of daylight and good lighting levels suitable for the typical tasks in each space.	<ul style="list-style-type: none"> Glare from light sources must be limited within the nominated area. Base light fixtures must be fitted with baffles, louvers, translucent diffusers, ceiling design, or other means or Unified Glare Rating (UGR), as estimated from the manufacturer's data sheets for a standard room, must not exceed the maximum values listed in Table 6.4 of AS/NZS 1680.1:2006. Where the nature of the task, layout and surface reflectance in a space are not known (e.g. child and care) the lighting system must comply with the Luminance selection system as detailed in Clause 8.3.4 of AS/NZS 1680.1:2006. 	DGD & FCCS		0	Yes			Target					As above, may limit potential light fitting choices. Budget of \$5 per m ²	
H11.3			Daylight	Maximise the number of occupants that are in or near daylight areas during their daily activities for all building types.	<ul style="list-style-type: none"> Ensure regularly occupied spaces are in reasonable proximity to glazed facades, windows or skylights. Control or mitigate glare in the daylight spaces. Maximise daylight to spaces that prioritise learning, healing, and living. Provide building occupants with unobstructed access to daylight indoor common spaces. 	DGD & FCCS		0	Yes			Met						Building is well oriented and designed for daylight penetration.
H11.4			Daylight - Credit	For non-residential buildings, at least 40% of the principle averaged across the building must receive high levels of daylight with no less than 20% on any floor or tenancy (whichever is smaller). For residential buildings, 60% of the combined living and bedroom area of each apartment unit must comply with the daylight requirements. Kitchens are not included in the calculations. The daylight levels must also be present in at least 20% of the area of each bedroom and living area. Residential buildings and hospitality buildings must provide room blackout blinds or curtains to all bedrooms. If blinds or curtains are part of a packaged offer, all blinds offered for the bedroom décor must be blackout blinds.	DGD & FCCS		2	No		2								Project will target strong daylight access for all occupied areas.
H11.5			Artificial Lighting	The walls within the field of view of occupants in regularly occupied spaces must have an average surface reflectance value of 0.70 and an average surface illuminance of at least 50% of the horizontal illuminance level required for task.	<ul style="list-style-type: none"> Vertical illuminance in workspaces: ensure that 50% of the horizontal task illuminance reaches the average eye height for 50% of primary spaces using vertical illuminance calculation grid. 	Aurum		2	No									To be reviewed with designer and lighting modeller in due course. May be achievable with wall washing within the residential areas.
H12.1	Acoustic Comfort	The building provides acoustic comfort for building occupants.	Internal Noise Levels	An Acoustic Comfort Strategy is prepared to describe how the building and acoustic design aims to deliver acoustic comfort to the building occupants.	Internal ambient noise levels in the nominated areas must be no greater than the upper range value and (D)noise and (Commercial) Tenancies no less than 5 dB below the lower range value relative to the activity type in each space as recommended in AS/NZS 2107.	Herring Stover, EMF Griffith & DGD		0	Yes		Met					Acoustic consultant to be appointed to review and optimise design as well as measuring performance at completion. Compliance with noise levels may be problematic.		
H12.2			Acoustic Separation	The building is designed and tested to achieve minimum acoustic performance requirements aligned with the Acoustic Comfort Strategy.	Address noise transmission between enclosed spaces within the nominated area either by sound privacy or insulation.	Herring Stover & DGD			No								Design will include strong noise separation between dwellings as part of a premium product offering. Cost may be prohibitive.	
H12.3			Impact Noise Transfer	<ul style="list-style-type: none"> Residential - 2 Additional Features Healthcare - 3 Additional Features Schools - 3 Additional Features Tenancies - 3 Additional Features Child and Care - 2 Additional Features Sports - 2 Additional Features Industrial - Separation Only 	Floors above nominated areas or adjacent spaces belonging to different tenancies which share a floor must not exceed 85 L _n /m ² s ² for floors above residential accommodation spaces -60 for all other spaces	Herring Stover & DGD		2	No					2			Noise separation between levels is important for project success. May be expensive.	
H12.4			Reverberation	The reverberation time in the nominated area must not exceed the maximum for the intended use recommended in AS/NZS2107. This does not apply for residential spaces.	Herring Stover & DGD			No										Not applicable for residential projects.

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H13.1	Exposure to Toxins	The building's occupants are not directly exposed to toxins in the space they work, play, or live in.	Paints, Adhesives, Sealants and Carpets	the building's paints, adhesives, sealants, carpets, and engineered wood products are low or non-toxic. Occupants are not exposed to banned or highly toxic materials in the building.	Use low toxicity paints, adhesives, sealants and floor coverings.	DND, EMF or BHS, Aurum, Aghasara, Iqon, Colibri & Aurum		0	Yes		Met					Commit to low toxicity paints, adhesives, sealants and floor coverings.	
			Engineered Wood products		Use low emission formaldehyde composite or engineered wood products.	DND		0	Yes		Met				Commit to use of E0 joinery and composite wood products.		
			No Lead, Asbestos and PCBs	Undertake comprehensive site survey for Lead, PCB's and asbestos. Take appropriate remediation action where relevant.	CHL		0	Yes		Met					No requirement, no existing buildings are being reused.		
			On-Site Toxicity Testing	On-site tests verify the building has low Volatile Organic Compounds (VOC) and formaldehyde levels.	Undertake performance testing on site at completion to confirm building meets target toxicity levels - TVOC < 0.27 ppm and Formaldehyde < 0.02ppm	FCDS & CHL		2	No				2			On-site review of VOC and formaldehyde content would verify performance. Minimum of 5 floors, 25 samples.	
H14.1	Amenity and Comfort	The building provides internal amenities that improve occupant experience of using the building.	Amenity and Comfort	The building has dedicated amenity rooms to act as parent room, relaxation room, or an exercise room.	Provide at least 10m ² and 2m ² per 20 regular occupants or staff (dedicated room(s) to provide either inclusivity, mindfulness or exercise for regular occupants. Room must be classified as either parent room, relaxation / meditation / prayer room or exercise room. Room must meet light quality, acoustic comfort and Design for Inclusion (where targeted) levels.	DND - Requires 120m ² not dedicated occupant welfare space		2	No		2					Building will provide amenity areas at podium level to support occupants.	
H15.1	Connection to Nature	The building fosters connection to nature for building occupants.	Views	The building provides views, includes indoor plants, and incorporates nature-inspired design.	>60% of primary occupied spaces are within 8m of a compliant view and	DND			No		Met						Design easily complies with the requirement for views.
			Plants		Indoor plants must be provided in the rehabilitated spaces. One or more plants in pots with a soil surface area totaling at least 500cm ² for every 20m ² of the primary spaces is required. Plants are to be supported by an ongoing 3 year maintenance plan and	DND & CHL		1	No								Plants could be provided in communal areas, consideration for credit to be reviewed with CBCA.
H15.3	Connection to Nature	The building fosters connection to nature for building occupants.	Nature-Inspired Design	The building provides views, includes indoor plants, and incorporates nature-inspired design.	Five additional nature-inspired design interventions must be provided in alignment with the following principles: <ul style="list-style-type: none"> • Elements that provide offering natural sensory experiences; • Elements that reflect natural and cultural patterns and forms; • Using natural materials; and • Natural motifs and art. 	DND			No								Integration of nature within the design can be accommodated, requires design review with project team.
H15.4			Interaction		5% of the building's floor area/ or site area (whichever is greater) is allocated to nature in which occupants can directly engage with	Occupants can interact with nature either inside the building, or externally through a green wall or roof garden. At least 5% of the building's floor area/ or site area (whichever is greater) must be allocated to the opportunity. The allocated area must be accessible and have the necessary infrastructure to allow the activity to occur (for example water source/taps for irrigation, storage area for tools and equipment).	CHL & DND		1	No							

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Resilient	R16.1	Climate Change Resilience	Climate Change Resilience	The project team completes the climate change pre-screening checklist. The project team communicates the building's exposure to climate change risks to the applicant.	Complete climate change risk review checklist and issue to the building applicant.	FCDS		0	Yes		Met					FCDS to complete detailed climate risk review.	
	R16.2		Climate Change Risk and Adaptation Assessment	The project team develops a project-specific, climate change risk and adaptation assessment for the building. Extreme and high risks are addressed.	Complete a detailed risk assessment and Mitigation strategy using an appropriately qualified professional. Ensure at least two and all Extreme and High risks are addressed.	Whole Design Team		1	No		1					* Envelope performance * On site generation * Natural Ventilation * Spare mechanical capacity * Spare electrical capacity	
	R17.1	Operations Resilience	Comprehensive Risk Assessment	The project team undertakes a comprehensive review of the acute shocks and chronic stresses likely to influence future building operations.	Appoint a suitably qualified professional to undertake a detailed review of operations, resilience including key shocks and stresses, such as infrastructure failure, health pandemic, water security, increasing energy costs and rising cyber dependency and	FCDS			No							Building will be designed to provide occupant support under shocks and stresses.	
	R17.2		Managing Risks	The building's design and future operational plan addresses any high or extreme system-level interdependency risks.	Provide design solutions to address at least two and all high and extreme risks and	FCDS supported by Whole Design Team		2	No		2					Design team can provide potential mitigation to significant risks to project.	
	R17.3		Addressing Power Loss	The building's design maintains a level of survivability and design purpose in a blackout.	Complete building performance assessment in black out conditions and provide appropriate design solutions to meet building purpose and provide a measure of survivability for occupants.	FCDS supported by Whole Design Team			No							Blackout review not likely to identify any significant issues. Requires review and confirmation of approach.	
	R18.1	Community Resilience	The building contributes to improving the resilience of the community.	Community Resilience	The project team undertakes a needs analysis of the community, identifies shocks and stresses that impact the building's ability to service the community, and develops responses to manage these.	FCDS supported by Whole Design Team		1	No		1					Provision of social housing considered to improve community resilience outcomes.	
	R19.1	Heat Resilience	The building reduces its impact on heat island effect.	Heat Island	At least 75% of the whole site area comprises of one or a combination of strategies that reduce the heat island effect.	Ensure >75% of site area is covered by materials which reduce heat island effect, including: * Vegetation and Green Roofs * Light Roofs (3 Year STR44 Flat roof) >34 (Pitched Roof) * Shaded or light Hardscaping (3 Year STR > 34) * Water bodies and/or water courses * Permanently Shaded site area	D&D & Swedberg	1	No				1			Design is compliant with the provision of a light roof and paved areas.	
	R20.1	Grid Resilience	Active Generation and Storage Systems	The building meets one or several of the following: * Provides active generation and storage systems; * Has the infrastructure to deliver an appropriate demand response strategy or * Has reduced its electricity consumption through passive design.	Design the building to have the capacity to reduce its electricity peak demand by 10% of the building's annual peak electricity demand (or at least a one-hour period). The peak demand reduction can occur through thermal storage solutions (such as chilled water storage systems), by electricity storage solutions (batteries), or through renewable on-site generation.	EMF Griffith & Aurnum			No								Design could achieve this outcome with batteries and load management system. Initial estimate of 50MWh battery system.
	R20.2		Demand Response	The building meets one or several of the following: * Provides active generation and storage systems; * Has the infrastructure to deliver an appropriate demand response strategy or * Has reduced its electricity consumption through passive design.	Develop a strategy and systems to automatically shed 10% of a building's peak electrical demand without affecting occupant amenity for at least 4 hours. Requires the EMS to have predictive capabilities and a demand management dashboard and automatic load shedding, including acceptance of external control signals. Strategy to be commissioned and demonstrated as functional.	EMF Griffith & Aurnum		3	No				3				EMS and load management may be able to achieve this outcome.
	R20.3		Passive Design Solutions	The building meets one or several of the following: * Provides active generation and storage systems; * Has the infrastructure to deliver an appropriate demand response strategy or * Has reduced its electricity consumption through passive design.	Deliver a naturally ventilated building which exceeds BCA requirements for building envelope performance. Alternatively, discuss options with the BCA for alternate compliance.	FCDS & D&D			No								Building is provided with air conditioning.

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P21.1	Uplift Carbon Emissions	The building's upfront carbon emissions from materials and products have been reduced and offset.	Minimum Expectation	Reduce the building's upfront carbon emissions reductions through good design and material selection.	Complete the LCA Calculator and demonstrate that the building's upfront carbon emissions are at least 10% less than those of a reference building.	CL, FCDs & DMO		0	Yes		Met					FCDs to complete LCA analysis for the building and optimise performance.	
			Net Zero Path		Employ an LCA professional to demonstrate the building's upfront carbon emissions are at least 20% less than those of a reference building - including any demolition points.	FCDs, CHL&DMO		3	5 Star	3			3		4 - 40% off table - Code wind loads to site specific wind study - Full transfer slab to limited transfer - Concrete substitution, carbon neutral concrete - Sustainable Steel and Concrete		
			Emissions Reduction		Employ an LCA professional to demonstrate the building's upfront carbon emissions are at least 40% less than those of a reference building - including any demolition works and all remaining emissions from Modules A1 - A5 are offset.	FCDs, Main Contractor, CHL&DMO		3	6 Star					3	Design team will utilise sustainable construction materials and processes to reduce building footprint. Cost budget based on 400g/m2 of GFA.		
P22.1	Energy Use	The building has low energy consumption.	Minimum Expectation	Reduce the building's energy consumption in comparison to BCA Section J minimum requirements. All systems and elements must exceed BCA minimum performance, irrespective of overall performance.	Complete building modelling and demonstrate that the building's energy use is at least 10% less than a reference building, excluding renewable generation on site - for residential buildings, no individual apartment can be less than the larger number of: - The minimum NABERS rating stated in the code, or - 6.5 star NABERS rating.	FCDs		0	Yes		Met					Project team to target 20% improvement for Section J. Roof = R4.5, Solar Absorbance < 0.35. Walls = R2.2, 20% insulation. Thermal Break. Blinds = Low-E Double Glazing, moderate tint. Operable windows. Roofs = R1.9 Board insulation to any exposed floors.	
			Net Zero Path		Complete building modelling and demonstrate that the building's energy use is at least 20% less than a reference building, including renewable generation on site.	FCDs		3	5 Star	3	3				Compliance to be achieved through envelope performance and services design. Minimum envelope performance 20% better than BCA. Mechanical systems to utilise natural ventilation as much as practicable.		
			Emissions Reduction		Complete building modelling and demonstrate that the building's energy use is at least 30% less than a reference building.	FCDs		3	5 Star				3		Requires building energy model and justification. Compliance assessed with provision of 200000 kWh Array. Will require major design re-iteration to achieve compliance. Lighting and air conditioning systems to be extremely efficient.		
P23.1	Energy Source	The building's energy comes from renewables.	Zero Carbon Action Plan	Ensure the building does not require fossil fuels to operate and develops a detailed plan to achieve net carbon neutrality in operation.	Develop a zero carbon action plan and have it endorsed by the building owner. The plan is to address all Scope 1 and 2 emissions, including refrigerants on site and provide design requirements, including additional spatial and infrastructure upgrade required. The plan is to include a cost analysis including potential savings to design as net zero from practical completion.	FCDs, Ions, Alphastra, DMF Griffiths, Corbett & Co.		0	Yes	0	Met					* Avoid all fossil fuels on site: - Central heat pump hot water - Offset diesel emissions from fire pumps - Reverse cycle heating	
			100% Renewable Electricity		The building is to ensure all electricity under the control of this building owner or operator must be generated and sourced from renewables. Tenant electricity can be excluded, where this is achieved by off-site renewables, a 5 Year contract must be presented.	FCDs, Ions, Alphastra, DMF Griffiths, Corbett & Co.		3	5 Star	3			3	Cost of offsetting energy consumption considered rationally sized. Budget based on expected energy use and 80% solar utilisation on site, offset for 5 years.			
			100% Renewable Energy		In addition to the above, where the building includes infrastructure which can use fossil fuels to power building systems, the applicant must demonstrate how it will not use fossil fuels during the building operation.	FCDs, Ions, Alphastra, DMF Griffiths, Corbett & Co.		3	5 Star	3			3	Offset of fossil fuels from minor uses provides limited value other than the credit points.			
P24.1	Other Carbon Emissions	The building's emissions from refrigerants and remaining carbon sources are eliminated or offset.	Net Zero Path	Utilise refrigerants with a GWP<10 or offset the GWP. Maintenance access must be provided to access and replace refrigerants.	DMF Griffiths		2	5 Star	2					2	Selection of GWP < 10 is not commercially feasible for this project. Limits product selection. Cost budget based on 120g/m2 of usable floor area.		
P24.2			Emission Elimination	Eliminate all other emissions, including elements above plus: - Life cycle emissions from modules B and C as calculated in Life Cycle Impacts; - Emissions from construction equipment use, and activities during construction on site.	CHL		2	6 Star								Cost budget based on 300g/m2 of GFA.	
P25.1a	Water Use	The building has low water use.	Water Reduction Prescriptive	Ensure the building meets minimum water performance requirements through a simple, prescriptive approach.	Provide low flow fixtures and appliances.	FCDs		0	Yes		Met						FCDs to complete performance modelling, expected to be achieved with low flow tapware and appliances.
P25.1b			Water Reduction Performance	Ensure the building meets minimum water performance requirements through a modelled approach.	Demonstrate a 15% performance improvement over minimum compliance using the Green Star Potable Water Calculator.	FCDs, Ions & DMO											Initial water modelling indicates 30% water saving potential.
P25.2			Improved Water Performance	Multi-unit residential buildings use 40% less potable water compared to a reference building.	Complete water modelling and demonstrate a 40 / 45% performance improvement in water consumption for the proposed development.	FCDs, Ions & DMO		3	No								Compliance could be achieved with a grey water collection / reuse system for toilet flushing.
P25.3			Exceptional Water Performance	Multi-unit residential buildings use 60% less potable water compared to a reference building.	Complete water modelling and demonstrate a 60 / 75% performance improvement in water consumption for the proposed development.	FCDs, Ions & DMO		3	No								
P26.1	Life Cycle Impacts	The building has lower environmental impacts from resource use over its lifespan than a typical building.	Life cycle impacts	The project demonstrates a 30% reduction in life cycle impacts when compared to standard practice.	Appoint an LCA practitioner and demonstrate an overall 30% performance improvement against all categories with no more than 10% increase in any category.	FCDs		2	No							2	Credit considered likely based on upfront carbon an energy emissions.

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P27.1	Movement and Place	The building's design and location encourages occupants and visitors to use active, low carbon, and mass transport options instead of private vehicles.	Showers		Provide showers at a rate of 1 per 50 occupants (2 per 200 occupants over 200)	D&D - Requires 7 Showers for Occupants		0	Yes			Target				Commercial tenancy to include 607 provisions within back of house areas.	
			Lockers		One locker must be provided for every eight staff occupants. The lockers must be secure and located in the changing rooms.	D&D - Requires 75 Lockers for Occupants		0	Yes			Target				Commercial tenancy to include 607 provisions within back of house areas.	
			Accessibility and Inclusion		The building includes showers and changing facilities for building occupants that are accessible, inclusive and located in a safe and protected space.	Upon accessing, pedestrians and cyclists must be protected from the elements and other vehicles. Access must be safe, with consideration given to avoiding steep gradients, surface grip levels and visibility around tight corners. Access to the facilities must be well lit between entryway to bike parking, all amenities and lift lobbies and main access points to the building. All regular building occupants must have easy access to lockers, showers, and building entry. Occupants must be able to find the facilities thanks to clear signage throughout the building and access points.	D&D		0	Yes		Met				Design will support occupant active transport.	
			Cyclist Facilities		The building's access must prioritise walking and cycling options. This means the building's access must be well lit, weather protected, and separated from vehicles. The building must also include access to cyclist facilities that are separated from the primary vehicle entrance to ensure safety.	Provide good access for cyclists and pedestrians: <ul style="list-style-type: none"> * Good lit * Weather Protected * Separated from Vehicles * Signposted * Secure * Access connected to relevant cyclist storage 	CHL & D&D			No		Met				Design includes parking bays for residents. Recommended addition of steel level bike parking for visitors.	
			Sustainable Transport		Prepare a sustainable transport plan which seeks to change the mode of transport away from single use vehicles.	Include: <ul style="list-style-type: none"> * Car share parking schemes * Infrastructure for future EV charging and parking * EV load management system 	CHL & Aurum		3	No			3				Provision of 10 EV charger would meet this requirement.
			Reducing Private Use		The building's design and location prioritises walking, cycling, and transport options that reduce the need for private fossil fuel powered vehicles.	Appoint a professional to provide a transport plan to reduce emissions associated with private vehicle use in comparison to a reference building. Includes reduced vehicle emissions, increase active transport and reduction in trip kilometres. And Provide at least 10 amenities across at least five categories within 400m of the project site. Prioritise pedestrians over cars with strict speed limits on site.	Corbett & PGS Consulting	x		No			Met				Project team includes a transport planner
			Walkability		Provide at least 10 amenities across at least five categories within 400m of the project site. Prioritise pedestrians over cars with strict speed limits on site.	FGCS					No			Met			
P28.1	Enjoyable Places	The building provides places that are enjoyable and inclusive.	Publicly Accessible Spaces	The building delivers memorable, beautiful, vibrant communal or public places where people want to gather and participate in the community. The spaces are inclusive, safe, flexible and enjoyable	For residential spaces provide 1.75m ² per dwelling, with a minimum of 250m ² and	D&D - Requires 236m ² Public Access Space		2	No							Design does not include sufficient public space.	
P28.2			Activation Strategy	Develop and fund an activation strategy to ensure programming continues after practical completion. The strategy must address target activities, funding, timing of activation, facilities/suppliers, encouragement, future implementation.	D&D & CHL			No									
P29.1	Contribution to Place	The building's design makes a positive contribution to the quality of the public environment.	Urban Context Analysis	The building's design contributes to the livability of the wider urban context and enhances the public realm.	Provide an urban context report that considers economic, physical, social and cultural factors and considers planned changes to the local area. The plan should identify challenges which the building can contribute to solving and demonstrate appropriate design responses or	D&D		2	No		2					Design is being undertaken in close collaboration with local council and key stakeholders. There is potential for the credit to be achieved based on the service provision.	
P29.2			Independent Design Review	Utilise an independent design review panel - such as the OGA - to undertake design reviews at key points in the design. As a minimum this includes at concept / schematic phase, design development phase and at building permit stage.	D&D & CHL			No									
P30.1	Culture Heritage and Identity	The building reflects local culture, heritage and identity	Community Led Design Response	The building's design reflects and celebrates local demographics and identifies the history of the place, and any hidden or minority stories. This collaboration was arrived through meaningful engagement with community groups early in the design process.	Produce a report detailing the community engagement activities undertaken and resultant design responses through features, such as: <ul style="list-style-type: none"> * Community art or placemaking projects; * Selection of suppliers/designers of artworks or cultural elements; * Building elements that tell stories of the past and heritage; and * Spaces and uses that reflect the local identities; or 	D&D & CHL		1	No							Project does not contribute to local culture or heritage.	
P30.2			Independent Design Review	Utilise an independent design review panel - such as the OGA - to undertake design reviews at key points in the design. As a minimum this includes at concept / schematic phase, design development phase and at building permit stage.	D&D & CHL			No									

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P31.1	Inclusive Construction Practices	The builder's construction practices promote diversity and reduce physical and mental health impacts	Minimum Requirement	During the building's construction, the head contractor provide gender inclusive facilities and protective equipment. The head contractor also install policies on-site to increase awareness and reduce incidence of discrimination, racism, bullying and alcohol awareness and mental health.	The main contractor shall provide gender specific bathrooms and PPE on site and provide policies and training on discrimination, racism, bullying, drug and alcohol awareness and mental health.	Main Contractor		0	Yes		Met					Contractor to ensure these requirements are met.	
P31.2			Needs Analysis	The head contractor provides high quality staff support on site to reduce at least five key physical and mental health aspects related to construction workers. They must also evaluate the effectiveness of their interventions.	The contractor must complete a needs analysis of site workers and contractors to determine appropriate actions. The policies and programs should be relevant to all construction workers on site for the full duration of construction. A mix of programs is acceptable throughout the duration of construction period. and	Main Contractor			No								
P31.3			Physical and Mental Health Impacts	The head contractor must introduce programs and solutions to address at least five of the following: • Social prevention; • Healthy eating and active living; • Reduce harmful alcohol and tobacco consumption and avoid drug use; • Increased social cohesion, community and cultural participation; • Understanding depression; • Preventing violence and injury; • Decreased psychological stress; and • Finding fulfillment at work or meaningful meditation. and		Main Contractor		1	No		1						NCIS to assist main contractor in demonstrating compliance.
P31.4			Evaluating Effectiveness	Provide an evaluation report to the client and sub-contractor including programs and initiatives delivered and whether they delivered the intended outcomes.		Main Contractor			No								
P32.1	Indigenous Inclusion	The building celebrates Aboriginal and Torres Strait Islander people, culture and heritage.	Reconciliation Action Plan	The building's design and construction celebrates Aboriginal and Torres Strait Islander people, culture and heritage by undertaking one or both of the following: • Playing an active role in the Organisational Reconciliation Action Plan; and • Incorporating design elements using the Indigenous Design and Planning principles.	Ensure that the project team includes a member of the organisation RAP Working Group. In addition, at least 50% of the project RAP targets are to have been met and all implemented actions related to the RAP are publicly reported on the Project's website. Any design element must be informed by consultation undertaken with the local Aboriginal and Torres Strait Islander community or through nominated representatives. or	DOD & DIL			No							Outside of the scope of this project.	
P32.2			Inclusion of Indigenous Design	Demonstrate that the Australian Indigenous Design Charter are incorporated within the design. As a minimum, ensure the following are addressed: • Indigenous Led: Ensure Aboriginal and Torres Strait Islander representation in the creation of the design. • Community Specific: Ensure respect for the diversity of Aboriginal and Torres Strait Islander culture by following community specific cultural protocols. • Impact of Design: Always consider the reception and implications of all designs so that they are respectful to Indigenous culture, and • Shared Knowledge (collaboration, co-creation, procurement): Develop and implement respectful methods for all levels of engagement and sharing of Indigenous knowledge (collaboration, co-creation, procurement).		DOD & DIL		2	No								
P33.1	Procurement and Workforce Inclusion	The building's construction facilitates workforce participation and economic development of disadvantaged and under-represented groups.	Social Procurement Strategy	Through the implementation of a social procurement strategy, at least 2% of the building's total contract value has been directed to generate employment opportunities for disadvantaged and under-represented groups.	Create a strategy which includes a description of project objectives, needs and targets, a demographic survey of the local region, description of roles and responsibilities, data collection tools and templates and reporting requirements.	CIL			No								
P33.2			Employment Opportunities Strategies	Direct 2% of project's contract value to generate employment opportunities for disadvantaged and under-represented groups either directly, through workforce targets, or indirectly, through social procurement.		CIL		2	No								Outside of the scope of this project.
P33.3			Exceptional Performance	Direct 4% of project's contract value to generate employment opportunities for disadvantaged and under-represented groups either directly, through workforce targets, or indirectly, through social procurement.		CIL		1	No								
P34.1	Design for Inclusion	The building is welcoming to a diverse population and is welcoming to their needs.	Accessible Navigation	The building is designed and constructed to be inclusive to a diverse range of people with different needs.	Ensure the building's design and construction must be able to be navigated and enjoyed by stakeholders of diverse ages, genders, and abilities (for example physical, sight, sound, mind, spectrum) including: • Equal access to the building: Provide equitable, appealing, safe, and secure access in a manner that does not segregate or stigmatise users through all principal entrance points and main thoroughfares inside and outside the building. • Diverse wayfinding: Introduce visual, physical, olfactory, and auditory solutions to help individuals navigate the site in a safe and enjoyable manner. and • Inclusive spaces: Introduce internal and external spaces for a diverse range of users, including parents, family restrooms, emergency rooms, quiet rooms and social interaction rooms. These rooms must be accessible to all users.	DOD		2	No					2		To be reviewed as design progresses. Recommended the project consider targeting.	
P34.2			Design for Dignity	Engagement with target groups has informed the inclusive design.	Ensure the design aligns with best practice guidelines, such as the Design for Dignity Guidelines: Principles for Beyond Compliance Accessibility in Urban Regeneration, building solutions that are expected to be included would be assistive technologies, emotional health spaces, acoustic treatments, adaptive strategies, gender, size, and physical appropriate facilities.		DOD		1	No							

Subiaco LRC Meeting - Development Management



Ref No.	Title	Aim of Credit	Sub-Element	Credit Criteria Summary	Design Team Actions	Key Stakeholder	PTS Compliance	Points Available	Minimum Compliance?	Net Zero Pathway	Base Design	Target Features	Stretch Targets	To Be Confirmed	Unlikely Points	Comments			
N35.1	Impacts to Nature	Ecological value is conserved and protected.	Impacts to Nature - Minimum Requirements		Ensure site preparation and construction works do not clear: - Old growth forest, - Prime agricultural land, - Any area within 100m of a wetland listed as being of 'High National Importance', - Aspects considered 'Matters of National Significance' listed under the Environmental Protection and Biodiversity Conservation Act (1999)	FDS			Yes		Met					No major issue for this project. Site is compliant.			
N35.2			Light Pollution to Neighbouring Bodies			Demonstrate that all outdoor lighting on the project complies with AS 4383 1:97 Control of the obtrusive effects of outdoor lighting.	Aurum		0	Yes		Met					Electrical consultant to design to avoid light spill and provide models in support.		
N35.3			Light Pollution to Night Sky				Demonstrate that no external luminaire on the project has a LOR that exceeds 5% relative to its actual mounted orientation or that the direct illuminance from external luminaires on the project produce a maximum initial point illuminance value no greater than 0.5 Lux to the Site Boundary and 0.1 Lux to 4.5m beyond the site into the night sky.	Aurum & Seedings		Yes		Met					Electrical consultant to design to avoid light spill and provide models in support.		
N35.4			Wetland Management Plan				Where the building is within 100m of a wetland, generate and make public a site-specific Wetland Management Plan. The plan must be prepared by a qualified ecologist or other qualified professional and include requirements for ongoing quarterly monitoring, annual reporting and management of the wetland ecosystem for a minimum of ten years.	CHL & Seedings		Yes		Met						Not applicable for this site.	
N35.5			Ecological Value				Demonstrate to current, future and past ecological values of the site are to be protected. Including assessment of local and regional threats and mitigation requirements and engaging with the local community and	CHL & Seedings		2	No							Project does not significantly address biodiversity requirements.	
N35.6			Diversity Protection				Where an ecologist has assessed the area as high ecological value the project must retain at least 50% of the site area and manage impacts of light and noise pollution, habitat connectivity, water quality, migration and two other local issues.	CHL & Seedings		2	No							Project is not considered high ecological value.	
N36.1	Biodiversity Enhancement	The building's landscape enhances the biodiversity of the site.	Landscape Area		Provide landscaping over 15% of the site area or 0.2% of the GFA - whichever is greater.	Seedings - Requires 1750m2 Landscaped Area			No								Excessive requirement for most projects.		
N36.2			Diversity of Species		The building's site includes an appropriate landscape area; The landscaping includes a diversity of species and prioritises the use of climate-resilient and indigenous plants; and The project team develops a site-specific Biodiversity Management Plan and provides it to the building owner or building name representative.	Seedings		2	No										
N36.3			Biodiversity Management Plan			An ecologist must assess and verify that the choice of landscaping and biodiversity is diverse and resilient to climate change impacts, thereby increasing the longevity of the landscape. An Ecologist must provide this narrative.	Seedings		No										
N36.4			Increased Landscape Area			Provide landscaping over 30% of the site area or 0.33% of the GFA - whichever is greater.	Seedings - Requires 1685m2 Landscaped Area			No									
N36.5			Increased Diversity of Species			A greater area of landscaping is provided; and The landscaping includes critically endangered and/or endangered plant species native to the bioregion.	Seedings		2	No									
N37.1a	Nature Connectivity	Wildlife movement is facilitated within and adjacent to the site	Landscaping		Provide landscape which is contiguous with existing, restored and new habitats. As a minimum requirement for habitat connectedness, the conservation area must make up at least 25% of the total external area within the building's site boundary to a minimum of 182m²	Seedings		2	No								Not feasible for this site.		
N37.1b			Infrastructure			Include design features such as a canopy bridge, wildlife tunnels, green roof, amphibian tunnels and green infrastructure are used to connect nature on site to adjacent natural areas.	Seedings & Colliers		No										
N38.1	Nature Stewardship	Biodiversity is restored beyond the building site.	Offsite Restoration		The building owner, as part of the project's development, undertakes activities that protect or restores biodiversity at scale beyond the development's boundary.	CHL		2	No								Could be considered by CHL.		
N39.1	Waterway Protection	Local waterways are protected, and the impacts of flooding and drought are reduced.	Run Off Volume		The development must demonstrate an annual average flow reduction (ML/y) of 40% compared to pre-development levels and meets specified pollutants targets.	Colliers		2	No		2								
N39.2			Water Pollution			Total Suspended Solids (TSS) 85% Crews Pollutants 90% Total Nitrogen 45% Total Phosphorus 65%	Colliers		No								Stormwater to be managed on site as far as practicable.		
N39.3			Run Off Volume			The development must demonstrate an annual average flow reduction (ML/y) of 80% compared to pre-development levels and meets specified pollutants targets.	Colliers		2	No		2							
N39.4			Water Pollution				Total Suspended Solids (TSS) 90% Crews Pollutants 85% Total Nitrogen 60% Total Phosphorus 70%	Colliers		No									

Subiaco LRC Meeting - Development Management



Ref No.	Title	Aim of Credit	Sub-Element	Credit Criteria Summary	Design Team Actions	Key Stakeholder	PTS Compliance	Points Available	Minimum Compliance?	Net Zero Pathway	Base Design	Target Features	Stretch Targets	To Be Confirmed	Unlikely Points	Comments
M40.1	Market Transformation	Celebrates initiatives or outcomes that are deemed new and break barriers, and in turn inspire others to follow.	Inclusive Construction Practices	The project is seeking to remove physical barriers to participation in the construction workforce for different groups, particularly women who represent less than 2% of the construction and building workforce	Women in construction represent more than 2% of construction and building workforce			1	No							Credits currently under review
M41.1			Net Zero	Where all Net Zero Pathway credits are met, a bonus credit is awarded	achieve all credits within the Zero Carbon Action Plan pathway			1	No			1				Required to be achieved for all 5-Star projects.
M41.2			Fossil Fuel Free Construction Site	The project minimises the use of fossil fuels, such as diesel, petrol or LPG, for powering construction machinery, equipment and site office during on-site construction activities. High Emitting Activities are defined as: - Excavation - Demolition - Earthworks - Concrete Pumping - Piling and Drilling - Generator - Cranes	The project has achieved Responsible Construction Credit <ul style="list-style-type: none"> • 20% of high emitting construction equipment on high emitting construction activities is fossil fuel free • The site office are powered by 100% renewable energy • All electricity used by the construction site is 100% renewable. 			1	No							
M41.3					<ul style="list-style-type: none"> • 50% of high emitting construction equipment on high emitting construction activities is fossil fuel free 			1	No							
M41.4					<ul style="list-style-type: none"> • 100% of all construction equipment is fossil fuel free 			1	No							
M41.5	Leadership Challenges	The project meets a Leadership Challenge developed by the GBCA			The building has both Best Practice Products and a high amount of Good Practice Products for structure, envelope, systems and/or finishes			1	No							
M41.6			Responsible Products	The building's products are comprised of responsibly manufactured products.	The building has both Best Practice Products and a high amount of Good Practice Products for structure, envelope, systems and/or finishes			1	No							
M41.7					The building has both Best Practice Products and a high amount of Good Practice Products for structure, envelope, systems and/or finishes			1	No							
M41.8					The building has both Best Practice Products and a high amount of Good Practice Products for structure, envelope, systems and/or finishes			1	No							
M41.9			Circular Economy	The project team has identified and implemented circular economy initiatives to increase the circularity of material(s)/product(s) within the supply chain(s) of the project.	<ul style="list-style-type: none"> • The project team identifies and implements circular economy principles and initiatives • The project team demonstrates an increased circularity of 10% (weighted by cost) 			2	No							
M41.10					The project team demonstrates an increased circularity of 20% (weighted by cost)			1	No							



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Commissioning Brief

Project:	CHL Subiaco
Service:	Independent Commissioning Agent
Subject:	Initial Commissioning Brief
Revision:	A
Date:	5 th January 2026
Author:	Graham Agar

Overview

FCDS have been commissioned to assist the CHL Subiaco team with commissioning management and agency as part of our role assisting the project to achieve an 'Australian Excellence' level, in comparison to the Green Star Buildings benchmarks. The role has been briefed to achieve compliance with the Green Star Verification and Handover elements for:

- Services and Maintainability Review
- Building Tuning
- Independent Commissioning Agent

The role also includes assistance with design documentation and construction phase services to assist with delivery of documentation at completion to support the Green Star As-Built certification.

The purpose of this brief is to inform the project team as to requirements and expectations for overall design practice and upcoming deliverables.

Service and Maintainability Review

FCDS will complete a detailed service and maintainability review for the project based on the DD (~60% complete) documentation. The review is an important part of the building sustainability strategy certification and will aim to address:

- Commission-ability
 - Can the proposed design be commissioned to demonstrate required performance.
 - Access, test points, key pre-requisites for commissioning
- Controllability
 - Will the systems be able to maintain stable operation.
 - Location of sensors / automation
 - Potential feedback loops
 - Clear definition of control requirements and interfaces
- Maintainability
 - Can the proposed design be maintained
 - Specialist products / contractors
 - Availability of spare parts / requirements for bespoke elements
- Operability
 - How will the proposed design be driven by occupants
 - Access to controls
 - Required training
- Safety

Ideally, FCDS will utilise documents generated by the design team for other deliverables, however, specific requirements / comments may be required. Where designers are providing reports, FCDS recommend including high level commentary against these requirements.



Project Timeline

Project Phase	FCDS Actions	Design Team Responsibilities
Schematic Design (~30%)	Provide Commissioning Brief and project team expectations	Brief / Reverse brief confirming: <ul style="list-style-type: none"> • System type • Design parameters • Safety Risks • Unusual maintenance requirements
Design Development (~60%)	Initial Services and Maintainability Review Draft Commissioning Specification	60% Design documentation SID Report Control intent / draft FCD
Contract / Building Licence Documentation (~80%)	Maintainability Report Commissioning and tuning requirements for design team documentation.	Documentation to address review commentary Sign off on Maintainability Report 80% Design documentation, including plans, schematics and specifications.
IFC (100%)	Commissioning and tuning specification.	Finalised construction documentation, with all relevant inclusions.

Codes and Standards

The project is required to commission to best practice international standards. FCDS recommend the use of the CIBSE commissioning codes as they provide coverage of all services:

- CIBSE Commissioning Code M (and ancillary codes for relevant services)
 - Code A – Air Distribution Systems
 - Code B – Boilers
 - Code C – Automatic Controls
 - Code L – Lighting
 - Code M – Management
 - Code R – Refrigeration
 - Code W – Water Distribution Systems

In addition, the building envelope is to be pressure tested to demonstrate a low level of leakage carried out by a member of the Air Tightness Testing and Measurement Association (ATTMA) or a testing member of the Air Infiltration and Ventilation Association of Australia (AIVAA) in accordance with AS/NZS ISO 9972:2015 Thermal performance of buildings - Determination of air permeability of buildings - Fan pressurization method.



Key Requirements

As the project is targeting a strong sustainability outcome, there will be an elevated requirement in terms of documentation and testing information at or prior to commissioning. FCDS note the following for comment and/or incorporation into documentation:

Testing Plans and Protocols

All contractors should be required to provide standard ITPs and commissioning method statements as part of the **shop drawing process**. This allows FCDS to review templates and confirm that they will meet Green Star and CIBSE requirements, potentially avoiding a lot of rework at practical completion.

All contractors should nominate a technical specialist to oversee commissioning of the various systems. This / these specialists will be a point of contact for the ICA and help to streamline the commissioning and handover process.

All contractors should advise major prerequisites for completion of their works as soon as practicable after appointment to ensure major commissioning milestones are tracked within the project program. For example:

- Mechanical
 - Power to mechanical boards
 - Building envelope completion
 - Quiet site for stair pressure testing
 - Completion of roof plant for air conditioning / circulation testing
- Metering
 - Power to site
 - Water to site
 - Communications infrastructure operational
 - Circuits / piping complete down stream

Commissioning Period – it is likely that the building will need at least a week following substantial completion to allow for testing of fire systems, building envelope, VOC and integrated systems building testing (Full function fire test, black out test). This should be highlighted within the project program.

Operation and Maintenance Manuals

Operation and Maintenance manuals make up more than 50% of the required documentation to justify the project level sustainability outcomes. Contractors are encouraged to provide draft O+Ms early within the CA phase to agree layout and general content. Consultants are requested to specify the following minimum inclusions – items in bold should be either dictated or closely reviewed by the designer:

- Summary sheet of relevant building service contacts
- **System-level information for all building systems**
- **Introduction and scope, including physical and functional descriptions**
- **Operating parameters** and procedures
- Preventive maintenance requirements, including procedures and schedules;
- Corrective maintenance requirements, including repair requirements;
- Service contacts, and any warranties and certificates.
- Up-to-date drawings incorporating at least:
 - Mechanical, electrical and hydraulic drawings and schematics covering all associated nominated building systems
 - Architectural, façade/building envelope drawings
 - Architectural layout of base building



Sustainability Outcomes

Based on the project Green Star approach, the following elements will need to be delivered by the various project trades. Consultants should ensure the design facilitates these outcomes as far as practical:

Architectural / Main Contractor

- Building Envelope Pressure Test
 - Covering at least:
 - All typical apartment types
 - All apartments at the highest level
 - All apartments at ground level
 - At least 10% of all apartments
 - Scheduled to permit witnessing
- Design to minimise penetrations to building envelope eg, maintain power points on internal walls only.
- Inspect penetrations during construction – risers, floor and roof penetrations.
- Detail responsibility for wall penetrations – sealing of plenum boxes etc.
- Onsite verification of:
 - Acoustic performance, including 'quiet site' to permit accurate measurement of noise levels due to installed systems and local environment
 - VOC content, including air quality measurement prior to occupancy
 - Requires completion of internal works prior to installation of tenant furniture etc.

Remote access and hydraulic / electrical metering interfaces are considered critical and shall be demonstrated to the satisfaction of the Superintendent's Representative prior to PC.

Similarly, all meters are to be provided with meter validation certificates, in accordance with NABERS validation protocols, prior to PC.

Mechanical

- Outside air flow rates for common areas
- Exhaust system flow rates
- Refrigerant content – species and quantity

Electrical

- Online availability / access to meter readings
- Meter and meter reading system validation for all water and electrical meters. All meters shall be provided with NABERS meter validation certificate prior to PC.
 - The contractor shall allow to provide load banks if needed to ensure sufficient usage to validate all meters.
- Lighting control functionality including:
 - Variable lux levels
 - Presence / absence detection and automated control of internal lighting
 - Automated control of external lighting

Hydraulic

- Water Metering System including shall be tested for accuracy, including completion of NABERS meter validation certification and connection to meter monitoring system. All meters shall be provided with NABERS meter validation certificate prior to PC.

Fire

In addition to all statutory tests, the contractor shall complete a fire system flow test to demonstrate >90% capture of pump flow test water on site.



Key Tuning Outcomes

FCDS note the following key commissioning / tuning outcomes for the project.

- Mechanical Capacity
 - Mechanical plant shall be tested in peak cooling and peak heating conditions. The mechanical contractor shall arrange for temperature logging in common areas on a peak day (maximum temperature over 37°C)
- Hot Water Capacity
 - Hot Water plant shall be tested in design heating conditions. The mechanical contractor shall arrange for temperature logging in common areas on a design winter day (minimum ambient <4°C).
- Solar Output
 - The solar system performance shall be trended over the year, with total annual output and monthly output shall be compared to model expectations

PTG/02566

Transport Impact Assessment Court Place - 154 Subiaco Road, Subiaco

28th January 2026 | Revision B
Prepared for Community Housing (Subi East) Limited

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REPORT DETAILS

Unique Document Identification

Document Title	Transport Impact Assessment – Court Place, 154 Subiaco Road, Subiaco
Project Number	PTG/02566
Document ID	TR-R001-B
Client	Community Housing (Subi East) Limited

PTG Consulting Office Details

Location	Perth, WA
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Revision Details

Revision No.	Date	Comments	Prepared By	Approved By
A	21/01/2026	For Issue	NA/LL	RJC
B	28/01/2026	Minor Update	LL	RJC

1 INTRODUCTION

1.1 Background

PTG Consulting Pty Ltd (PTG) has been commissioned by **Community Housing (Subi East) Limited** (“the Client”) to prepare a Traffic Impact Assessment (TIA) for the proposed mixed-use residential development located at Lot No.154 Subiaco Road, Subiaco, WA (the “Site”).

This report has been prepared in accordance with the Western Australian Planning Commission (WAPC) Transport Assessment Guidelines for Developments: Volume 4 - Individual Developments (2016) and the Transport Impact Assessment (TIA) Checklist is included at **Appendix A**.

Specifically, this report aims to assess the operations of the proposed development internally and its connections to the adjacent road network, with a focus on traffic volumes, parking, access and accessibility.

This report also outlines the requirements and opportunities associated with traffic and transport within the development, referencing relevant Council and WAPC policies and guidelines as well as best-practice planning within Western Australia.

2 EXISTING SITUATION

2.1 Regional Context

The Site is located at Lot No. 154 Subiaco Road within the City of Subiaco and is located around 3.0 kilometres northwest of Perth CBD as presented in [Figure 1](#).

Figure 1 Site Location



Source: Main Roads Road Information Mapping System and MetroMap

2.2 Existing Site Uses

The Site is currently a vacant lot.

2.3 Existing Parking and Demand

The Site has no existing parking and demand.

2.4 Existing Access Arrangements

The Site has no existing access arrangements.

2.5 Existing Site Traffic

The Site is currently empty and generates no vehicular trips.

2.6 Surrounding Land Uses

The Subi East Master Plan shown in [Figure 2](#) indicates that the land uses surrounding the Site will consist of residential developments, education, and community/recreation land uses.

Figure 2 Subi East Master Plan



Source: Subi East Master Plan

2.7 Existing Road Network and Traffic Management

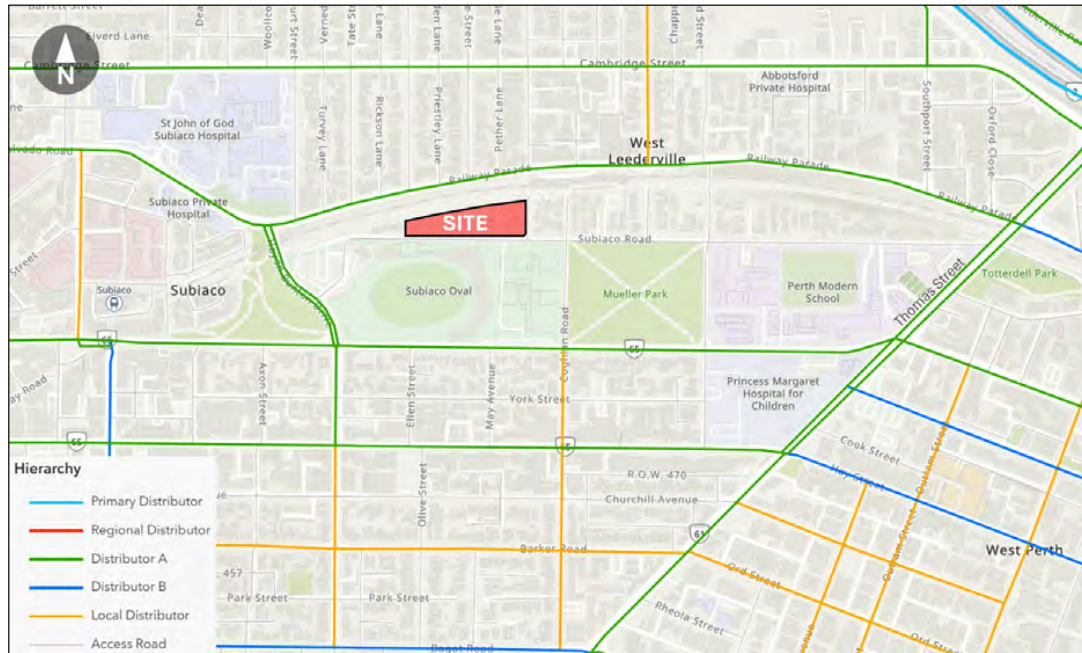
The road network within Western Australia is defined by Main Roads WA Road Hierarchy which describes the function, characteristic and management of each type of road. A description of each road type as per Main Roads WA Road Hierarchy criteria is summarised in [Table 1](#).

Table 1 Road Hierarchy Description

Road Type	Description
Primary Distributor	Provide for major regional and inter-regional traffic movement and carry large volumes of generally fast-moving traffic. Some are strategic freight routes, and all are State Roads. They are managed by Main Roads Western Australia.
District Distributor A	Carry traffic between industrial, commercial and residential areas and generally connect to Primary Distributors. These are likely to be truck routes and provide only limited access to adjoining property. They are managed by local government.
District Distributor B	Perform a similar function to type A District Distributors but with reduced capacity due to flow restrictions from access to and roadside parking alongside adjoining property. These are often older roads with a traffic demand in excess of that originally intended. District Distributor A and B roads run between land-use cells and generally not through them, forming a grid which would ideally space them around 1.5 kilometres apart. They are managed by local government.
Regional Distributor	Roads that are not Primary Distributors, but which link significant destinations and are designed for efficient movement of people and goods within and beyond regional areas. They are managed by local government.
Local Distributor (Urban)	Roads that carry traffic within a cell and link District Distributors or Regional Distributors at the boundary, to access roads. The route of Local Distributors should discourage through traffic so that the cell formed by the grid of District Distributors only carries traffic belonging to or serving the area. These roads should accommodate buses but discourage trucks. Urban Local Distributor roads are managed by local government.
Local Distributor (Rural)	Connect to other Rural Distributors and to Rural Access Roads. Not Regional Distributors, but which are designed for efficient movement of people and goods within regional areas. Rural Local Distributor roads are managed by local government.
Access Road	Provide access to abutting properties with amenity, safety and aesthetic aspects having priority over the vehicle movement function. These roads are bicycle and pedestrian friendly. They are managed by local government.

Figure 3 shows the road hierarchy network and Table 2 provides a summary of the road characteristics of the surrounding road network.

Figure 3 Road Hierarchy



Source: Main Roads WA Road Information Mapping System

Table 2 Existing Road Network

Road Name	Hierarchy	Jurisdiction	No. of Lanes	No. of Footpaths	Approximate Road Pavement Width (m)	Speed Limit
Thomas Street	Distributor A	Local Government	6	2	21.2 (including 1.2m median)	60 km/h
Railway Parade	Distributor A	Local Government	2	2*	8.6 (including 2m median)	50 km/h
Haydn Bunton Drive	Distributor A	Local Government	4	2	16.2 (including 1.8m median)	50 km/h
Subiaco Road	Access Road	Local Government	2	2	6.2	40 km/h
Court Place	Access Road	Local Government	2	2	6.4	40 km/h

*Path is discontinuous

Source: MRWA Road Information Mapping System & Metromap

2.8 Traffic Flows on Surrounding Roads

The existing traffic volumes of the roads surrounding the Site were sourced from Main Roads WA Traffic Map and are summarised in [Table 3](#).

Table 3 Existing Traffic Volumes

Road Name	Source	Date	AM Peak (vph)	PM Peak (vph)	Weekday (vpd)
Haydn Bunton Drive (south of Railway Parade)	SCATS	2023	336	686	5,204
Loftus Street (north of Railway Parade)	SCATS	2023	2,597	1,706	27,366
Railway Parade (east of Haydn Bunton Drive)	SCATS	2023	839	586	7,684
Railway Parade (west of Northwood Street)	Traffic Map	2023/2024	1,370	1,225	15,247
Railway Parade (west of Thomas Street)	SCATS	2023	297	326	5,023
Railway Parade (east of Thomas Street)	SCATS	2023	485	386	4,574
Salvado Road (west of Haydn Bunton Drive)	SCATS	2023	825	577	7,282
Subiaco Road (west of Thomas Street)	Traffic Map	2020/2021	136	179	1,092
Thomas Street (south of Railway Parade)	SCATS	2023	1,275	2,471	28,127

2.9 Operation of Surrounding Intersections

The nearest key intersection located approximately 250 metres from the Site is the Haydn Bunton Drive and Subiaco Road intersection. This is a left-in left-out give-way-controlled intersection as illustrated in [Figure 4](#).

Figure 4 Haydn Bunton Drive / Subiaco Road Intersection



Source: MetroMap

2.10 Existing Pedestrian/Cycle Networks

Figure 5 shows the existing pedestrian and cycling network as shown in MetroMap. Shared paths are present on both sides of Subiaco Road, along with raised pedestrian crossings. Pedestrians and cyclists may access West Leederville train station via Court Place (east of the development). A principal shared path also runs parallel to the rail line.

Figure 5 Existing Pedestrian/Cycling Network



Source: MetroMap

2.11 Existing Public Transport Services

Transperth Bus Routes 28, 95, 96 and 97 run in the surrounding area. Selected services of bus routes 95, 96 and 97 operate during school days only and traverse Roberts Road. The nearest bus stop to the proposed development is Robert Road Bob Hawke College Stand 1 (around 200 m). In addition to bus services, the area is also well-serviced by nearby train infrastructure. The combination of bus and train services provides flexible and reliable transport choices for daily commuting, school travel and access to key destinations across the metropolitan area.

Two Transperth train lines operate near the proposed development. Both the Fremantle Line and the Airport Line include a stop at West Leederville Station providing convenient access to a wider rail network for residents and visitors.

Table 4 and **Table 5** show the frequency of bus and train trips, respectively.

Table 4 Bus Route Service Frequency Table

Bus Route	Route Description	Service Frequencies		
		Weekdays	Saturdays	Sundays & Public Holidays
28	Perth - Claremont Station via Perth High Performance Centre	12 minutes (Peak Hour)		60 minutes
95	Glendalough Station - Subiaco Station via Harbourn Street	Two (2) AM trips* & Two (2) PM trips*		No Service
96	University of WA South - Leederville Station via QEII Medical Centre & Thomas Street	One (1) AM trip* & Two (2) PM trips*		No Service
97	University of WA South - Subiaco Station via QEII Medical Centre & Rokeby Road	One (1) PM trip only*		No Service

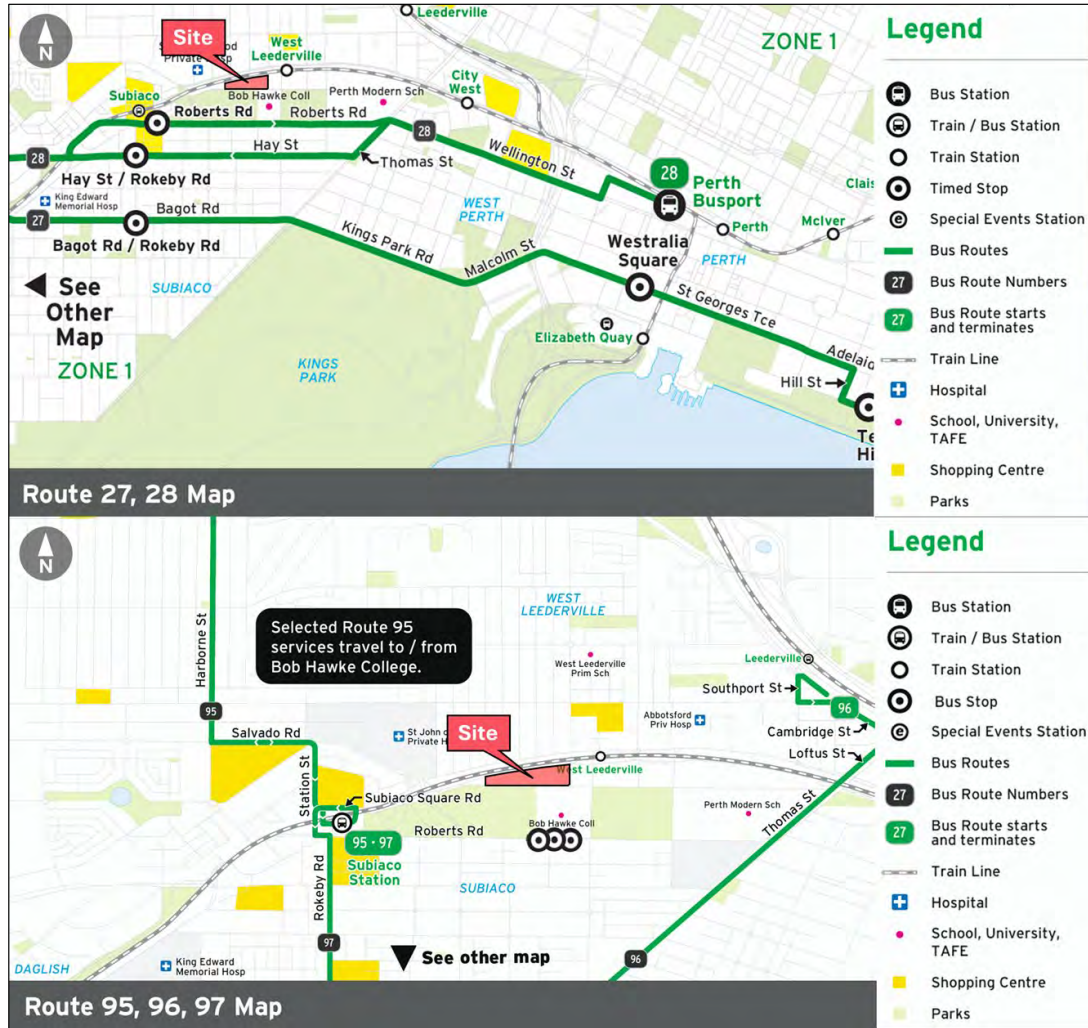
*Operates on school days only.

Table 5 Train Route Service Frequency Table

Train Route	Service Frequencies		
	Weekdays	Saturdays	Sundays & Public Holidays
Fremantle Line	10 minutes (Peak Hour)	10 minutes (Peak Hour)	10 minutes (Peak Hour)
Airport Line	12 minutes (Peak Hour)	12 minutes (Peak Hour)	12 minutes (Peak Hour)

Figure 6 shows the bus and train routes and the bus stop location at West Leederville.

Figure 6 Bus and Train Routes Map

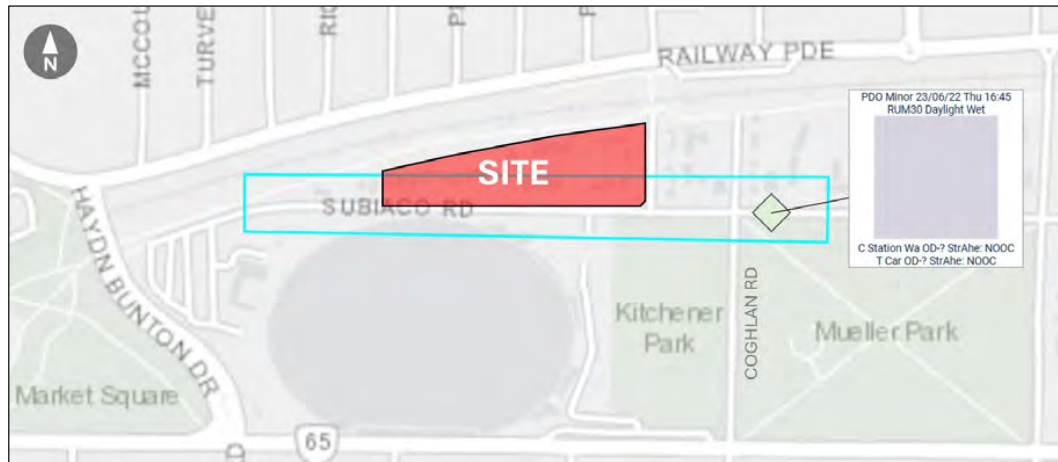


Source: Transperth

2.12 Crash Analysis

A crash assessment for the surrounding road network of the Site has been completed using Main Roads WA Reporting Centre. The assessment covers all the recorded crashes for the 5-year period between 1 January 2020 to 31 December 2024.

Figure 7 Crash Location



Source: MRWA Crash Map

Crash data is summarised as follows:

- » A total of one (1) crash was recorded within the vicinity of the Site, with no fatal crashes recorded.
- » One (1) recorded crash resulted in minor property damage.

Overall, based on the crash data, it is not expected that the proposed property will have an impact on road safety in the area. However, it is important to note that the road geometry of Subiaco Road has undergone significant changes during the assessment period. These modifications may have altered traffic flow, driver behaviour, and visibility conditions. As a result, the historic crash data may no longer accurately reflect the current or future road environment.

3 PROPOSED DEVELOPMENT

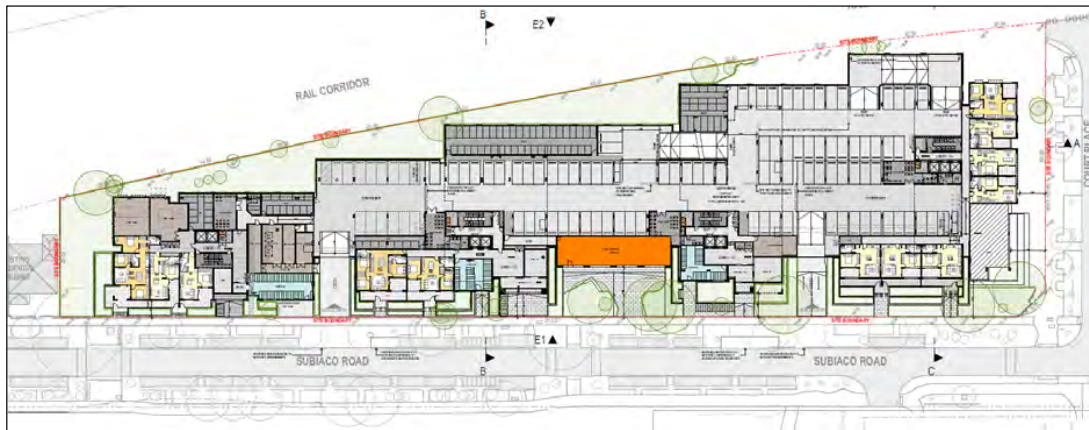
3.1 Proposed Land Uses

The proposed development is a mixed-use residential development with the following breakdown, and the site plan is shown in

Figure 8:

- » 447 dwelling units
 - 1-bedroom: 288 dwelling units
 - 2-bedroom: 139 dwelling units
 - 3-bedroom: 20 dwelling units
- » 352 m² Commercial/ Retail

Figure 8 Proposed Site Plan

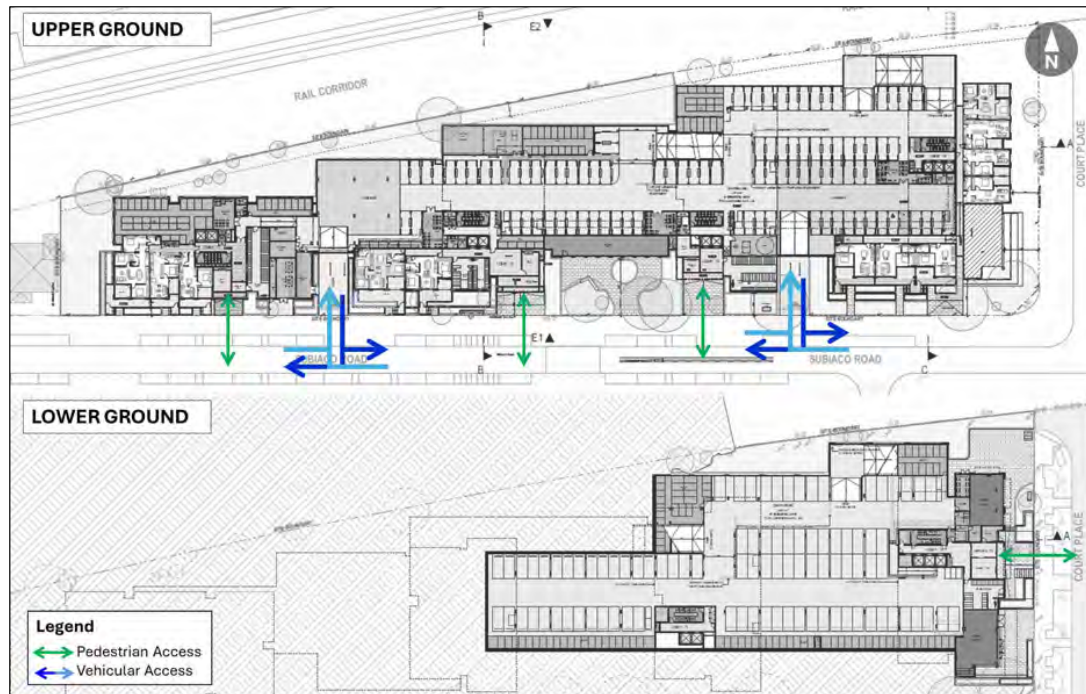


Source: DKO Architecture (WA) Pty Ltd. (January 2025)

3.2 Access Arrangements

Figure 9 shows two full-movement vehicle access points proposed to be located along Subiaco Road and three pedestrian access points along Subiaco Road to reach the upper ground level. Another pedestrian entry point is located along Court Place to access the lower ground level. Based on the proposed land use, the types of vehicles expected to access the development include passenger cars and waste trucks/ occasional delivery vehicles.

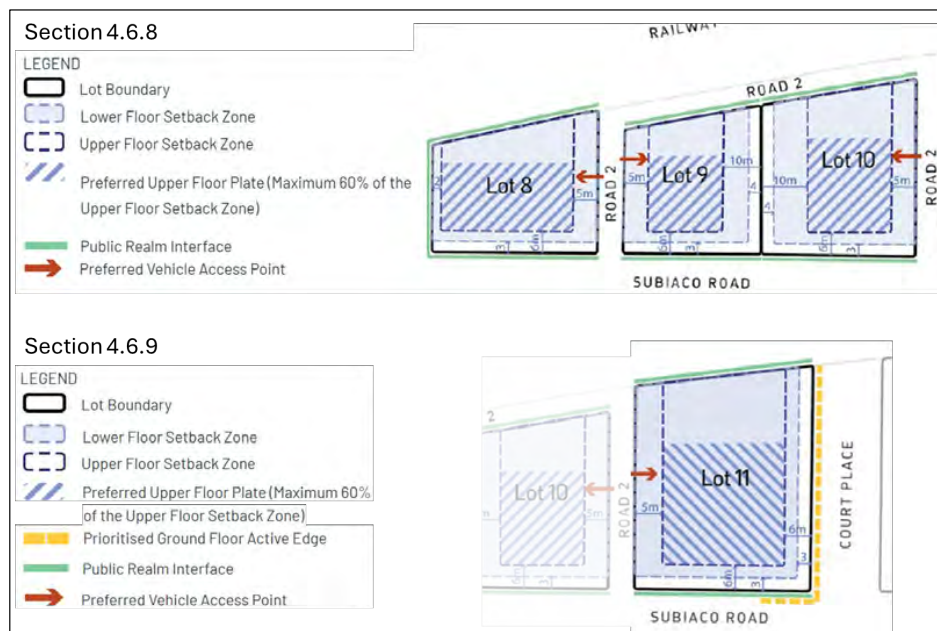
Figure 9 Proposed Access Points



Source: DKO Architecture (WA) Pty Ltd.

According to the Subiaco Oval and Railway Precinct Design Guidelines Section 4.6.8, crossovers for Lots 8-10 from Subiaco Road are not permitted for these four lots and all vehicle access should be from Road 2. The preferred vehicle access points as per the guidelines are shown in **Figure 10**.

Figure 10 Design Guidelines for Lots 8-11



Source: Subiaco Oval and Railway Precinct Design Guidelines

A two-way vehicle crossover (Road 2) is proposed to connect to Subiaco Road, providing access to the underground car park serving the proposed lots.

3.3 Swept Path

3.3.1 B85 and B99 Vehicles

Swept path assessment has been conducted with B85 and B99 design vehicles near the access points and within the ramps and internal roadways, as shown in [Figure 11](#) to [Figure 13](#). The swept path diagrams demonstrate that these design vehicles are able to adequately enter and exit the proposed Site without any encroachments. Slight overlapping between vehicle paths was observed near ramp entrances. It is recommended that traffic management measures such as convex mirrors be installed near ramp corners so that appropriate visibility can be achieved. Kerb/line markings of the parking spaces nearest to the ramps should also be modified to accommodate vehicle turns.

Vehicle path overlapping near ramp entrance are considered acceptable given the directional nature of residential traffic movements during peak periods. During AM peak, vehicle movements will predominantly be outbound as residents leave for work and other activities. Conversely, inbound movements during this period will be minimal.

During the PM peak hour, the reverse travel pattern applies, with vehicle movements predominantly inbound as residents return to the development. Outbound movements during this period are generally limited. This directional split significantly reduces the likelihood of conflicting inbound and outbound movements occurring simultaneously near the ramp entrance. Furthermore, give way line marking is proposed throughout the car park at strategic locations to improve movement and circulation at key junctions.

Figure 11 Swept Path Analysis - Lower Ground

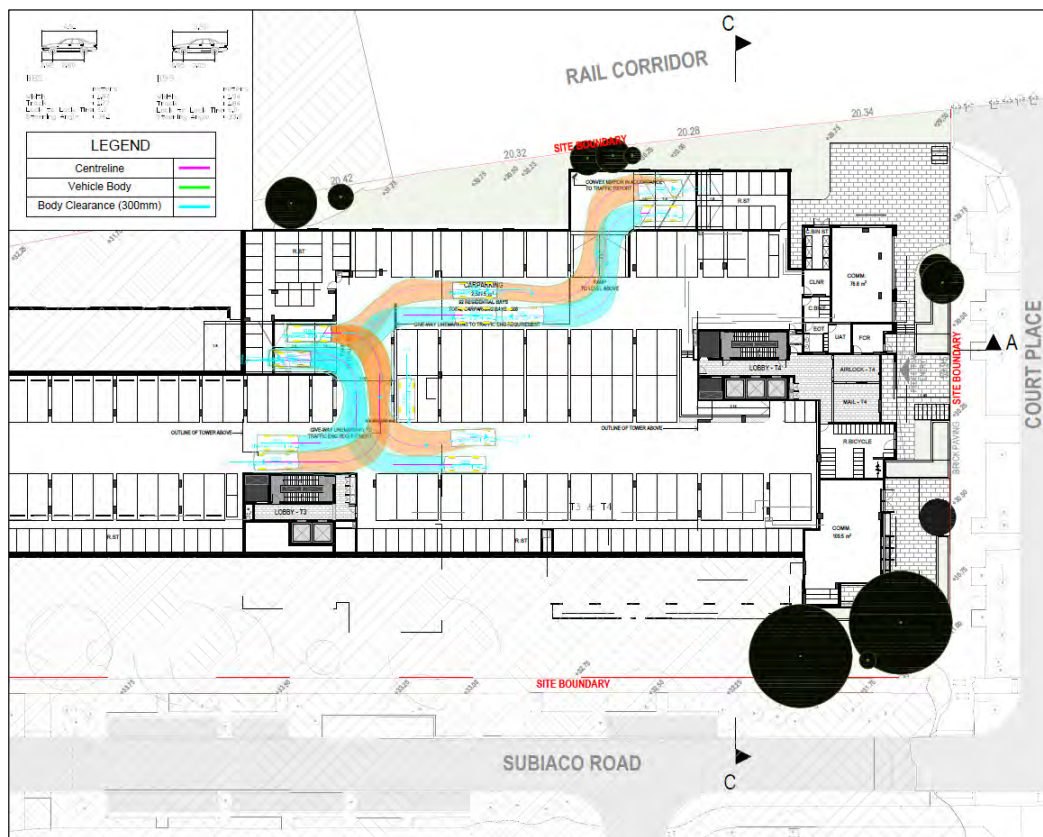


Figure 12 Swept Path Analysis - Upper Ground

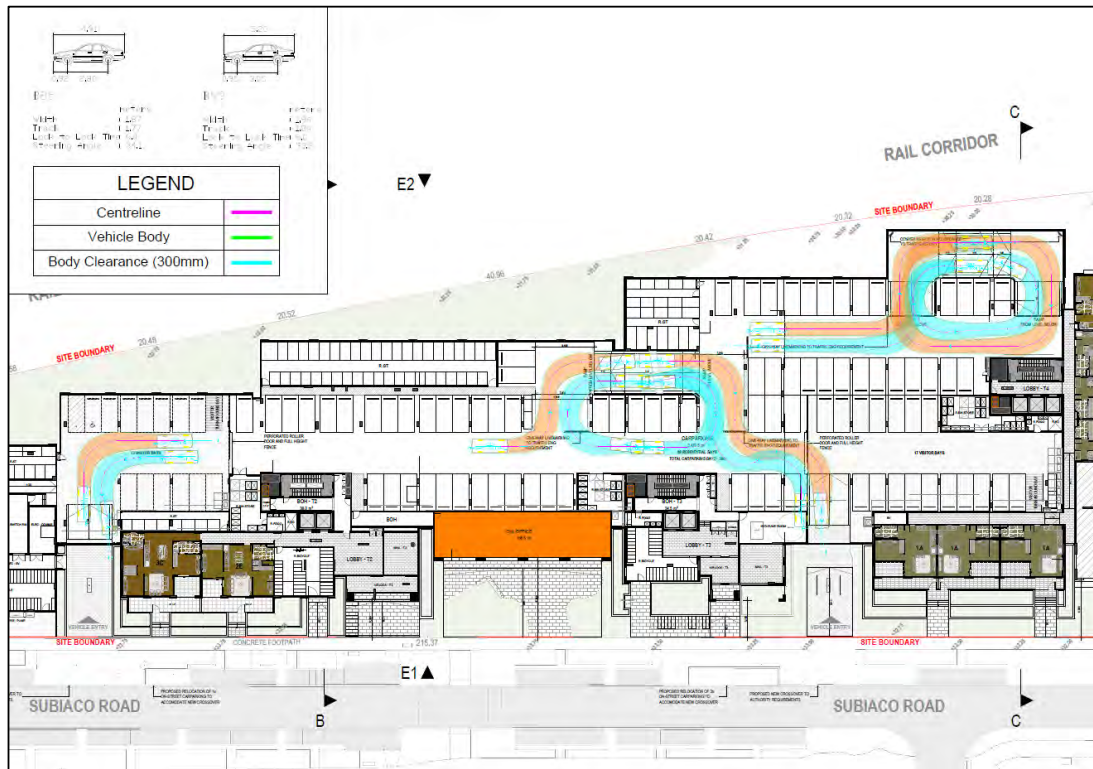
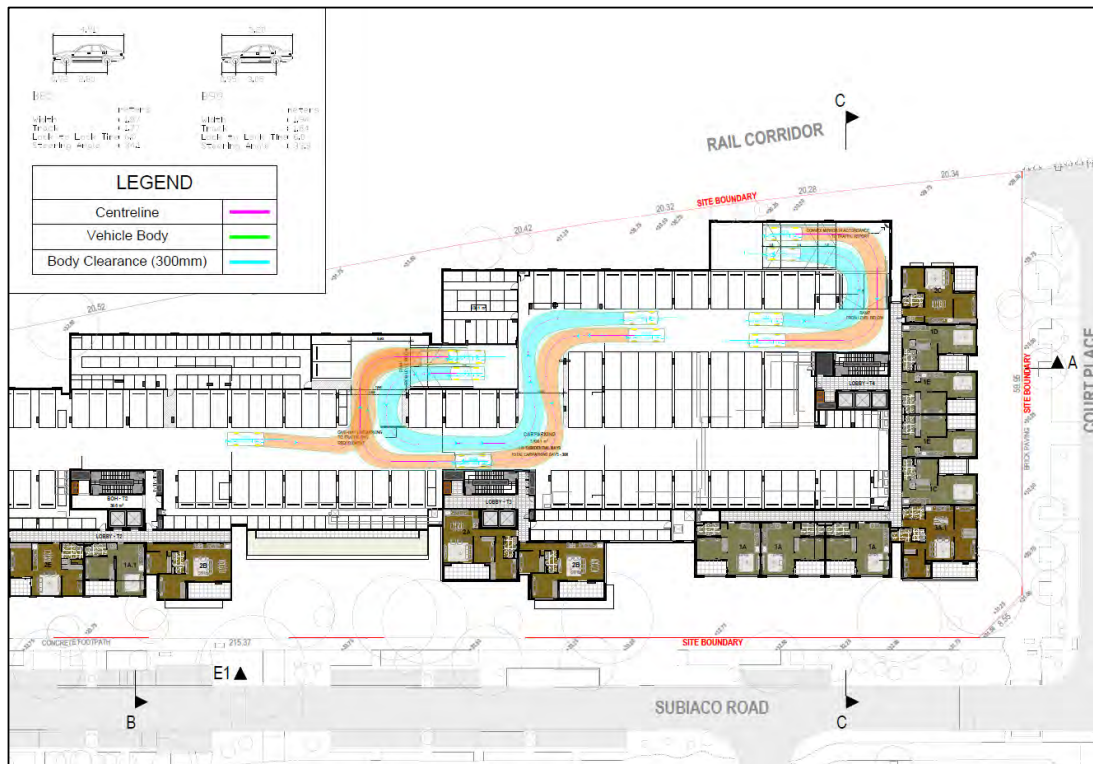


Figure 13 Swept Path Analysis - Level 01



3.3.2 Service Vehicle (Waste Truck)

A swept path assessment was undertaken for the 6.9 m waste truck and the waste collection procedures for Tower 1 to 4 and the commercial tenancy is illustrated in the figures below.

Figure 14 shows the waste truck route and collection points for Tower 2, 3 & 4. The waste collection vehicle will enter the Site via the eastern access and reverse into the collection area to service the residential waste for Tower 4. Upon completion, the vehicle will proceed forward to the collection area for Tower 3 to collect residential waste and then continue forward again to service Tower 2. After completing waste collection for Towers 2, 3 and 4, the vehicle will exit the Site in a forward direction via the western access. **Figure 15** shows the swept path.

Figure 14 Waste Truck Route and Collection Points – Tower 2,3 & 4

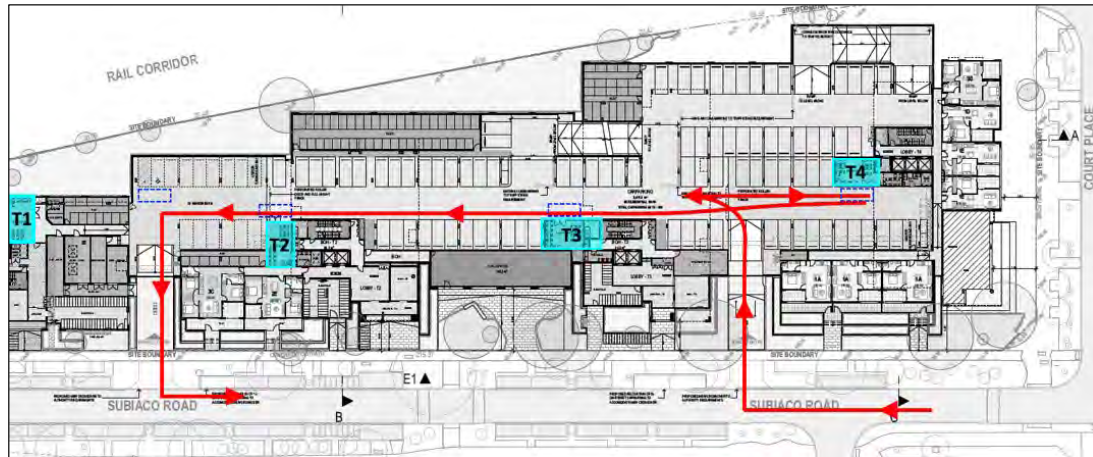
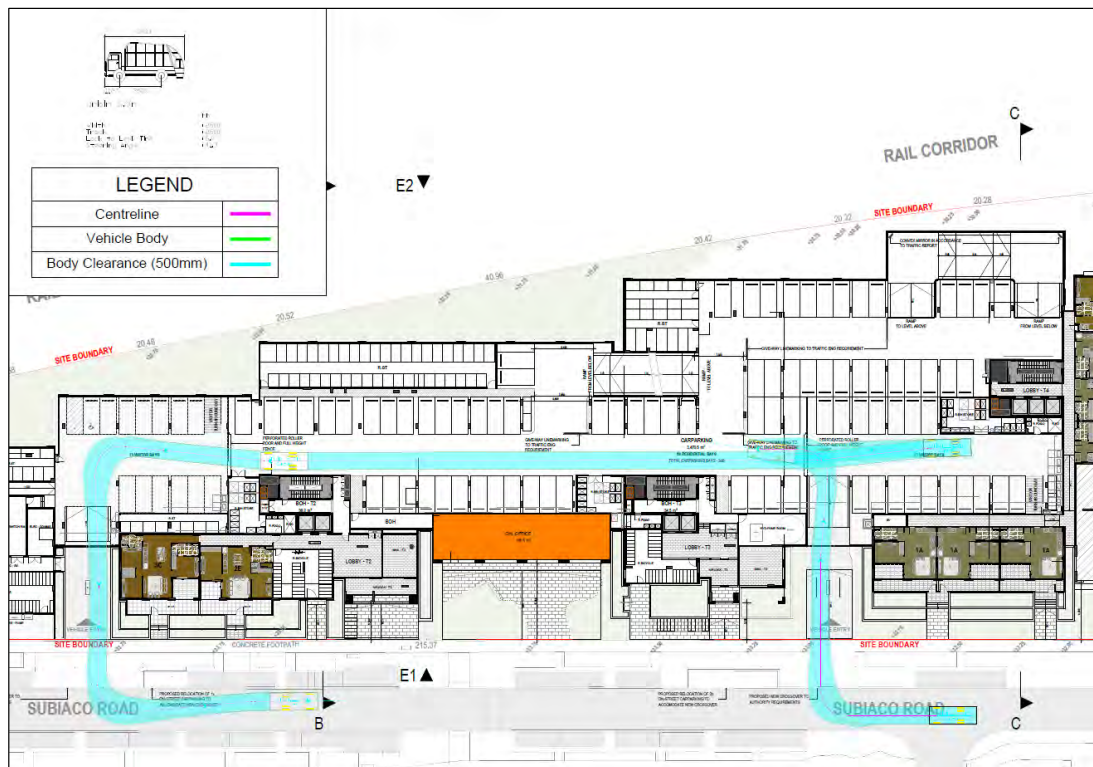


Figure 15 Collection of Residential Wastes at Tower 2, Tower 3 and Tower 4



Upon exiting the Site, the service vehicle will perform a turnaround at Court Place and travel back to Subiaco Road and re-enter the Site via the western crossover to undertake waste collection from Tower 1 as shown in [Figure 16](#). The waste truck enters the Site in forward gear and reverses into the collection area to collect the wastes from Tower 1, then proceeds forward to exit through the eastern crossover. [Figure 17](#) shows the swept path for Tower 1 waste collection and [Figure 18](#) shows the commercial waste collection from Court Place.

Figure 16 Waste Truck Route and Collection Points – Tower 1



Figure 17 Collection of Residential Wastes at Tower 1

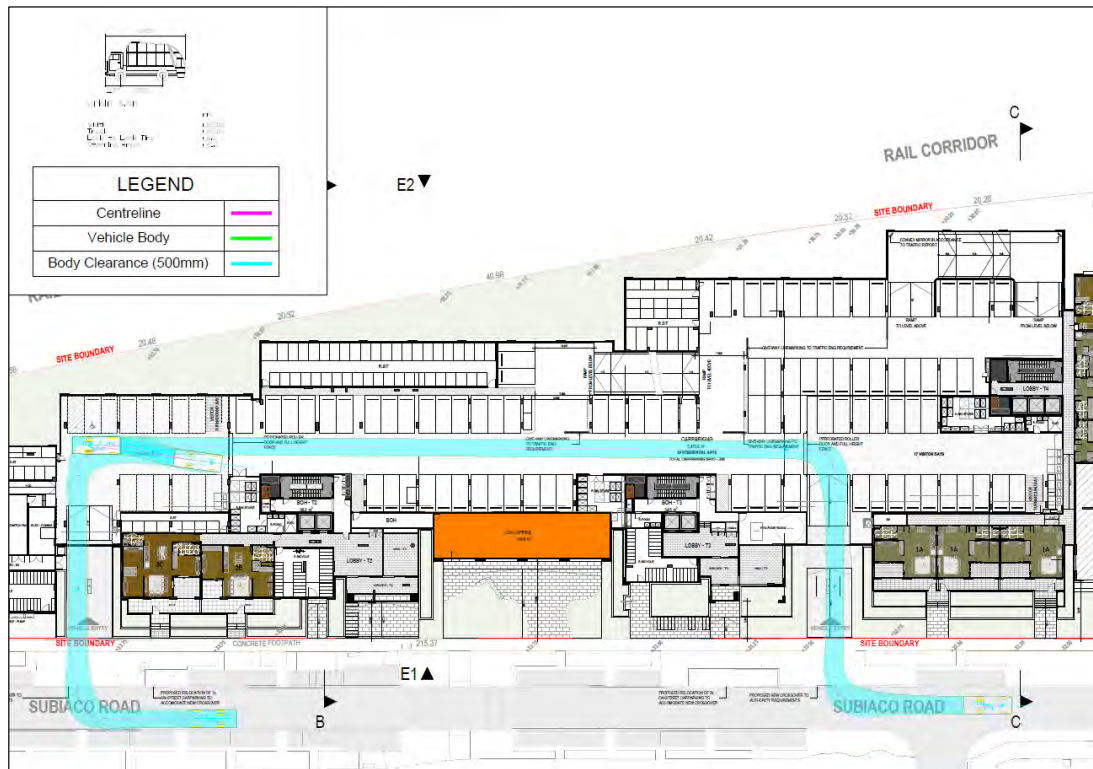
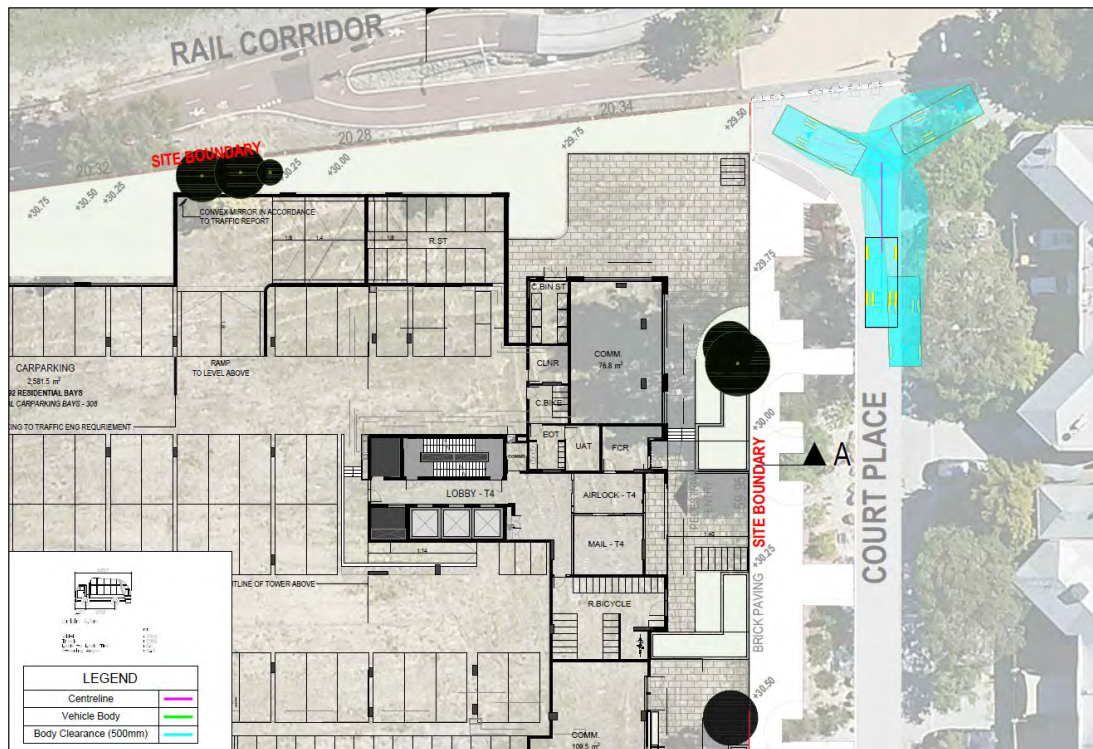


Figure 18 Collection of Commercial Wastes along Court Place



3.4 Parking Compliance

The provided parking facilities in the Site should be designed to be consistent with the requirements of AS2890.1 for User Class 1A. [Table 6](#) outlines the parking compliance assessment findings for the proposed parking area within the Site and is found to be compliant with the standards.

Table 6 Parking Compliance

Parameter	Minimum Requirements	Site Plan	Remarks
90-Degree Parking Bay Width, m	2.4	2.4	No non compliances identified
90-Degree Parking Bay Length, m	5.4	5.4	No non compliances identified
Aisle Width, m	5.8 (90-Degree Parking)	5.9	No non compliances identified
Ramp Grade	Up to 20 m long: 1 in 4 (25%) maximum	1:4 (25%) maximum	No non compliances identified
Circulation Roadway Width, m	5.5 (two-way)	5.5 (two-way)	No non compliances identified
Access Driveway Width, m	Entry & exit width (combined): 6.0 to 9.0	6.5	No non compliances identified
Blind Aisle Extension, m	1.0	1.0	No non compliances identified

3.5 Parking Provision

3.5.1 Car Parking

The statutory car parking requirement as per *The Subiaco Oval and Railway Precinct Design Guidelines* recommends using the parking ratios set out in R-Codes Volume 2. For non-residential land uses, the car parking requirements are assessed in accordance with the *Subiaco Oval and Railway Precinct Design Guidelines* (AO 2.11.2).

A summary of the parking requirements and provisions of the proposed development is detailed in [Table 7](#). A shortfall of 98 residential parking bays and 32 residential visitor bays has been identified in accordance with the relevant guidelines. However, the subject site is located in a highly accessible area with convenient access to public transport, including a nearby train station and high-frequency bus services operating along Roberts Road and the surrounding road network. In addition, peak demand for residential visitor parking is anticipated to predominantly occur during the evening period, at which time a level of reciprocal use of the commercial parking bays is expected.

The site is also well served by an established walking and cycling network. This includes shared paths on both sides of Subiaco Road, as well as a principal shared path that runs parallel to the railway line, providing direct and continuous connections to key destinations in the area.

Having convenient access to these facilities incentivises more people to walk and cycle and help reduce reliance on vehicular travel and thereby reducing the demand for parking.

The Development WA guidelines also seeks to take a minimalist approach to car parking in recognition to areas with close proximity to high frequency rail and bus services and high-quality cycle paths.

Table 7 Car Parking Requirements & Provision

Land Use	Reference	Yield	Car Parking Rates	Car Parking Requirements (bays)		Car Parking Provision (bays)
1 bedroom dwelling	R Codes	288 dwelling units	0.75 bay per dwelling	216	375	277
2 bedroom dwelling		139 dwelling units	1 bay per dwelling	139		
3+ bedroom dwelling		20 dwelling units	1 bay per dwelling	20		
Visitors		447 dwelling units	1 bay per 4 dwellings up to 12 dwellings 1 bay per eight dwellings for the 13 th dwelling and above	58		30
Retail (Non-residential)	AO 2.11.2	352 m ²	1 bay per 100 m ² NLA	4		
Total				437		307

3.5.2 Bicycle Parking

The bicycle parking rates are provided in accordance with *The Subiaco Oval and Railway Precinct Design Guidelines*. The proposed development plans incorporate bicycle storage for residential units, visitors and commercial tenancies. The bicycle parking requirements and provisions are presented in [Table 8](#).

Table 8 Bicycle Parking Requirements & Provision

Land Use	Reference	Yield	Bicycle Parking Rates	Bicycle Parking Requirements (bays)	Bicycle Parking Provision
Residential	AO 2.12.1	447 dwelling units	1 per dwelling	447	143
Visitors		447 dwelling units	1 per 10 dwelling	45	48
Retail (Non-residential)	AO 2.12.1	352 m ²	1 bay per 100 m ² NLA	4	14
Total				496	205

While a shortfall in residential bicycle parking provision has been identified, the availability of nearby public transport is a significantly more attractive and preferred mode of travel for many occupants.

CHL is the largest community housing provider in Australia, managing a portfolio of more than 12,000 properties nationwide. Based on their extensive operational experience across a diverse range of sites, CHL has consistently observed that on-site bicycle storage facilities are typically underutilised and, in many cases, largely remain vacant.

3.5.3 Motorcycle Parking

The requirement for motorcycle/scooter parking as per the City of Subiaco Local Planning Scheme (LPS) No. 5 is shown in [Table 9](#). A shortfall of 6 motorcycle bays has been identified and this reduced provision is not expected to present any significant issues given the site's proximity to excellent public transport facilities.

Table 9 Motorcycle Parking Requirements

Land Use	Reference	Yield	Motorcycle Parking Rates	Motorcycle Parking Requirements	Motorcycle Parking Provision
Residential	City of Subiaco LPS 5	307 car parking bays	1 motorcycle/ scooter bay per 10 car bays for developments exceeding 20 dwellings	31 motorcycle bays	25 motorcycle bays

3.6 End of Trip Facilities

In addition to meeting the bicycle parking requirements, the proposed development must also provide End-of-Trip (EoT) facilities such as lockers. [Table 10](#) details the EoT requirements for the proposed development. These facilities will primarily be used by tenants and workers on Site.

Table 10 End of Trip Facilities Requirements

Use	Reference	Yield	End-of-Trip Facilities Requirements	
Lockers	AO 2.11.2	4 bicycle bays	1.5 lockers for every non-residential bicycle bay	6 lockers

The *Subiaco Oval and Railway Precinct Design* Guidelines also mentions that one (1) unisex shower and change room must be provided for developments with less than 10 non-residential bicycle parking spaces.

3.7 Specific Issues

No specific issues were identified for the Site.

4 CHANGES TO SURROUNDING TRANSPORT NETWORKS

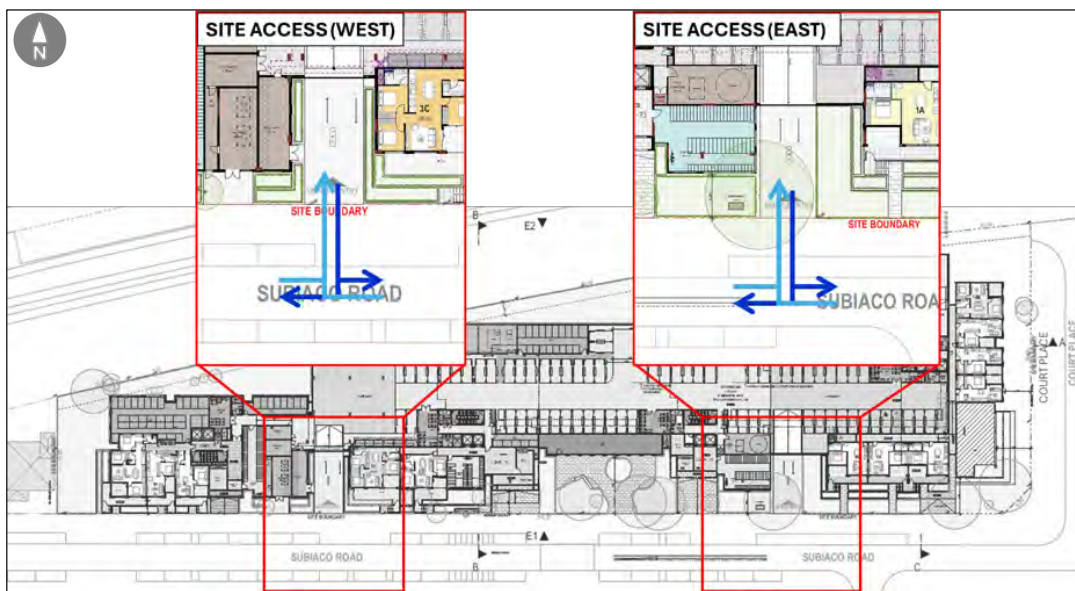
4.1 Road Network

The proposed development does not include any major changes to the surrounding road network. As part of the Subi East Redevelopment Master Plan, Subiaco Road will be designed as a boulevard with street level activation and mature tree coverage. Subiaco Road has also been recently extended to intersect with Haydn Bunton Drive to improve traffic flow within Subi East.

4.2 Intersection Layouts and Controls

Figure 19 presents the proposed two vehicular accesses to the Site from Subiaco Road, both uncontrolled intersections with full turning movements.

Figure 19 Subiaco Road / Site Access (West) & Subiaco Road / Site Access (East) Intersections

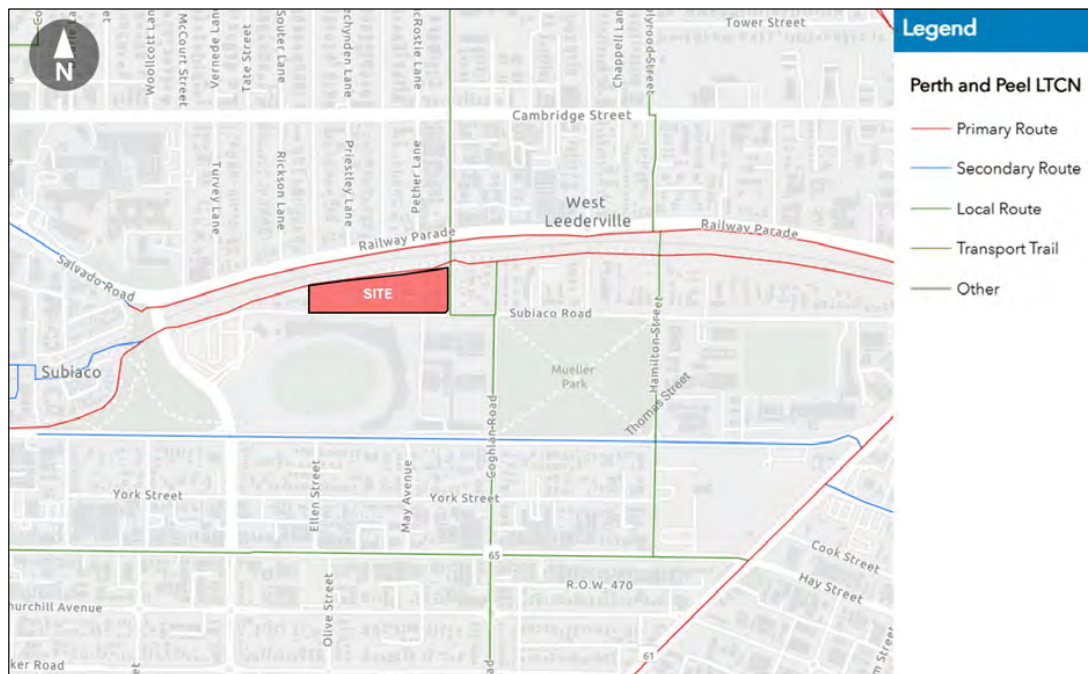


4.3 Pedestrian/Cycle Networks and Crossing Facilities

Figure 20 illustrates the proposed pedestrian and cycling infrastructure as identified in the Long-Term Cycle Network (LTCN) Map. According to the LTCN classification, both Railway Parade and Thomas Street have been designated as Primary Routes and a section of Subiaco Road has been classified as a Local Route.

Moreover, the Subi East Master Plan envisions upgrading Subiaco Road into a pedestrian and cycle-friendly street, with a shaded boulevard of mature trees for pedestrians and cyclists moving through the area.

Figure 20 Proposed Pedestrian/Cycling Network



Source: Perth and Peel LTCN

4.4 Public Transport Services

PTG contacted Public Transport Authority and was informed that there are no proposed changes in the short-term. However, there are medium to long-term changes to the existing Bus Route 28 service. At present, due to the one-way street system, buses operate along Roberts Road eastbound and Hay Street westbound. Once the one-way system is removed by the cities of Perth (east of Thomas) and Subiaco (west of Thomas), the PTA will realign bus services along Hay Street to two-way. As a result, bus services will no longer regularly operate along Roberts Street. Bus services currently serving Bob Hawke College will continue to do so regardless of the two-way street's changes, meaning that the PTA would require Roberts Road to remain trafficable for bus services.

5 INTERGRATION WITH SURROUNDING AREA

5.1 Surrounding Major Attractors/Generators

The surrounding major attractors and generators of the proposed development include:

- » Subiaco Oval and Mueller Park to the south of the Site;
- » Commercial and retail development to the west (Subiaco Square);
- » Perth CBD approximately 3km southeast of the Site;
- » Education facilities (Perth Modern School to the east & Bob Hawke College to the south); and
- » St. John of God Hospital to the north of the Site.

Figure 21 Surrounding Major Attractors/Generators



5.2 Committed Development and Transport Proposals

Various developments including residential, mixed-use, commercial and community uses have been planned as part of the East Subi Redevelopment Master Plan, as shown in [Figure 22](#).

Figure 22 Land Use and Urban Form - Subi East Master Plan



Source: Subi East Master Plan

5.3 Proposed Changes to Land Uses within 1200 metres

There are no major changes to land uses within 1200 metres of the Site.

5.4 Travel Desire Lines from Development to the Attractors/Generators

Travel desire lines from the Site will predominantly be directed north, east and south towards the external attractors and generators.

For nearby attractors such as train stations and Subiaco Square, pedestrian and cyclists are likely to travel along the footpaths on Railway Parade and Haydn Bunton Drive.

5.5 Adequacy and Deficiencies of the External Transport Networks

The existing transport network is deemed suitable and sufficient to accommodate the proposed development.

6 ANALYSIS OF TRANSPORT NETWORK

6.1 Assessment Years and Time Periods

In accordance with the WAPC Transport Impact Assessment Guidelines, assessment has been undertaken for the following years:

- » Year of full opening of the development: 2028
- » 10 years after full opening of the development: 2038

The identified peak hours, based on existing traffic flows, are as follows:

- » AM Peak: 08:00-09:00
- » PM Peak: 16:45-17:45

6.2 Development Generated Traffic

The traffic generation for the Site has been calculated based on the proposed land uses stated in [Table 11](#). The trip generation calculations are summarised in [Table 12](#). Due to the Site's excellent location near the City, rail station and cycle facilities, the trip estimate is likely to be an over-estimate.

Table 11 Trip Generation and Distribution Rates

Land Use	Reference	Trip Generation Rates		Trip Distribution Rates			
		AM Peak	PM Peak	AM (IN)	AM (OUT)	PM (IN)	PM (OUT)
Residential (<3 beds)	RTA v2.2	0.5 per dwelling	0.5 per dwelling	25%	75%	62.5%	37.5%
Residential (3+ beds)	RTA v2.2	0.65 per dwelling	0.65 per dwelling	25%	75%	62.5%	37.5%
Retail (Mixed)	WAPC	2.5 per 100m ²	5.0 per 100m ²	80%	20%	50%	50%

Table 12 Total Trip Generation

Land Use	Yield	AM Peak		PM Peak	
		IN	OUT	IN	OUT
Residential (<3 beds)	427 dwellings	53	160	133	80
Residential (3+ beds)	20 dwellings	3	10	8	5
Retail (Mixed)	351.8 m ²	7	2	9	9
Total		236		244	

The volume of traffic generated by the proposed development is estimated to be 236 trips in the AM peak and 244 trips in the PM peak.

6.3 Distribution of Generated Traffic

The distribution of the generated trips was based on the existing traffic count data at Subiaco Road and was assumed to be 68% to/from the east and 32% to/from the west as illustrated in [Figure 23](#).

[Table 13](#) details the trip distribution results.

Figure 23 Distribution of Generated Traffic



Table 13 Trip Distribution

AM Peak				PM Peak			
IN		OUT		IN		OUT	
From East	From West	To East	To West	From East	From West	To East	To West
44	20	117	55	102	48	64	30

6.4 Background and 'With Development' Traffic Flows

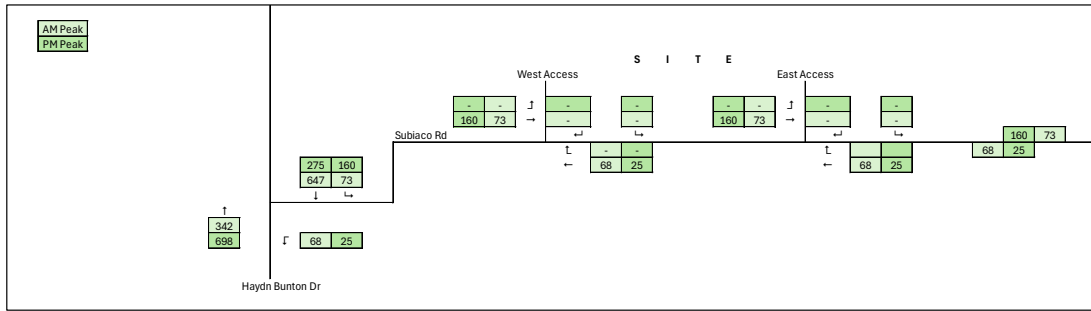
6.4.1 Background Traffic

Background traffic volumes and turning movements at key intersections were gathered from Main Roads WA Traffic Map and SCATS data at signalised intersection. The existing background peak hour traffic data in the surrounding road network is provided in [Figure 24](#).

The historical traffic data in the vicinity of the Site suggest that there was minimal growth to the surrounding road network in the previous years. However, the assessment also considers the potential traffic growth, particularly along Subiaco Road as the East Subi redevelopments take place. For the purpose of this assessment:

- » 0.5% per annum growth rate has been applied from existing data to opening year (2028).
- » 2.0% per annum growth rate has been applied from 2028 (opening) to 2038 (10-year horizon)

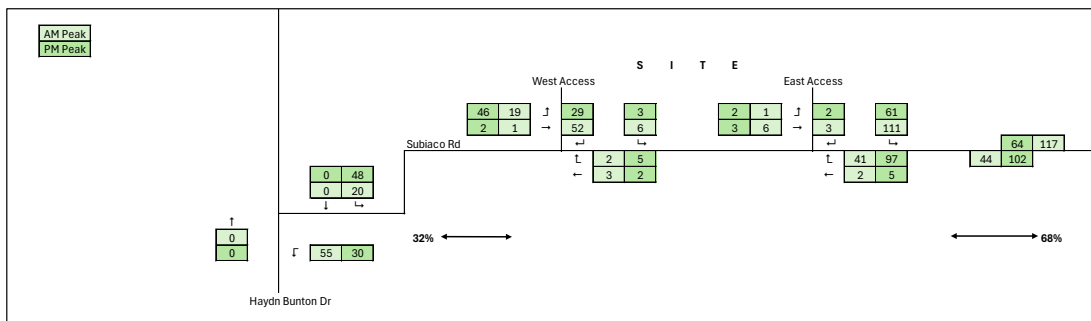
Figure 24 Background Traffic Peak Hour Flow (2026)



6.4.2 Development Traffic

The development traffic peak hour flows illustrated in [Figure 25](#) has been developed based on the traffic distribution discussed in [Section 6.3](#).

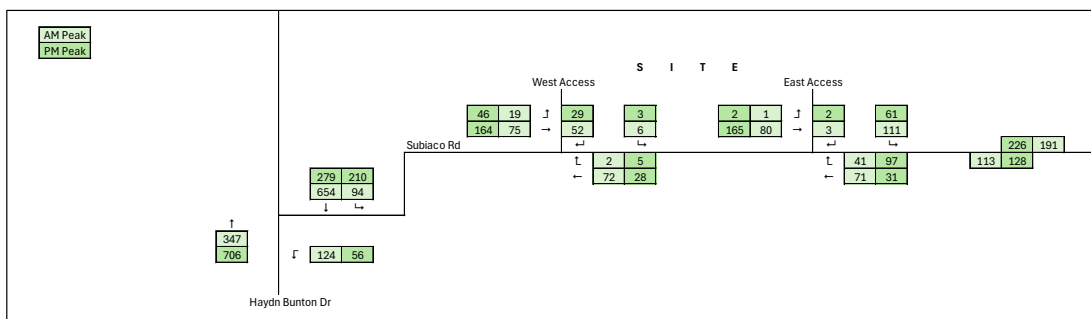
Figure 25 Development Traffic Peak Hour Flow



6.4.3 Opening Year Traffic (2028)

[Figure 26](#) presents traffic flows for the opening year, combining background volumes with trips generated by the proposed development.

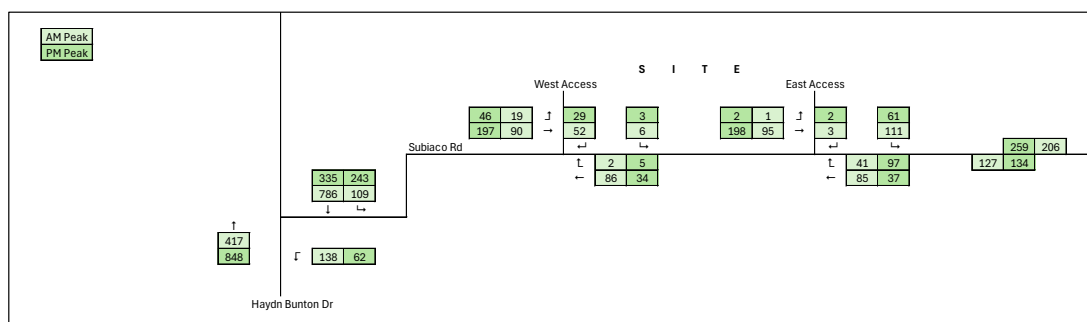
Figure 26 Opening Year, 2028 (Background + Development Traffic)



6.4.4 10-Year Horizon Traffic (2038)

Shown in **Figure 27** are the traffic flows 10 years after the full opening of the development, which include existing background traffic and development traffic.

Figure 27 10-year Horizon, 2038 (Background + Development Traffic)



6.5 Performance Criteria

The intersections have been analysed using the SIDRA analysis program. This program calculates the performance of intersections based on input parameters, including geometry and traffic volumes. As an output, SIDRA provides values for the Degree of Saturation (DOS), queue lengths, delays, level of service, and 95th Percentile Queue. These parameters are defined as follows:

- » Degree of Saturation (DOS): is the ratio of the arrival traffic flow to the capacity of the approach during the same period. The Degree of Saturation ranges from close to zero for varied traffic flow, up to one for saturated flow or capacity.
- » 95% Percentile Queue Length: is the statistical estimate of the queue length up to or below which 95% of all observed queues would be expected.
- » Average Delay: is the average of all travel time delays for vehicles through the intersection.
- » Level of Service (LOS): is the qualitative measure describing operational conditions within a traffic stream and the perception by motorists and/or passengers. [Table 14](#) provides a description of the different levels of service.

Table 14 Level of Service (LoS) Performance Criteria

LOS	Description	Signalised Intersection	Roundabout	Unsignalised Intersection
A	No or minimal delays (best condition). Queues are rarely more than one vehicle	≤10 sec	≤10 sec	≤10 sec
B	Short traffic delays. Occasionally more than one queued vehicle.	10-20 sec	10-20 sec	10-15 sec
C	Average traffic delays. Often more than one queued.	20-35 sec	20-35 sec	15-25 sec
D	Long traffic delays. Regularly more than one queued vehicle.	35-55 sec	35-50 sec	25-35 sec
E	Very long traffic delays. Traffic demand is near or equal to the practical capacity of the intersection. Almost always more than one queued vehicle.	55-80 sec	50-70 sec	35-50 sec
F	Forced flow conditions with extensive delays caused by geometric and/or operational constraints external to the intersection.	≥80 sec	≥70 sec	≥50 sec

6.6 Analysis of Development Accesses

An assessment for sight lines along the frontage of the proposed development was undertaken in accordance with AS2890.1: 2004 Parking Facilities: Off-street Car Parking. Based on frontage road speed of 40km/h, the desirable 5 sec gap required is 55 metres on both sides. [Figure 28](#) shows the assessment near the proposed crossovers.

Sightline assessment for both access points indicate that several existing on-street bays are within the hatched area (red hatch), thus, it is recommended to remove these on-street bays to limit sightline obstruction and ensure compliance with relevant sight distance and safety standards.

Figure 28 Sight Line Assessment



6.7 Impact on Surrounding Roads and Intersections

SIDRA Intersection has been used to assess the impact of proposed development on the surrounding intersections. The results for each approach are presented in the form of Degree of Saturation (DoS), Average Delay, Level of Service (LoS) and 95th Percentile Queue in metres.

6.7.1 Assessed Intersections

SIDRA analysis has been undertaken for the following intersections:

- » Haydn Bunton Drive / Subiaco Road
- » Subiaco Road / Site Access (West)
- » Subiaco Road / Site Access (East)

6.7.2 Analysis Scenarios

The SIDRA analysis was undertaken for the following scenarios:

- » Scenario 1: Opening Year (2028 Background + Development Traffic)
- » Scenario 2: 10-Year Horizon (2038 Background + Development Traffic)

6.7.3 Haydn Bunton Drive / Subiaco Road

The intersection layout modelled in SIDRA is shown in [Figure 29](#).

The summary of the SIDRA analysis results for Scenarios 1 and 2 are presented in [Table 15](#) and [Table 16](#), respectively. The results indicate that the intersection would operate satisfactorily and would be able to cater for the proposed development yield in 2028 and 2038.

Figure 29 Haydn Bunton Drive / Subiaco Road SIDRA Layout

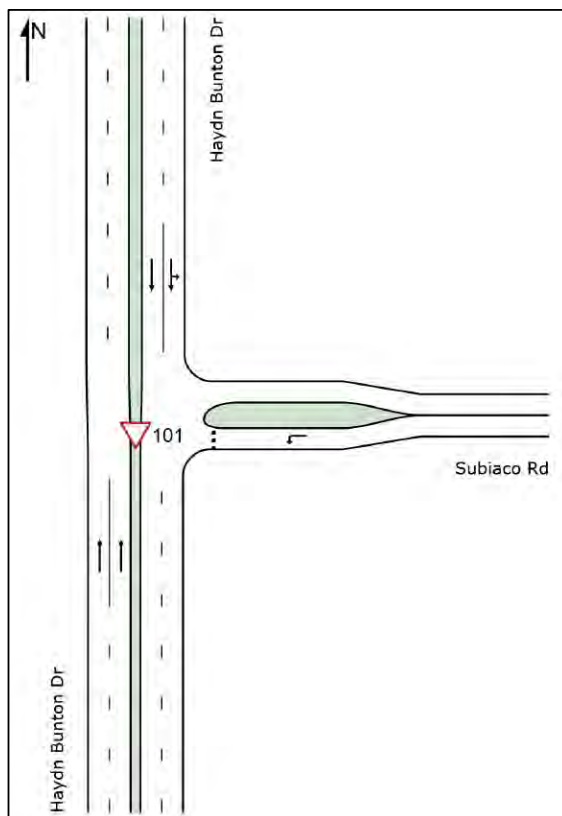


Table 15 Haydn Bunton Drive / Subiaco Road SIDRA Results (Scenario 1)

Intersection Approach	Turn	AM Peak				PM Peak			
		DOS	Delay (s)	LOS	95% Queue (m)	DOS	Delay (s)	LOS	95% Queue (m)
Haydn Bunton Drive (S)	T	0.094	0	LOS A	0	0.191	0	LOS A	0
Subiaco Road (E)	L	0.133	4.8	LOS A	3.7	0.046	3.5	LOS A	1.3
Haydn Bunton Drive (N)	L	0.204	4.6	LOS A	0	0.138	4.6	LOS A	0
	T	0.204	0	LOS A	0	0.138	0	LOS A	0
		0.204	4.8	LOS A	3.7	0.191	4.6	LOS A	1.3

Note that the average delay for priority-controlled intersections is based on the worst-performing movement.

Table 16 Haydn Bunton Drive / Subiaco Road SIDRA Results (Scenario 2)

Intersection Approach	Turn	AM Peak				PM Peak			
		DOS	Delay (s)	LOS	95% Queue (m)	DOS	Delay (s)	LOS	95% Queue (m)
Haydn Bunton Drive (S)	T	0.113	0	LOS A	0	0.229	0	LOS A	0
Subiaco Road (E)	L	0.159	5.2	LOS A	4.5	0.052	3.6	LOS A	1.5
Haydn Bunton Drive (N)	L	0.244	4.6	LOS A	0	0.164	4.6	LOS A	0
	T	0.244	0	LOS A	0	0.164	0	LOS A	0
		0.244	5.2	LOS A	4.5	0.229	4.6	LOS A	1.5

Note that the average delay for priority-controlled intersections is based on the worst-performing movement.

6.7.4 Subiaco Road / Site Access (West)

The intersection layout modelled in SIDRA is shown in [Figure 30](#).

The summary of the SIDRA analysis results for Scenarios 1 and 2 are presented in [Table 17](#) and [Table 18](#), respectively. The results indicate that the future intersection would operate satisfactorily and would be able to cater for the proposed development yield in 2028 and 2038.

Figure 30 Subiaco Road / Site Access (West) SIDRA Layout

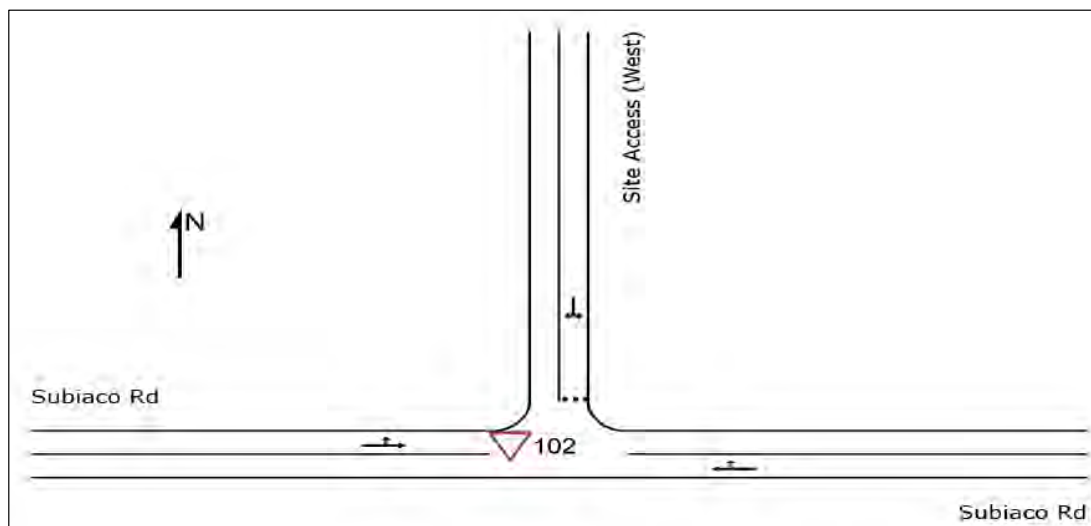


Table 17 Subiaco Road / Site Access (West) SIDRA Results (Scenario 1)

Intersection Approach	Turn	AM Peak				PM Peak			
		DOS	Delay (s)	LOS	95% Queue (m)	DOS	Delay (s)	LOS	95% Queue (m)
Subiaco Road (E)	T	0.042	0	LOS A	0.1	0.02	0.2	LOS A	0.3
	R	0.042	3.6	LOS A	0.1	0.02	4.2	LOS A	0.3
Western Site Access (N)	L	0.053	0.2	LOS A	1.3	0.03	0.5	LOS A	0.7
	R	0.053	1.2	LOS A	1.3	0.03	1.4	LOS A	0.7
Subiaco Road (W)	L	0.053	3.5	LOS A	0	0.12	3.5	LOS A	0
	T	0.053	0	LOS A	0	0.12	0	LOS A	0
		0.053	3.6	LOS A	1.3	0.12	4.2	LOS A	0.7

Note that the average delay for priority-controlled intersections is based on the worst-performing movement.

Table 18 Subiaco Road / Site Access (West) SIDRA Results (Scenario 2)

Intersection Approach	Turn	AM Peak				PM Peak			
		DOS	Delay (s)	LOS	95% Queue (m)	DOS	Delay (s)	LOS	95% Queue (m)
Subiaco Road (E)	T	0.05	0	LOS A	0.1	0.024	0.2	LOS A	0.3
	R	0.05	3.6	LOS A	0.1	0.024	4.4	LOS A	0.3
Western Site Access (N)	L	0.055	0.3	LOS A	1.3	0.033	0.6	LOS A	0.8
	R	0.055	1.3	LOS A	1.3	0.033	1.6	LOS A	0.8
Subiaco Road (W)	L	0.062	3.5	LOS A	0	0.14	3.5	LOS A	0
	T	0.062	0	LOS A	0	0.14	0	LOS A	0
		0.062	3.6	LOS A	1.3	0.14	4.4	LOS A	0.8

Note that the average delay for priority-controlled intersections is based on the worst-performing movement.

6.7.5 Subiaco Road / Site Access (East)

This intersection layout modelled in SIDRA is shown in [Figure 31](#).

The summary of the SIDRA analysis results for Scenarios 1 and 2 are presented in [Table 19](#) and [Table 20](#), respectively. The results indicate that the future intersection would operate satisfactorily and would be able to cater for the proposed development yield in 2028 and 2038.

Figure 31 Subiaco Road / Site Access (East) SIDRA Layout

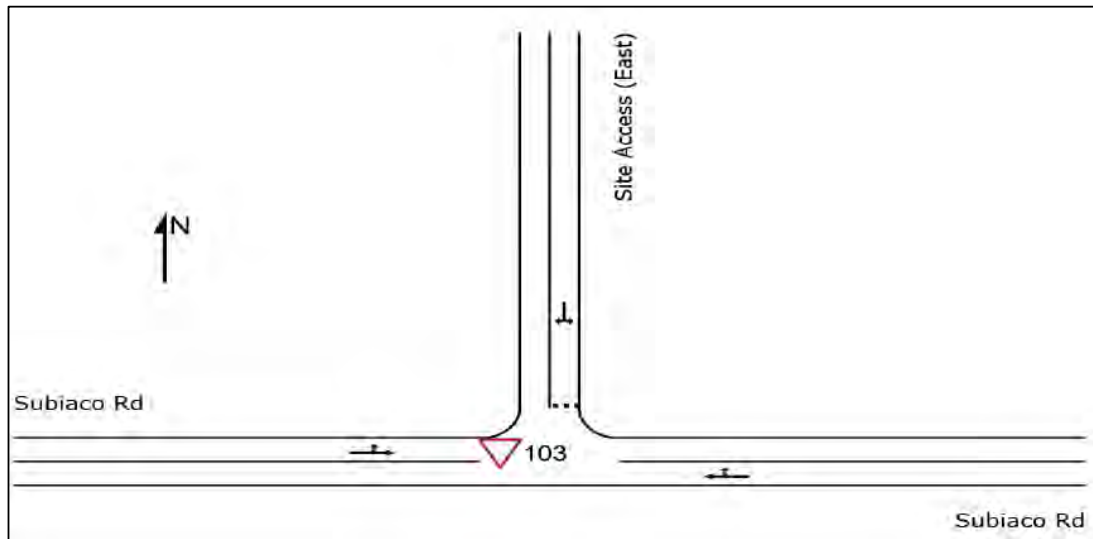


Table 19 Subiaco Road / Site Access (East) SIDRA Results (Scenario 1)

Intersection Approach	Turn	AM Peak				PM Peak			
		DOS	Delay (s)	LOS	95% Queue (m)	DOS	Delay (s)	LOS	95% Queue (m)
Subiaco Road (E)	T	0.066	0.1	LOS A	1.8	0.084	0.6	LOS A	3
	R	0.066	3.8	LOS A	1.8	0.084	4.1	LOS A	3
Eastern Site Access (N)	L	0.08	0.3	LOS A	2.4	0.048	0.5	LOS A	1.4
	R	0.08	1.4	LOS A	2.4	0.048	1.8	LOS A	1.4
Subiaco Road (W)	L	0.045	3.4	LOS A	0	0.096	3.4	LOS A	0
	T	0.045	0	LOS A	0	0.096	0	LOS A	0
		0.08	3.8	LOS A	2.4	0.096	4.1	LOS A	3

Note that the average delay for priority-controlled intersections is based on the worst-performing movement.

Table 20 Subiaco Road / Site Access (East) SIDRA Results (Scenario 2)

Intersection Approach	Turn	AM Peak				PM Peak			
		DOS	Delay (s)	LOS	95% Queue (m)	DOS	Delay (s)	LOS	95% Queue (m)
Subiaco Road (E)	T	0.074	0.1	LOS A	1.8	0.09	0.7	LOS A	3.3
	R	0.074	3.8	LOS A	1.8	0.09	4.3	LOS A	3.3
Eastern Site Access (N)	L	0.081	0.3	LOS A	2.4	0.05	0.7	LOS A	1.4
	R	0.081	1.5	LOS A	2.4	0.05	2	LOS A	1.4
Subiaco Road (W)	L	0.054	3.4	LOS A	0	0.116	3.5	LOS A	0
	T	0.054	0	LOS A	0	0.116	0	LOS A	0
		0.081	3.8	LOS A	2.4	0.116	4.3	LOS A	3.3

Note that the average delay for priority-controlled intersections is based on the worst-performing movement.

6.7.6 Summary

Overall, the proposed development is unlikely to have any significant impact on the surrounding road network. All key intersections assessed would operate satisfactorily and would be able to cater for the proposed development yield during the peak periods in the opening year (2028) and 10 years after the opening year (2038).

7 ROAD SAFETY AND ACCESSIBILITY IMPACT

7.1 Impact on Neighbouring Areas

The proposed development is a mixed-use residential development which will have a similar behaviour to the surrounding residential developments. The traffic generated by the proposed development is considered relatively low and is unlikely to have any major impact to the surrounding residential developments.

7.2 Road Safety

The major intersections in the vicinity of the Site are already signalised, a Left-in Left-out intersection, and a full movement access with low turning movements. Additionally, vehicles are less likely to travel at high speed due to the sub-urban streetscape. The plan to boulevard Subiaco Road will also encourage vehicles to travel at a slower speed, improving the overall road safety of the surrounding road network.

7.3 Public Transport Access

The proposed development does not include any upgrades to public transport infrastructure and is not expected to negatively affect existing public transport access. All public transport accessibility is likely to remain as per existing.

PTG Consulting has consulted the PTA and was informed that there will not be any significant changes to the surrounding public transport network in the near future.

7.4 Pedestrian/Cycle Access/Amenity

The proposed development has taken into consideration of pedestrian and cycling amenity by providing footpath connections and bicycle parking, which encourages the use of active transport. Further progress of the East Subi Redevelopment Plan will see improvements to the overall pedestrian and cycling network in the vicinity of the Site.

7.5 Analysis of Pedestrian/Cycle Network

Overall, the proposed development is likely to provide a positive impact to the surrounding pedestrian and cycling network and will likely encourage increased use of active transportation.

7.6 Safe Walk/Cycle to School

The existing footpath network in the vicinity of the Site has good connections to the school sites in the vicinity of the Site. Further improvements will be implemented as the East Subi Redevelopment progress gradually.

8 SUMMARY AND CONCLUSIONS

This report has been prepared in accordance with the Western Australian Planning Commission (WAPC) Transport Assessment Guidelines for Developments: Volume 4 - Individual Developments (2016).

The following conclusions can be drawn from this report:

- » The proposed development is estimated to generate 236 trips in the AM peak and 244 trips in the PM peak.
- » Two vehicular and three pedestrian access points are located along Subiaco Road, and one pedestrian access is located along Court Place.
- » The Site benefits from good public transport, pedestrian and cycling network accessibility. Having convenient access to these facilities incentivises more people to walk and cycle and help reduce reliance on vehicular travel and thereby reducing the demand for parking. The proposed development is not expected to have any negative impact on the existing public transport, pedestrian and cycling network.
- » Proposed car parking provisions shows a shortfall of 98 residential parking bays and 32 residential visitor bays in accordance with the relevant guidelines. However, the subject site is located in a highly accessible area with convenient access to public transport, including a nearby train station and high-frequency bus services operating along Roberts Road and the surrounding road network. The site is also well served by an established walking and cycling network. This includes shared paths on both sides of Subiaco Road, as well as a principal shared path that runs parallel to the railway line, providing direct and continuous connections to key destinations in the area. Having convenient access to these facilities incentivises more people to walk and cycle and help reduce reliance on vehicular travel and thereby reducing the demand for parking.
- » While a shortfall in residential bicycle parking provision has been identified, the availability of nearby public transport is a significantly more attractive and preferred mode of travel for many occupants. The development is located within a highly walkable, transit-oriented environment where public transport and walking are expected to account for a substantial proportion of trips. CHL is the largest community housing provider in Australia, managing over 12,000 properties nationwide. Based on their extensive experience across these sites, bicycle storage facilities are consistently underutilised and largely remain vacant.
- » Sightline assessment for both access points indicate that several existing on-street bays are within the sight line triangle, thus, it is recommended to remove these on-street bays to limit sightline obstruction.
- » SIDRA analysis has been conducted at Haydn Bunton Drive / Subiaco Road, Subiaco Road / Site Access (West) and Subiaco Road / Site Access (East) intersections. Results suggest that the traffic generated by the proposed development is unlikely to have any significant impact to the surrounding road network.
- » Due to the nature of the development and the existing road environment and layouts, it is envisaged that any impact on road safety would be negligible.



Appendix A

WAPC CHECKLIST FOR INDIVIDUAL
DEVELOPMENT - TRANSPORT
IMPACT ASSESSMENT



APPENDIX A - WAPC CHECKLIST

Item	Status	Comments/Proposal
Summary	Section 8	
Introduction/Background	Section 1	
name of applicant and consultant	Section 1	
development location and context	Section 1	
brief description of development proposal	Section 1	
key issues	Section 1	
background information	Section 1	
Existing situation	Section 2	
existing site uses (if any)	Section 2	
existing parking and demand (if appropriate)	Section 2	
existing access arrangements	Section 2	
existing site traffic	Section 2	
surrounding land uses	Section 2	
surrounding road network	Section 2	
traffic management on frontage roads	Section 2	
traffic flows on surrounding roads (usually AM and PM peak hours)	Section 2	
traffic flows at major intersections (usually AM and PM peak hours)	Section 2	
operation of surrounding intersections	Section 2	
existing pedestrian/cycle networks	Section 2	
existing public transport services surrounding the development	Section 2	
crash data	Section 2	
Development proposal	Section 3	
regional context	Section 2	
proposed land uses	Section 3	
table of land uses and quantities	Section 3	
access arrangements	Section 3	
parking provision	Section 3	
end of trip facilities	Section 3	
any specific issues	Section 3	
road network	Section 4	

intersection layouts and controls	Section 4	
pedestrian/cycle networks and crossing facilities	Section 4	
public transport services	Section 4	
Integration with surrounding area	Section 5	
surrounding major attractors/ generators	Section 5	
committed developments and transport proposals	Section 5	
proposed changes to land uses within 1200 metres	Section 5	
travel desire lines from development to these attractors/ generators	Section 5	
adequacy of existing transport networks	Section 5	
deficiencies in existing transport networks	Section 5	
remedial measures to address deficiencies	N/A	
Analysis of transport networks	Section 6	
assessment years	Section 6	
time periods	Section 6	
development generated traffic	Section 6	
distribution of generated traffic	Section 6	
parking supply and demand	N/A	
base and 'with development' traffic flows	Section 6	
analysis of development accesses	Section 6	
impact on surrounding roads	Section 6	
impact on intersections	Section 6	
impact on neighbouring areas	Section 7	
road safety	Section 7	
public transport access	Section 7	
pedestrian access/amenity	Section 7	
cycle access/amenity	Section 7	
analysis of pedestrian/cycle networks	Section 7	
safe walk/cycle to school (for residential and school site developments only)	Section 7	
traffic management plan (where appropriate)	N/A	
Conclusions	Section 8	



Appendix B

SITE PLANS

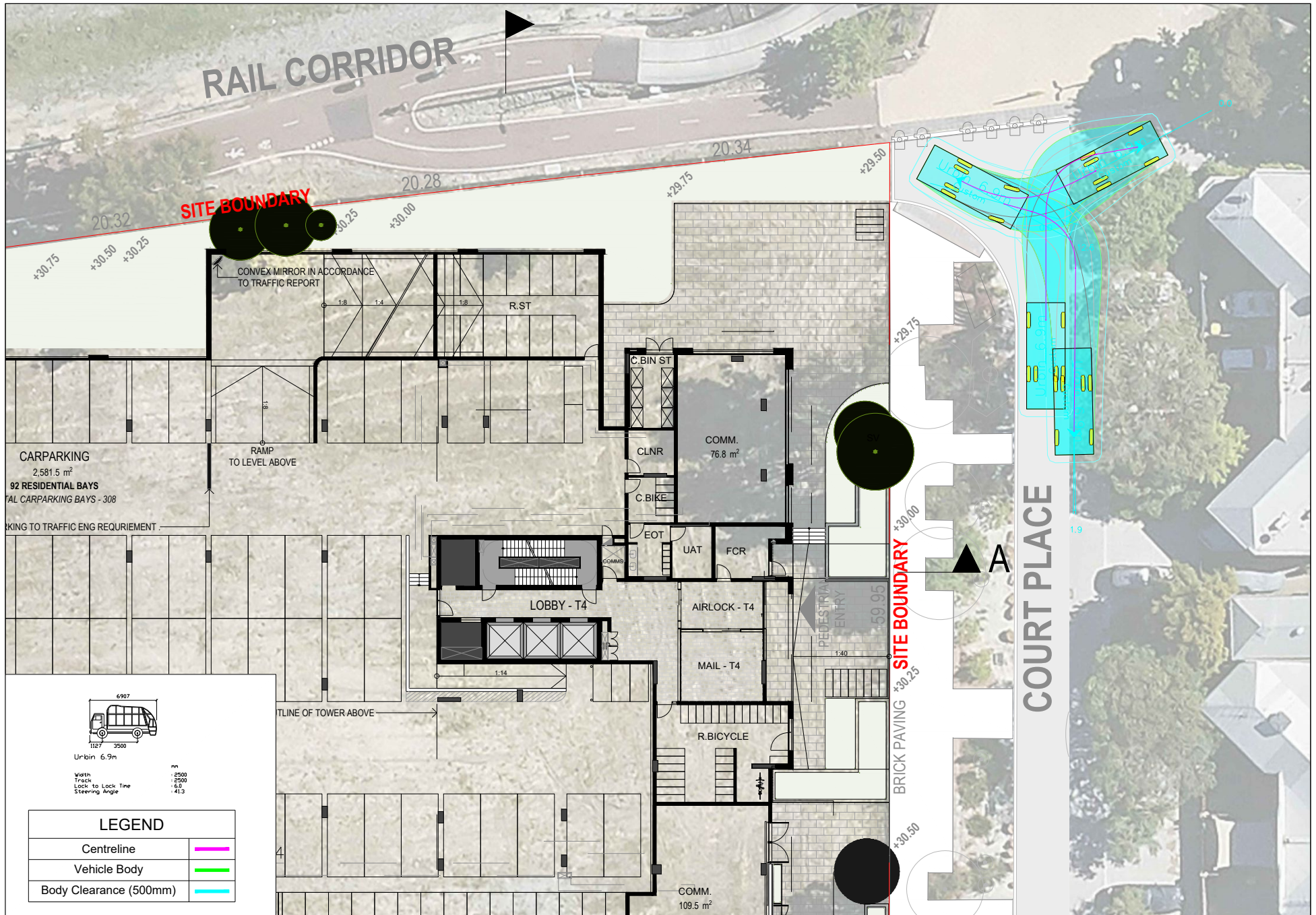




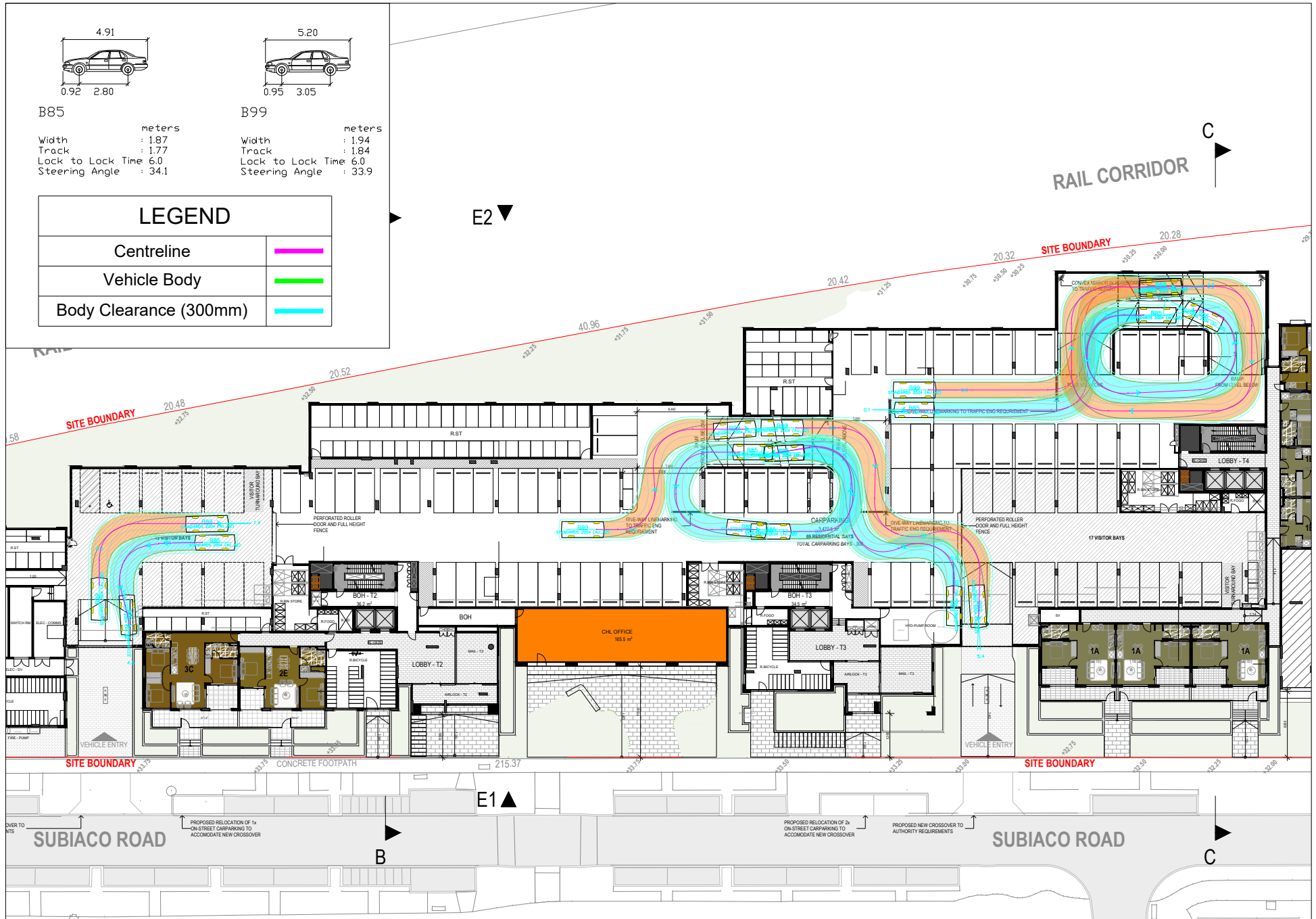
Appendix C

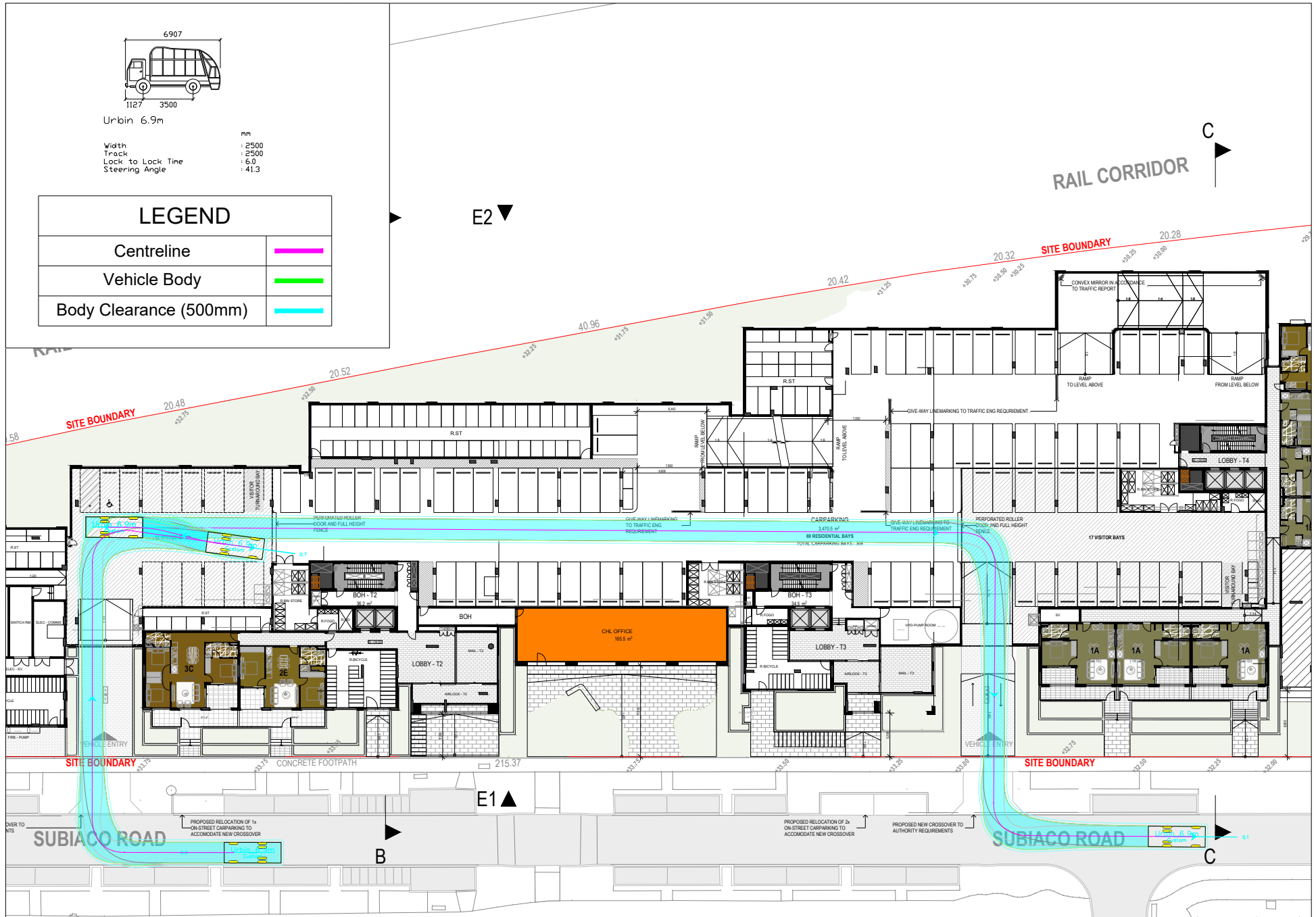
SWEPT PATHS

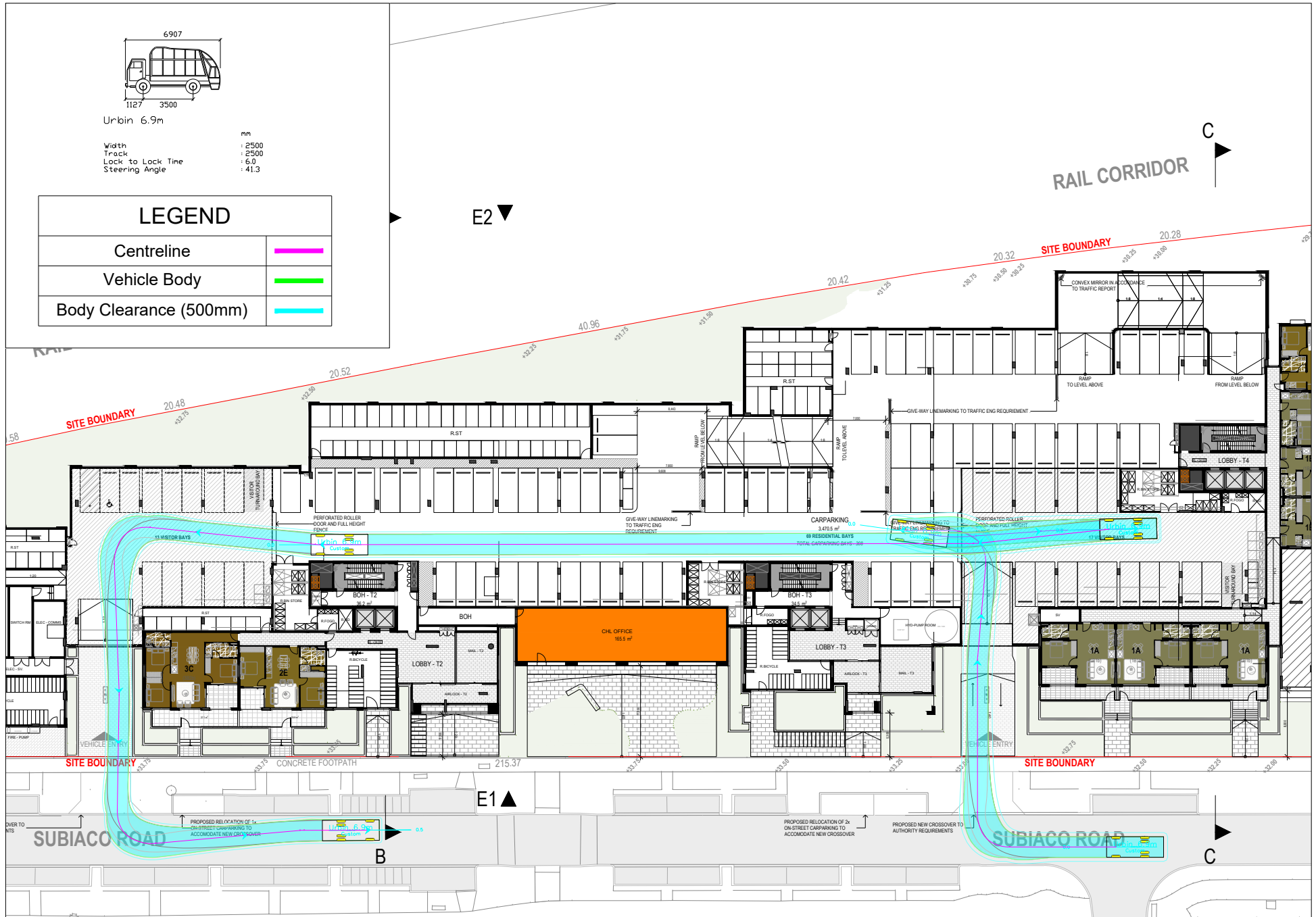




Subiaco LRC Meeting - Development Management







Appendix D

SIDRA MOVEMENT SUMMARIES

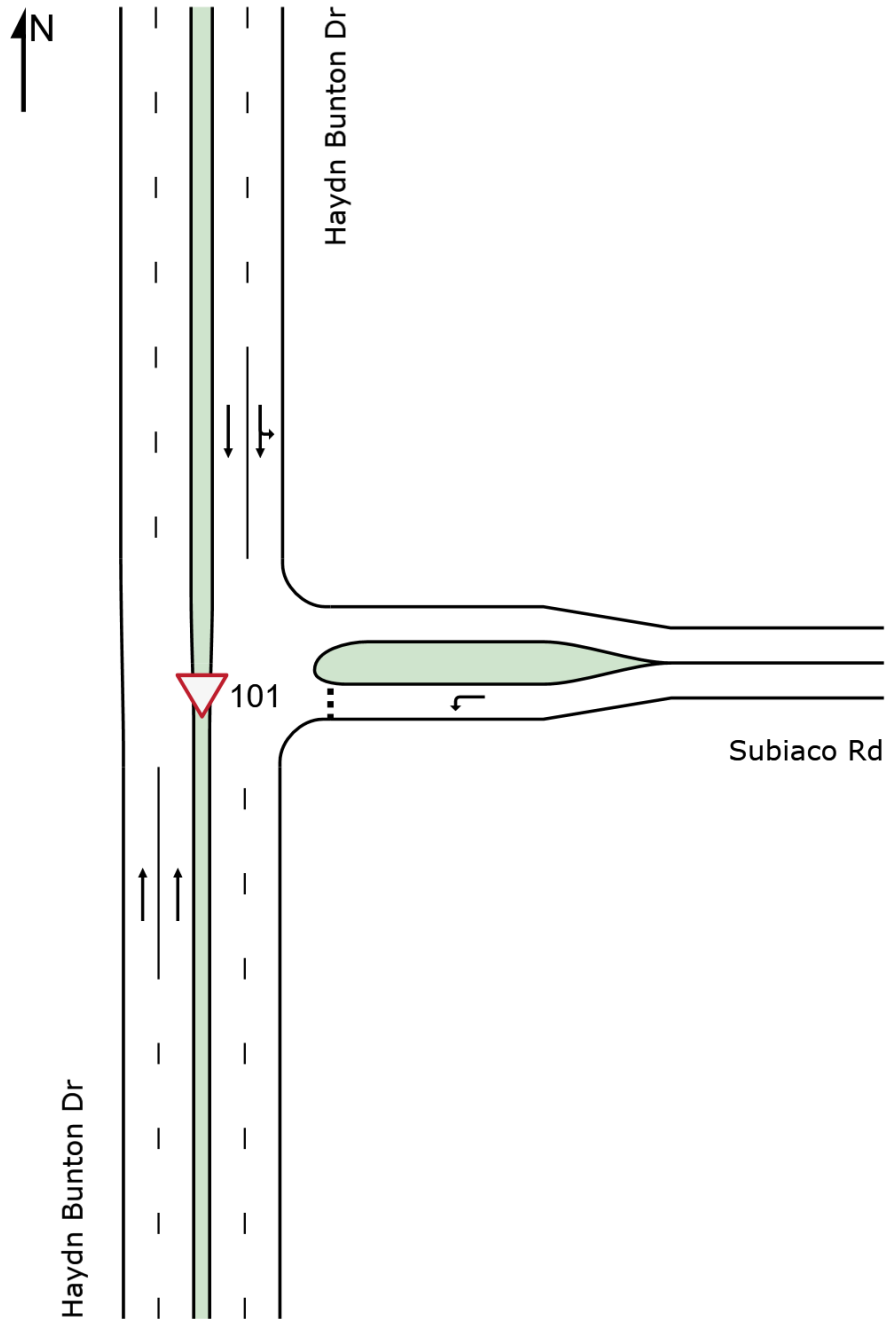


SITE LAYOUT

▽ Site: [101] Haydn Bunton Dr/Subiaco Rd_AM (2028)

New Site
Site Category: (None)
Give-Way (Two-Way)
Site Scenario: 1 | Local Volumes

Layout pictures are schematic functional drawings reflecting input data. They are not design drawings.

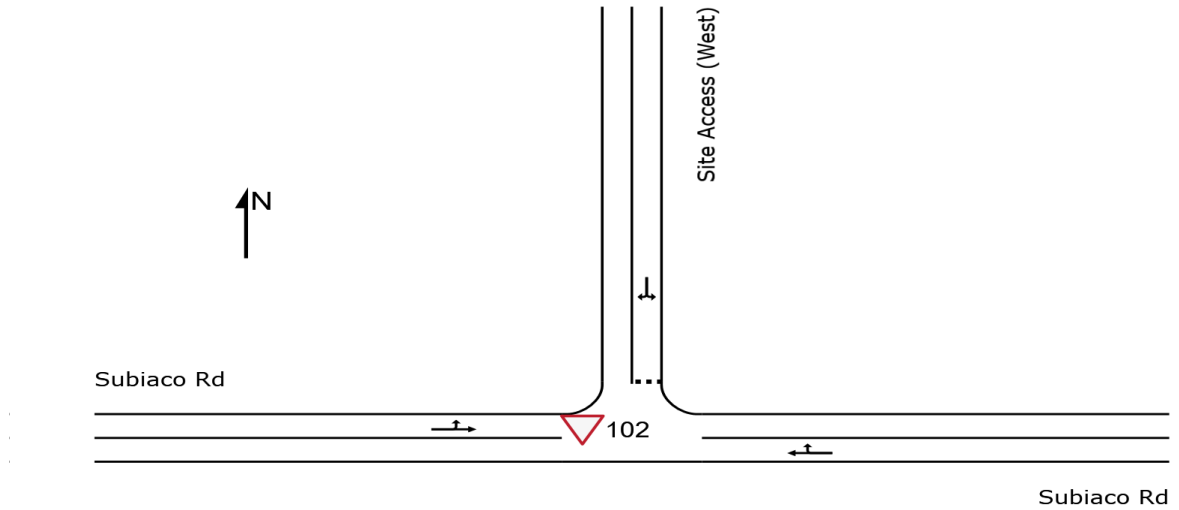


SITE LAYOUT

▽ Site: [102] Subiaco Rd/Site Access (West)_AM (2028)

New Site
Site Category: (None)
Give-Way (Two-Way)
Site Scenario: 1 | Local Volumes

Layout pictures are schematic functional drawings reflecting input data. They are not design drawings.



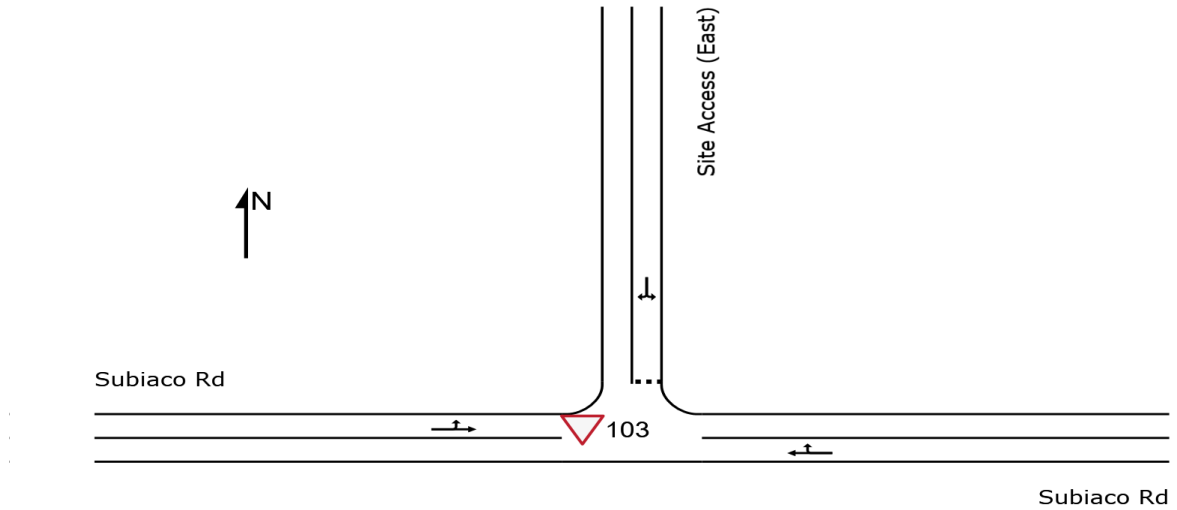
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SITE LAYOUT

▽ Site: [103] Subiaco Rd/Site Access (East)_AM (2028)

New Site
Site Category: (None)
Give-Way (Two-Way)
Site Scenario: 1 | Local Volumes

Layout pictures are schematic functional drawings reflecting input data. They are not design drawings.



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MOVEMENT SUMMARY

Site: [101] Haydn Bunton Dr/Subiaco Rd_AM (2028)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Rate	Number of Cycles to Depart	Aver. Speed
			[Total HV] veh/h	[Total HV] %	[Total HV] veh/h	[Total HV] %				[Veh.] veh	[Dist] m				
South: Haydn Bunton Dr															
2	T1	All MCs	365	0.0	365	0.0	0.094	0.0	LOS A	0.0	0.0	0.00	0.00	0.00	50.0
Approach			365	0.0	365	0.0	0.094	0.0	NA	0.0	0.0	0.00	0.00	0.00	50.0
East: Subiaco Rd															
4	L2	All MCs	131	2.4	131	2.4	0.133	4.8	LOS A	0.5	3.7	0.38	0.56	0.38	34.4
Approach			131	2.4	131	2.4	0.133	4.8	LOS A	0.5	3.7	0.38	0.56	0.38	34.4
North: Haydn Bunton Dr															
7	L2	All MCs	99	3.2	99	3.2	0.204	4.6	LOS A	0.0	0.0	0.00	0.14	0.00	44.5
8	T1	All MCs	688	0.0	688	0.0	0.204	0.0	LOS A	0.0	0.0	0.00	0.06	0.00	48.6
Approach			787	0.4	787	0.4	0.204	0.6	NA	0.0	0.0	0.00	0.07	0.00	47.7
All Vehicles			1283	0.5	1283	0.5	0.204	0.9	NA	0.5	3.7	0.04	0.10	0.04	45.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

▽ Site: [102] Subiaco Rd/Site Access (West)_AM (2028)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance																
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Cycles Rate to Depart	Number of Cycles	Aver. Speed	
			[Total HV]	[Total HV]			v/c	sec			[Veh.]	[Dist]			km/h	
			veh/h	%	veh/h	%										
East: Subiaco Rd																
5	T1	All MCs	76	4.2	76	4.2	0.042	0.0	LOS A	0.0	0.1	0.01	0.01	0.01	0.01	39.8
6	R2	All MCs	2	0.0	2	0.0	0.042	3.6	LOS A	0.0	0.1	0.01	0.01	0.01	0.01	35.2
Approach			78	4.1	78	4.1	0.042	0.1	NA	0.0	0.1	0.01	0.01	0.01	0.01	39.8
North: Site Access (West)																
7	L2	All MCs	6	0.0	6	0.0	0.053	0.2	LOS A	0.2	1.3	0.23	0.22	0.23	0.23	28.1
9	R2	All MCs	55	0.0	55	0.0	0.053	1.2	LOS A	0.2	1.3	0.23	0.22	0.23	0.23	33.6
Approach			61	0.0	61	0.0	0.053	1.1	LOS A	0.2	1.3	0.23	0.22	0.23	0.23	33.3
West: Subiaco Rd																
10	L2	All MCs	20	0.0	20	0.0	0.053	3.5	LOS A	0.0	0.0	0.00	0.10	0.00	0.00	28.7
11	T1	All MCs	79	4.0	79	4.0	0.053	0.0	LOS A	0.0	0.0	0.00	0.10	0.00	0.00	39.2
Approach			99	3.2	99	3.2	0.053	0.7	NA	0.0	0.0	0.00	0.10	0.00	0.00	36.9
All Vehicles			238	2.7	238	2.7	0.053	0.6	NA	0.2	1.3	0.06	0.10	0.06	0.06	37.0

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

▽ Site: [103] Subiaco Rd/Site Access (East)_AM (2028)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Cycles Rate to Depart	Number of Cycles	Aver. Speed
			[Total HV]	[Total HV]	[Total HV]	[Total HV]	v/c	sec		[Veh. veh	[Dist] m				km/h
East: Subiaco Rd															
5	T1	All MCs	75	4.2	75	4.2	0.066	0.1	LOS A	0.2	1.8	0.14	0.21	0.14	35.7
6	R2	All MCs	43	0.0	43	0.0	0.066	3.8	LOS A	0.2	1.8	0.14	0.21	0.14	28.1
Approach			118	2.7	118	2.7	0.066	1.5	NA	0.2	1.8	0.14	0.21	0.14	33.7
North: Site Access (East)															
7	L2	All MCs	117	0.0	117	0.0	0.080	0.3	LOS A	0.3	2.4	0.18	0.07	0.18	25.6
9	R2	All MCs	3	0.0	3	0.0	0.080	1.4	LOS A	0.3	2.4	0.18	0.07	0.18	28.3
Approach			120	0.0	120	0.0	0.080	0.3	LOS A	0.3	2.4	0.18	0.07	0.18	25.7
West: Subiaco Rd															
10	L2	All MCs	1	0.0	1	0.0	0.045	3.4	LOS A	0.0	0.0	0.00	0.01	0.00	25.7
11	T1	All MCs	84	3.8	84	3.8	0.045	0.0	LOS A	0.0	0.0	0.00	0.01	0.00	39.9
Approach			85	3.7	85	3.7	0.045	0.0	NA	0.0	0.0	0.00	0.01	0.00	39.7
All Vehicles			323	2.0	323	2.0	0.080	0.7	NA	0.3	2.4	0.12	0.10	0.12	32.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

Site: [201] Haydn Bunton Dr/Subiaco Rd_PM (2028)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Rate	Number of Cycles to Depart	Aver. Speed
			veh/h	%	veh/h	%				[Veh.]	[Dist]				
South: Haydn Bunton Dr															
2	T1	All MCs	743	0.0	743	0.0	0.191	0.0	LOS A	0.0	0.0	0.00	0.00	0.00	49.9
Approach			743	0.0	743	0.0	0.191	0.0	NA	0.0	0.0	0.00	0.00	0.00	49.9
East: Subiaco Rd															
4	L2	All MCs	59	5.4	59	5.4	0.046	3.5	LOS A	0.2	1.3	0.08	0.44	0.08	35.4
Approach			59	5.4	59	5.4	0.046	3.5	LOS A	0.2	1.3	0.08	0.44	0.08	35.4
North: Haydn Bunton Dr															
7	L2	All MCs	221	5.2	221	5.2	0.138	4.6	LOS A	0.0	0.0	0.00	0.48	0.00	40.5
8	T1	All MCs	294	0.0	294	0.0	0.138	0.0	LOS A	0.0	0.0	0.00	0.04	0.00	49.0
Approach			515	2.2	515	2.2	0.138	2.0	NA	0.0	0.0	0.00	0.23	0.00	43.8
All Vehicles			1317	1.1	1317	1.1	0.191	0.9	NA	0.2	1.3	0.00	0.11	0.00	45.9

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

Site: [202] Subiaco Rd/Site Access (West)_PM (2028)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Cycles Rate to Depart	Number of Cycles	Aver. Speed
			[Total HV] veh/h	[Total HV] %	[Total HV] veh/h	[Total HV] %				[Veh.] veh	[Dist] m				
East: Subiaco Rd															
5	T1	All MCs	29	10.7	29	10.7	0.020	0.1	LOS A	0.0	0.3	0.11	0.12	0.11	38.9
6	R2	All MCs	5	0.0	5	0.0	0.020	4.2	LOS A	0.0	0.3	0.11	0.12	0.11	33.0
Approach			35	9.1	35	9.1	0.020	0.8	NA	0.0	0.3	0.11	0.12	0.11	38.5
North: Site Access (West)															
7	L2	All MCs	3	0.0	3	0.0	0.032	0.5	LOS A	0.1	0.7	0.28	0.26	0.28	27.8
9	R2	All MCs	31	0.0	31	0.0	0.032	1.4	LOS A	0.1	0.7	0.28	0.26	0.28	33.4
Approach			34	0.0	34	0.0	0.032	1.4	LOS A	0.1	0.7	0.28	0.26	0.28	33.1
West: Subiaco Rd															
10	L2	All MCs	48	0.0	48	0.0	0.121	3.5	LOS A	0.0	0.0	0.00	0.10	0.00	28.6
11	T1	All MCs	173	6.7	173	6.7	0.121	0.0	LOS A	0.0	0.0	0.00	0.10	0.00	39.1
Approach			221	5.2	221	5.2	0.121	0.8	NA	0.0	0.0	0.00	0.10	0.00	36.7
All Vehicles			289	5.1	289	5.1	0.121	0.8	NA	0.1	0.7	0.05	0.12	0.05	36.5

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

Site: [203] Subiaco Rd/Site Access (East)_PM (2028)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Cycles Rate to Depart	Number of Cycles	Aver. Speed
			[Total HV]	[Total HV]	[Total HV]	[Total HV]	v/c	sec		[Veh. veh]	[Dist m]				km/h
East: Subiaco Rd															
5	T1	All MCs	33	9.7	33	9.7	0.084	0.6	LOS A	0.4	3.0	0.29	0.42	0.29	32.0
6	R2	All MCs	102	0.0	102	0.0	0.084	4.1	LOS A	0.4	3.0	0.29	0.42	0.29	23.8
Approach			135	2.3	135	2.3	0.084	3.3	NA	0.4	3.0	0.29	0.42	0.29	26.5
North: Site Access (East)															
7	L2	All MCs	64	0.0	64	0.0	0.048	0.5	LOS A	0.2	1.4	0.27	0.14	0.27	24.9
9	R2	All MCs	2	0.0	2	0.0	0.048	1.8	LOS A	0.2	1.4	0.27	0.14	0.27	27.7
Approach			66	0.0	66	0.0	0.048	0.6	LOS A	0.2	1.4	0.27	0.14	0.27	25.0
West: Subiaco Rd															
10	L2	All MCs	2	0.0	2	0.0	0.096	3.4	LOS A	0.0	0.0	0.00	0.01	0.00	25.7
11	T1	All MCs	174	6.7	174	6.7	0.096	0.0	LOS A	0.0	0.0	0.00	0.01	0.00	39.9
Approach			176	6.6	176	6.6	0.096	0.0	NA	0.0	0.0	0.00	0.01	0.00	39.7
All Vehicles			377	3.9	377	3.9	0.096	1.3	NA	0.4	3.0	0.15	0.18	0.15	32.7

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

Site: [301] Haydn Bunton Dr/Subiaco Rd_AM (2038)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Rate	Number of Cycles to Depart	Aver. Speed
			[Total HV] veh/h	[Total HV] %	[Total HV] veh/h	[Total HV] %				[Veh.] veh	[Dist] m				
South: Haydn Bunton Dr															
2	T1	All MCs	439	0.0	439	0.0	0.113	0.0	LOS A	0.0	0.0	0.00	0.00	0.00	50.0
Approach			439	0.0	439	0.0	0.113	0.0	NA	0.0	0.0	0.00	0.00	0.00	50.0
East: Subiaco Rd															
4	L2	All MCs	145	2.9	145	2.9	0.159	5.2	LOS A	0.6	4.5	0.43	0.60	0.43	34.2
Approach			145	2.9	145	2.9	0.159	5.2	LOS A	0.6	4.5	0.43	0.60	0.43	34.2
North: Haydn Bunton Dr															
7	L2	All MCs	115	3.7	115	3.7	0.244	4.6	LOS A	0.0	0.0	0.00	0.13	0.00	44.4
8	T1	All MCs	827	0.0	827	0.0	0.244	0.0	LOS A	0.0	0.0	0.00	0.06	0.00	48.6
Approach			942	0.4	942	0.4	0.244	0.6	NA	0.0	0.0	0.00	0.07	0.00	47.7
All Vehicles			1526	0.6	1526	0.6	0.244	0.9	NA	0.6	4.5	0.04	0.10	0.04	45.8

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

▽ Site: [302] Subiaco Rd/Site Access (West)_AM (2038)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Cycles Rate to Depart	Number of Cycles	Aver. Speed
			[Total HV]	[Total HV]	[Total HV]	[Total HV]	v/c	sec		[Veh.]	[Dist]				km/h
			veh/h	%	veh/h	%				veh	m				
East: Subiaco Rd															
5	T1	All MCs	91	4.7	91	4.7	0.050	0.0	LOS A	0.0	0.1	0.01	0.01	0.01	39.8
6	R2	All MCs	2	0.0	2	0.0	0.050	3.6	LOS A	0.0	0.1	0.01	0.01	0.01	35.2
Approach			93	4.5	93	4.5	0.050	0.1	NA	0.0	0.1	0.01	0.01	0.01	39.8
North: Site Access (West)															
7	L2	All MCs	6	0.0	6	0.0	0.055	0.3	LOS A	0.2	1.3	0.25	0.24	0.25	28.0
9	R2	All MCs	55	0.0	55	0.0	0.055	1.3	LOS A	0.2	1.3	0.25	0.24	0.25	33.5
Approach			61	0.0	61	0.0	0.055	1.2	LOS A	0.2	1.3	0.25	0.24	0.25	33.2
West: Subiaco Rd															
10	L2	All MCs	20	0.0	20	0.0	0.062	3.5	LOS A	0.0	0.0	0.00	0.08	0.00	28.7
11	T1	All MCs	95	4.4	95	4.4	0.062	0.0	LOS A	0.0	0.0	0.00	0.08	0.00	39.3
Approach			115	3.7	115	3.7	0.062	0.6	NA	0.0	0.0	0.00	0.08	0.00	37.3
All Vehicles			268	3.1	268	3.1	0.062	0.6	NA	0.2	1.3	0.06	0.09	0.06	37.3

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

▽ Site: [303] Subiaco Rd/Site Access (East)_AM (2038)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Cycles Rate to Depart	Number of Cycles	Aver. Speed
			[Total HV]	[Total HV]	[Total HV]	[Total HV]	v/c	sec		[Veh.]	[Dist]				km/h
			veh/h	%	veh/h	%				veh	m				
East: Subiaco Rd															
5	T1	All MCs	89	4.7	89	4.7	0.074	0.1	LOS A	0.2	1.8	0.14	0.19	0.14	36.0
6	R2	All MCs	43	0.0	43	0.0	0.074	3.8	LOS A	0.2	1.8	0.14	0.19	0.14	28.4
Approach			133	3.2	133	3.2	0.074	1.4	NA	0.2	1.8	0.14	0.19	0.14	34.2
North: Site Access (East)															
7	L2	All MCs	117	0.0	117	0.0	0.081	0.3	LOS A	0.3	2.4	0.20	0.08	0.20	25.4
9	R2	All MCs	3	0.0	3	0.0	0.081	1.5	LOS A	0.3	2.4	0.20	0.08	0.20	28.1
Approach			120	0.0	120	0.0	0.081	0.3	LOS A	0.3	2.4	0.20	0.08	0.20	25.5
West: Subiaco Rd															
10	L2	All MCs	1	0.0	1	0.0	0.054	3.4	LOS A	0.0	0.0	0.00	0.00	0.00	25.7
11	T1	All MCs	100	4.2	100	4.2	0.054	0.0	LOS A	0.0	0.0	0.00	0.00	0.00	39.9
Approach			101	4.2	101	4.2	0.054	0.0	NA	0.0	0.0	0.00	0.00	0.00	39.7
All Vehicles			354	2.4	354	2.4	0.081	0.6	NA	0.3	2.4	0.12	0.10	0.12	33.3

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

Site: [401] Haydn Bunton Dr/Subiaco Rd_PM (2038)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Rate	Number of Cycles to Depart	Aver. Speed
			veh/h	%	veh/h	%				v/c	sec				
South: Haydn Bunton Dr															
2	T1	All MCs	893	0.0	893	0.0	0.229	0.0	LOS A	0.0	0.0	0.00	0.00	0.00	49.9
Approach			893	0.0	893	0.0	0.229	0.0	NA	0.0	0.0	0.00	0.00	0.00	49.9
East: Subiaco Rd															
4	L2	All MCs	65	6.5	65	6.5	0.052	3.6	LOS A	0.2	1.5	0.10	0.44	0.10	35.2
Approach			65	6.5	65	6.5	0.052	3.6	LOS A	0.2	1.5	0.10	0.44	0.10	35.2
North: Haydn Bunton Dr															
7	L2	All MCs	256	5.8	256	5.8	0.164	4.6	LOS A	0.0	0.0	0.00	0.47	0.00	40.5
8	T1	All MCs	353	0.0	353	0.0	0.164	0.0	LOS A	0.0	0.0	0.00	0.04	0.00	48.9
Approach			608	2.4	608	2.4	0.164	2.0	NA	0.0	0.0	0.00	0.22	0.00	43.8
All Vehicles			1566	1.2	1566	1.2	0.229	0.9	NA	0.2	1.5	0.00	0.10	0.00	45.9

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

▽ Site: [402] Subiaco Rd/Site Access (West)_PM (2038)

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New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Cycles Rate to Depart	Number of Cycles	Aver. Speed
			[Total HV]	[Total HV]	[Total HV]	[Total HV]	v/c	sec		[Veh. veh	[Dist] m				km/h
East: Subiaco Rd															
5	T1	All MCs	36	11.8	36	11.8	0.024	0.2	LOS A	0.0	0.3	0.10	0.11	0.10	39.0
6	R2	All MCs	5	0.0	5	0.0	0.024	4.4	LOS A	0.0	0.3	0.10	0.11	0.10	33.2
Approach			41	10.3	41	10.3	0.024	0.7	NA	0.0	0.3	0.10	0.11	0.10	38.7
North: Site Access (West)															
7	L2	All MCs	3	0.0	3	0.0	0.033	0.6	LOS A	0.1	0.8	0.31	0.28	0.31	27.6
9	R2	All MCs	31	0.0	31	0.0	0.033	1.6	LOS A	0.1	0.8	0.31	0.28	0.31	33.3
Approach			34	0.0	34	0.0	0.033	1.5	LOS A	0.1	0.8	0.31	0.28	0.31	33.0
West: Subiaco Rd															
10	L2	All MCs	48	0.0	48	0.0	0.140	3.5	LOS A	0.0	0.0	0.00	0.09	0.00	28.7
11	T1	All MCs	207	7.1	207	7.1	0.140	0.0	LOS A	0.0	0.0	0.00	0.09	0.00	39.2
Approach			256	5.8	256	5.8	0.140	0.7	NA	0.0	0.0	0.00	0.09	0.00	37.1
All Vehicles			331	5.7	331	5.7	0.140	0.8	NA	0.1	0.8	0.04	0.11	0.04	36.9

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

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MOVEMENT SUMMARY

Site: [403] Subiaco Rd/Site Access (East)_PM (2038)

Output produced by SIDRA INTERSECTION Version: 10.0.5.217

New Site

Site Category: (None)

Give-Way (Two-Way)

Site Scenario: 1 | Local Volumes

Vehicle Movement Performance															
Mov ID	Turn	Mov Class	Demand Flows		Arrival Flows		Deg. Satn	Aver. Delay	Level of Service	95% Back Of Queue		Prop. Qued	Eff. Stop of Cycles Rate to Depart	Number of Cycles	Aver. Speed
			[Total HV]	[Total HV]	[Total HV]	[Total HV]	v/c	sec		[Veh.]	[Dist]				km/h
			veh/h	%	veh/h	%				veh	m				
East: Subiaco Rd															
5	T1	All MCs	39	10.8	39	10.8	0.090	0.7	LOS A	0.4	3.3	0.32	0.43	0.32	32.0
6	R2	All MCs	102	0.0	102	0.0	0.090	4.3	LOS A	0.4	3.3	0.32	0.43	0.32	23.8
Approach			141	3.0	141	3.0	0.090	3.3	NA	0.4	3.3	0.32	0.43	0.32	26.8
North: Site Access (East)															
7	L2	All MCs	64	0.0	64	0.0	0.050	0.7	LOS A	0.2	1.4	0.30	0.17	0.30	24.7
9	R2	All MCs	2	0.0	2	0.0	0.050	2.0	LOS A	0.2	1.4	0.30	0.17	0.30	27.5
Approach			66	0.0	66	0.0	0.050	0.7	LOS A	0.2	1.4	0.30	0.17	0.30	24.8
West: Subiaco Rd															
10	L2	All MCs	2	0.0	2	0.0	0.116	3.5	LOS A	0.0	0.0	0.00	0.00	0.00	25.7
11	T1	All MCs	208	7.1	208	7.1	0.116	0.0	LOS A	0.0	0.0	0.00	0.00	0.00	39.9
Approach			211	7.0	211	7.0	0.116	0.0	NA	0.0	0.0	0.00	0.00	0.00	39.7
All Vehicles			418	4.5	418	4.5	0.116	1.2	NA	0.4	3.3	0.16	0.17	0.16	33.3

Site Level of Service (LOS) Method: Delay (SIDRA). Site LOS Method is specified in the Parameter Settings dialog (Options tab).

Vehicle movement LOS values are based on average delay per movement.

Minor Road Approach LOS values are based on average delay for all vehicle movements.

NA (TWSC): Level of Service is not defined for major road approaches or the intersection as a whole for Two-Way Sign Control (HCM LOS rule).

Two-Way Sign Control Capacity Model: SIDRA Standard.

Delay Model: SIDRA Standard (Control Delay: Geometric Delay is included).

Queue Model: SIDRA queue estimation methods are used for Back of Queue and Queue at Start of Gap.

Gap-Acceptance Capacity Formula: SIDRA Standard (Akçelik M3D).

HV (%) values are calculated for All Movement Classes of All Heavy Vehicle Model Designation.

Arrival Flows used in performance calculations are adjusted to include any Initial Queued Demand and Upstream Capacity Constraint effects.

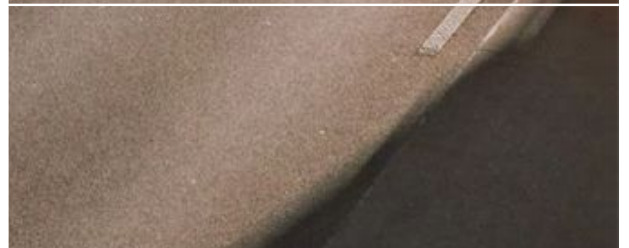
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Waste Management Plan

154 Subiaco Road, Subiaco

Prepared for Community Housing (Subi East) Limited

21 January 2026

Project Number: WMP25053

Waste Management Plan
 154 Subiaco Road, Subiaco
 Community Housing (Subi East) Limited



DRAFT

DOCUMENT CONTROL					
Version	Description	Date	Author	Reviewer	Approver
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Ann Brouwer	Project Manager – Waste Management Plan Lead (WA)	WMP25053-02_Waste Management Plan_0.3			
Signature					
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Executive Summary

Community Housing (Subi East) Limited is seeking development approval for the proposed mixed-use development located at 154 Subiaco Road, Subiaco (the Proposal).

To satisfy the conditions of the development application the City of Subiaco (the City) requires the submission of a Waste Management Plan (WMP) that will identify how waste is to be stored and collected from the Proposal. Talis Consultants has been engaged to prepare this WMP to satisfy the City's requirements.

A summary of the bin size, numbers, collection frequency and collection method is provided in the below table.

Proposed Waste Collection Summary

Waste Type	Generation (L/week)	Bin Size (L)	Number of Bins	Collection Frequency	Collection
Residential Tower 1 Bin Storage Area					
Refuse	5,976	660	Five	Twice each week	Private Contractor
Recycling	3,320	660	Three	Twice each week	Private Contractor
FOGO	664	240	Three	Twice each week	Private Contractor
Residential Tower 2 Bin Storage Area					
Refuse	9,252	660	Eight	Twice each week	Private Contractor
Recycling	5,140	660	Four	Twice each week	Private Contractor
FOGO	1,028	240	Three	Twice each week	Private Contractor
Residential Tower 3 Bin Storage Area					
Refuse	7,920	660	Six	Twice each week	Private Contractor
Recycling	4,400	660	Four	Twice each week	Private Contractor
FOGO	880	240	Two	Twice each week	Private Contractor
Residential Tower 4 Bin Storage Area					
Refuse	16,920	660	Thirteen	Twice each week	Private Contractor
Recycling	9,400	660	Eight	Twice each week	Private Contractor
FOGO	1,880	240	Four	Twice each week	Private Contractor
Commercial Bin Storage Area					
Refuse	4,043	660	Four	Twice each week	Private Contractor
Recycling	2,734	660	Three	Twice each week	Private Contractor

A private contractor will service the residential waste onsite, directly from the respective Tower's Bin Storage Area. The private contractor's waste collection vehicle will enter and exit the Proposal in forward gear via Subiaco Road.

A private contractor will also service the commercial waste directly from the Commercial Bin Storage Area from a truck stopping point on Court Place.

A building manager/caretaker will oversee the relevant aspects of waste management at the Proposal.

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1 Introduction

Community Housing (Subi East) Limited is seeking development approval for the proposed mixed-use development located at 154 Subiaco Road, Subiaco (the Proposal).

To satisfy the conditions of the development application the City of Subiaco (the City) requires the submission of a Waste Management Plan (WMP) that will identify how waste is to be stored and collected from the Proposal. Talis Consultants has been engaged to prepare this WMP to satisfy the City's requirements.

The Proposal is bordered by Railway Parade to the north, Court Place to the east, Subiaco Road to the south and residential properties to the west, as shown in [Figure 1](#).

1.1 Objectives and Scope

The objective of this WMP is to outline the equipment and procedures that will be adopted to manage waste (refuse, recyclables and FOGO) at the Proposal. Specifically, the WMP demonstrates that the Proposal is designed to:

- Adequately cater for the anticipated volume of waste to be generated;
- Provide adequately sized Bin Storage Areas, including appropriate bins; and
- Allow for efficient collection of bins by appropriate waste collection vehicles.

To achieve the objective, the scope of the WMP comprises:

- Section 2: Waste Generation;
- Section 3: Internal Transfer of Waste;
- Section 4: Waste Storage;
- Section 5: Waste Collection;
- Section 6: Waste Management; and
- Section 7: Conclusion.

2 Waste Generation

The following section shows the waste generation rates used and the estimated waste volumes to be generated at the Proposal.

2.1 Proposed Tenancies

The anticipated volume of refuse, recyclables and FOGO is based on the number of residential apartments and the floor area (m²) of the commercial tenancies at the Proposal. The Proposal consists of the following:

Tower 1 Bin Storage Area:

- One Bedroom Apartments – 47;
- Two Bedroom Apartments – 22; and
- Three Bedroom Apartments – 1.

Tower 2 Bin Storage Area:

- One Bedroom Apartments – 79;
- Two Bedroom Apartments – 29; and
- Three Bedroom Apartments – 2.

Tower 3 Bin Storage Area:

- One Bedroom Apartments – 53; and
- Two Bedroom Apartments – 38.

Tower 4 Bin Storage Area:

- One Bedroom Apartments – 109;
- Two Bedroom Apartments – 50; and
- Three Bedroom Apartments – 17.

Commercial Bin Storage Area:

- Tenancy 1 (assumed café) – 77m²;
- Tenancy 2 (assumed café) – 110m²; and
- CHL Office – 165m².

2.2 Waste Generation Rates

In order to achieve an accurate projection of waste volumes for the Proposal, consideration was given to the City of Subiaco's *Multi Use Developments and Commercial Waste Guidelines* (2017) and the City of Perth's *Waste Guidelines for New Developments* (Revision 5, effective from June 2019).

[Table 2-1](#) shows the waste generation rates which have been applied to the Proposal.

Table 2-1: Waste Generation Rates

Tenancy Use Type	Guideline Reference	*Refuse Generation Rate	Recycling Generation Rate	*FOGO Generation Rate
Residential				
One Bedroom Apartments	Subiaco – One Bedroom	72L/week	40L/week	8L/week
Two Bedroom Apartments	Subiaco – Two Bedroom	108L/week	60L/week	12L/week
Three Bedroom Apartments	Subiaco – Three Bedroom or greater	216L/week	120L/week	24L/week
Commercial				
Tenancy 1 (assumed café)	Perth – Café	300L/100m ² /day	200L/100m ² /day	-
Tenancy 2 (assumed café)	Perth – Café	300L/100m ² /day	200L/100m ² /day	-
CHL Office	Perth – Office	10L/100m ² /day	10L/100m ² /day	-

*The FOGO waste generation rate has been assumed as 10% of the refuse generation rate. Based on the Client's knowledge of existing developments it has been assumed that a small percentage of residents will participate in separating the third FOGO waste stream.

2.3 Waste Generation Volumes

Waste generation is estimated by volume in litres (L) as this is generally the influencing factor when considering bin size, numbers and storage space required.

2.3.1 Tower 1 Residential Waste

Waste generation volumes in litres per week (L/week) adopted for this waste assessment are shown in [Table 2-2](#). It is estimated that the Tower 1 residential apartments at the Proposal will generate 5,976L of refuse, 3,320L of recycling and 664L of FOGO each week.

Table 2-2: Estimated Waste Generation – Tower 1

Tower 1 Residential Apartments	Number of Apartments	Waste Generation Rate (L/week)	Waste Generation (L/week)
Refuse			
One Bedroom Apartments	47	72	3,384
Two Bedroom Apartments	22	108	2,376
Three Bedroom Apartments	1	216	216
Total			5,976
Recyclables			
One Bedroom Apartments	47	40	1,880
Two Bedroom Apartments	22	60	1,320
Three Bedroom Apartments	1	120	120
Total			3,320
FOGO			
One Bedroom Apartments	47	8	376
Two Bedroom Apartments	22	12	264
Three Bedroom Apartments	1	24	24
Total			664

2.3.2 Tower 2 Residential Waste

Waste generation volumes in litres per week (L/week) adopted for this waste assessment are shown in [Table 2-3](#). It is estimated that the Tower 2 residential apartments at the Proposal will generate 9,252L of refuse, 5,140L of recyclables and 1,028L of FOGO.

Table 2-3: Estimated Waste Generation – Tower 2

Tower 2 Residential Apartments	Number of Apartments	Waste Generation Rate (L/week)	Waste Generation (L/week)
Refuse			
One Bedroom Apartments	79	72	5,688
Two Bedroom Apartments	29	108	3,132
Three Bedroom Apartments	2	216	432
Total			9,252
Recyclables			
One Bedroom Apartments	79	40	3,160
Two Bedroom Apartments	29	60	1,740
Three Bedroom Apartments	2	120	240
Total			5,140
FOGO			
One Bedroom Apartments	79	8	632
Two Bedroom Apartments	29	12	348
Three Bedroom Apartments	2	24	48
Total			1,028

2.3.3 Tower 3 Residential Waste

Waste generation volumes in litres per week (L/week) adopted for this waste assessment are shown in [Table 2-4](#). It is estimated that the Tower 3 residential apartments at the Proposal will generate 7,920L of refuse, 4,400L of recyclables and 880L of FOGO each week.

Table 2-4: Estimated Waste Generation – Tower 3

Tower 3 Residential Apartments	Number of Apartments	Waste Generation Rate (L/week)	Waste Generation (L/week)
Refuse			
One Bedroom Apartments	53	72	3,816
Two Bedroom Apartments	38	108	4,104
Total			7,920
Recyclables			
One Bedroom Apartments	53	40	2,120
Two Bedroom Apartments	38	60	2,280
Total			4,400
FOGO			
One Bedroom Apartments	53	8	424
Two Bedroom Apartments	38	12	456
Total			880

2.3.4 Tower 4 Residential Waste

Waste generation volumes in litres per week (L/week) adopted for this waste assessment are shown in [Table 2-5](#). It is estimated that the Tower 4 residential apartments at the Proposal will generate 16,920L of refuse, 9,400L of recyclables and 1,880L of FOGO each week.

Table 2-5: Estimated Waste Generation – Tower 4

Tower 4 Residential Apartments	Number of Apartments	Waste Generation Rate (L/week)	Waste Generation (L/week)
Refuse			
One Bedroom Apartments	109	72	7,848
Two Bedroom Apartments	50	108	5,400
Three Bedroom Apartments	17	216	3,672
Total			16,920
Recyclables			
One Bedroom Apartments	109	40	4,360
Two Bedroom Apartments	50	60	3,000
Three Bedroom Apartments	17	120	2,040
Total			9,400
FOGO			
One Bedroom Apartments	109	8	872
Two Bedroom Apartments	50	12	600
Three Bedroom Apartments	17	24	408
Total			1,880

2.3.5 Commercial Waste

Waste generation volumes in litres per week (L/week) adopted for this waste assessment are shown in [Table 2-6](#). It is estimated that the commercial tenancies at the Proposal will generate 4,043L of refuse and 2,734L of recyclables each week.

Table 2-6: Estimated Waste Generation – Commercial

Commercial Tenancies	Area (m ²)	Waste Generation Rate (L/100m ² /day)	Waste Generation (L/week)
Refuse			
Tenancy 1 (assumed café)	77	300	1,617
Tenancy 2 (assumed café)	110	300	2,310
CHL Office	165	10	116
Total			4,043
Recyclables			
Tenancy 1 (assumed café)	77	200	1,078
Tenancy 2 (assumed café)	110	200	1,540
CHL Office	165	10	116
Total			2,734

3 Internal Transfer of Waste

To ensure that waste is managed appropriately at the Proposal, it is important to allow for sufficient space to accommodate the required quantity of bins within the Bin Storage Areas. The transfer of bins, quantity, size, and design of the Bin Storage Areas is described in the following sections.

All bins will be colour coded and labelled in accordance with Australian Standards (AS 4123.7) to assist residents, visitors, staff, and cleaners to dispose of their separate waste materials in the correct bins.

3.1 Residential Internal Bins

To promote positive recycling behaviour and maximise diversion from landfill, the residential apartments will have room to accommodate two under counter/kitchen bins for the separate disposal of refuse and recyclables and smaller bins/caddies for food organics.

The resident will then take the contents of their internal refuse and recycling bins to the respective Waste Chute Room on each residential level for disposal, as described below, and their internal FOGO kitchen caddy directly to the respective Tower's FOGO Bin Storage Area on the Upper Ground Level.

3.1.1 Waste Chute System

To assist with efficient disposal of waste to the Residential Bin Storage Areas, a dual chute waste system will be utilised within each of the Proposal's residential towers for refuse and recycling waste, as shown in [Diagram 1](#).

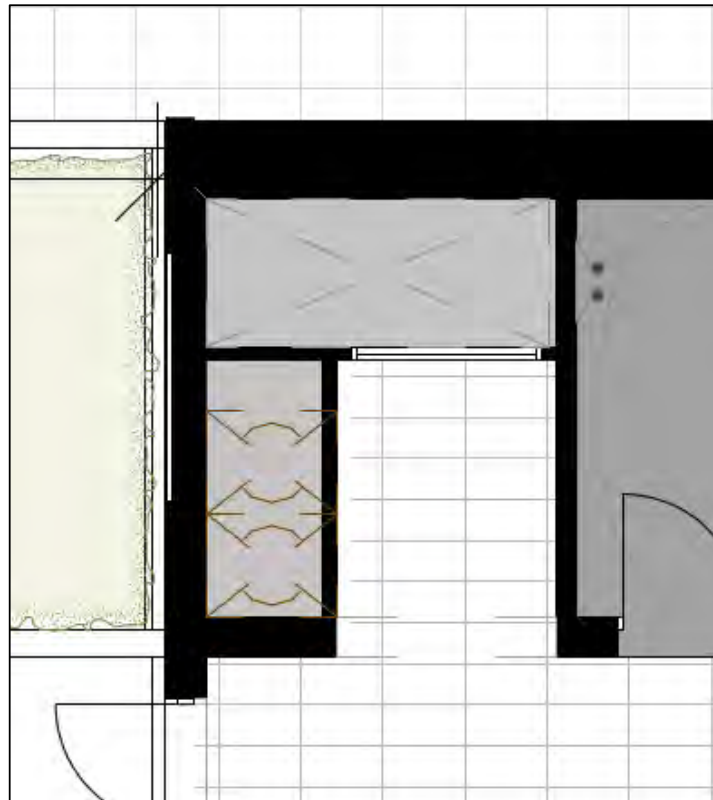
The waste chutes will be located in close proximity to the elevators on each residential level in each Tower, have self-closing doors with and bottom hinge and fire rated to AS1530.4-2005. Chutes are typically 610mm in diameter and are ventilated with an extraction fan at the top to reduce odour and insulated for noise reduction. The chutes will be routinely cleaned via chute flushing operations. Please note, the exact design of the systems to be utilised at the Proposal will be determined at a later date following discussions with waste chute providers as the designs are finalised.

The Proposal is intending to utilise a linear track system at the terminus of each of the waste chute systems to improve the efficiency of the building manager/caretaker manoeuvring bins within the Residential Bin Storage Areas. Each linear track system will be capable of accommodating two 660L bins which can be automatically or manually rotated to ensure the capture of waste material exiting the chute system. It can also be designed to send the building manager/caretaker a digital alert of the bins capacity to ensure the swapping of empty and full bins is done in a timely manner.

The building manager/caretaker will be required to manually swap full bins with empty bins on the linear track system, as necessary.

The building manager/caretaker will provide training materials for all new residents on how to use the waste system (typically within a new resident/tenant's handbook). The building manager/caretaker will advise residents of any changes to the waste system. In addition, signage will be included in the waste chute rooms to remind residents how to correctly dispose of their waste materials in the chute, and to provide relevant contact details for waste management at the Proposal.

Diagram 1: Example of a Typical Residential Waste Chute Room



3.2 Commercial Internal Bins

The commercial tenancies will also have a minimum of two internal bins to facilitate the separate disposal of refuse and recycling. These bins will be transferred by the tenants, staff or cleaners to the Commercial Bin Storage Area located on the Lower Ground Floor and deposited into the correct bin.

Internal bins from Tenancy 1 and Tenancy 2 located on the Lower Ground Floor will be transferred via the external pedestrian walkway along Court Place. Internal bins from the CHL Office located on the Upper Ground Floor will be transferred via the T4 lift to the Lower Ground Floor and then via the external pedestrian walkway along Court Place. This internal servicing method may be conducted outside of main operational hours to mitigate disturbances to residents/staff/visitors.

4 Waste Storage

Waste materials generated within the Proposal will be collected in the bins located in the Bin Storage Areas, as shown in [Diagram 2](#) to [Diagram 5](#), and discussed in the following sub-sections.

4.1 Bin Sizes

[Table 4-1](#) gives the typical dimensions of standard bins sizes that may be utilised at the Proposal. It should be noted that these bin dimensions are approximate and can vary slightly between suppliers.

Table 4-1: Typical Bin Dimensions

Dimensions (m)	Bin Sizes		
	240L	660L	1,100L
Depth	1,060	1,200	1,460
Width	730	780	1,090
Height	550	1,260	1,360

Reference: City of Subiaco's Multi Use Developments and Commercial Waste Guidelines (2017)

4.2 Tower 1 Bin Storage Area Size

To ensure sufficient area is available for storage of the Tower 1 bins, the amount of bins required for the Bin Storage Area was modelled utilising the estimated waste generation in [Table 2-2](#), bin sizes in [Table 4-1](#) and based on collection of refuse and recyclables twice each week.

Based on the results shown in [Table 4-2](#) the Tower 1 Bin Storage Area has been sized to accommodate:

- Five 660L refuse bins;
- Three 660L recycling bins; and
- Three 240L FOGO bins.

Table 4-2: Bin Requirements for Bin Storage Area – Tower 1

Waste Stream	Waste Generation (L/week)	Number of Bins Required		
		240L	660L	1,100L
Refuse	5,976	13	5	3
Recycling	3,320	7	3	2
FOGO	664	3	-	-

The configuration of these bins within the Tower 1 Bin Storage Area is shown in [Diagram 2](#). It is worth noting that the number of bins and corresponding placement of bins shown in [Diagram 2](#) represents the maximum requirements assuming two collections each week of refuse, recyclables and FOGO.

Diagram 2: Tower 1 Bin Storage Area



4.3 Tower 2 Bin Storage Area Size

To ensure sufficient area is available for storage of the Tower 2 bins, the amount of bins required for the Bin Storage Area was modelled utilising the estimated waste generation in [Table 2-3](#), bin sizes in [Table 4-1](#) and based on collection of refuse and recyclables twice each week.

Based on the results shown in [Table 4-3](#) the Tower 2 Bin Storage Area has been sized to accommodate:

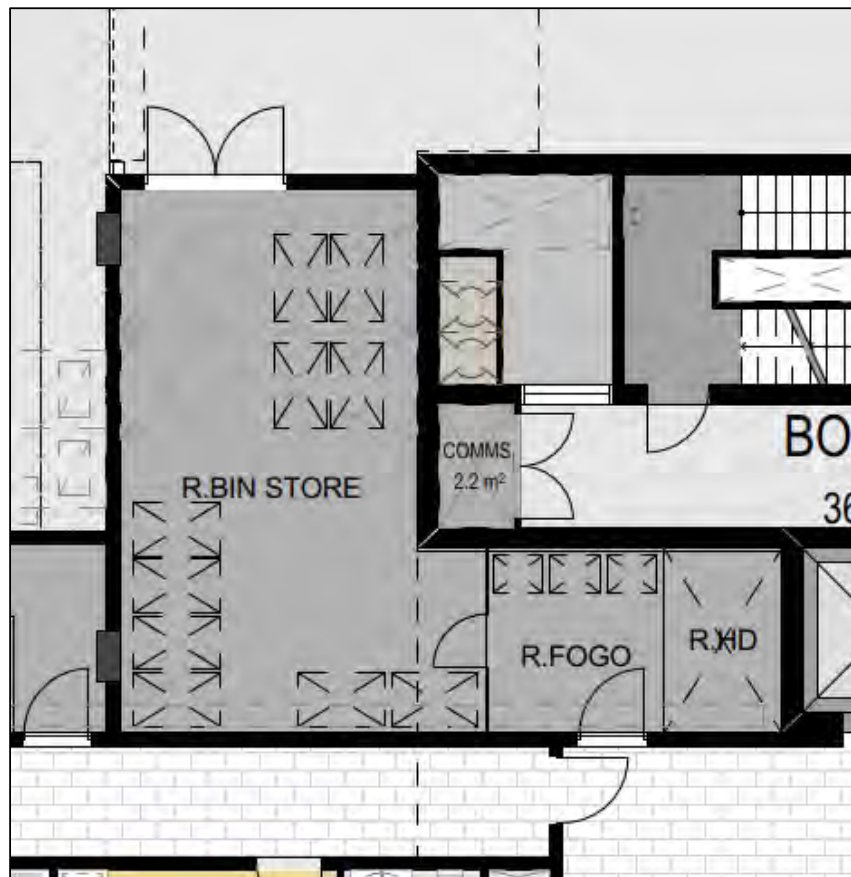
- Eight 660L refuse bins;
- Four 660L recycling bins; and
- Three 240L FOGO bins.

Table 4-3: Bin Requirements for Bin Storage Area – Tower 2

Waste Stream	Waste Generation (L/week)	Number of Bins Required		
		240L	660L	1,100L
Refuse	9,252	20	8	5
Recycling	5,140	11	4	3
FOGO	1,028	3	-	-

The configuration of these bins within the Tower 2 Bin Storage Area is shown in [Diagram 3](#). It is worth noting that the number of bins and corresponding placement of bins shown in [Diagram 3](#) represents the maximum requirements assuming two collections each week of refuse, recyclables and FOGO.

Diagram 3: Tower 2 Bin Storage Area



4.4 Tower 3 Bin Storage Area Size

To ensure sufficient area is available for storage of the Tower 3 bins, the amount of bins required for the Bin Storage Area was modelled utilising the estimated waste generation in [Table 2-4](#), bin sizes in [Table 4-1](#) and based on collection of refuse and recyclables two times each week.

Based on the results shown in [Table 4-4](#) the Tower 3 Bin Storage Area has been sized to accommodate:

- Six 660L refuse bins;
- Four 660L recycling bins; and
- Two 240L FOGO bins.

Table 4-4: Bin Requirements for Bin Storage Area – Tower 3

Waste Stream	Waste Generation (L/week)	Number of Bins Required		
		240L	660L	1,100L
Refuse	7,920	17	6	4
Recycling	4,400	10	4	2
FOGO	880	2	-	-

The configuration of these bins within the Tower 3 Bin Storage Area is shown in [Diagram 4](#). It is worth noting that the number of bins and corresponding placement of bins shown in [Diagram 4](#) represents the maximum requirements assuming two collections each week of refuse, recyclables and FOGO.

Diagram 4: Tower 3 Bin Storage Area



4.5 Tower 4 Bin Storage Area Size

To ensure sufficient area is available for storage of the Tower 4 bins, the amount of bins required for the Bin Storage Area was modelled utilising the estimated waste generation in [Table 2-5](#), bin sizes in [Table 4-1](#) and based on collection of refuse and recyclables twice each week.

Based on the results shown in [Table 4-5](#) the Tower 4 Bin Storage Area has been sized to accommodate:

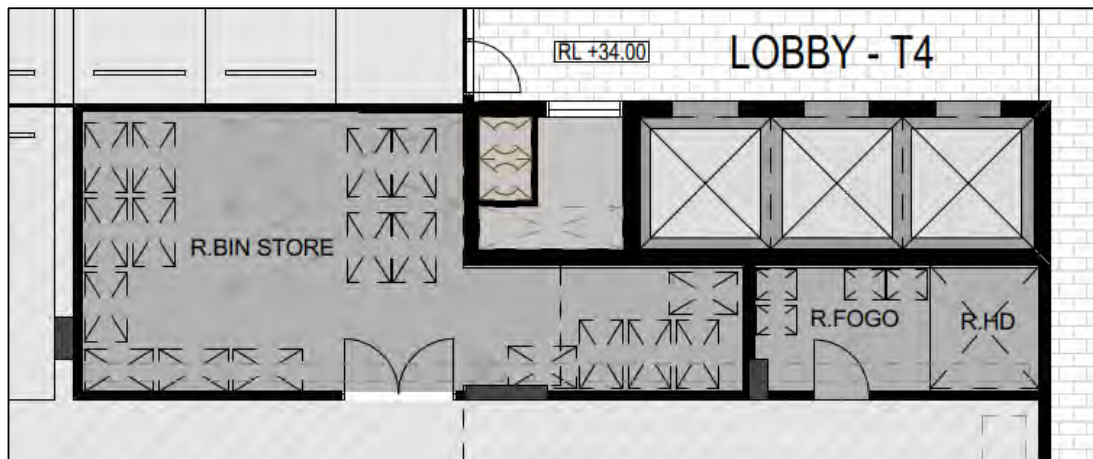
- Thirteen 660L refuse bins;
- Eight 660L recycling bins; and
- Four 240L FOGO bins.

Table 4-5: Bin Requirements for Bin Storage Area – Tower 4

Waste Stream	Waste Generation (L/week)	Number of Bins Required		
		240L	660L	1,100L
Refuse	16,920	36	13	8
Recycling	9,400	20	8	5
FOGO	1,880	4	-	-

The configuration of these bins within the Tower 4 Bin Storage Area is shown in [Diagram 5](#). It is worth noting that the number of bins and corresponding placement of bins shown in [Diagram 5](#) represents the maximum requirements assuming two collections each week of refuse, recyclables and FOGO.

Diagram 5: Tower 4 Bin Storage Area



4.6 Commercial Bin Storage Area Size

To ensure sufficient area is available for storage of the commercial bins, the amount of bins required for the Commercial Bin Storage Area was modelled utilising the estimated waste generation in [Table 2-6](#), bin sizes in [Table 4-1](#) and based on collection of refuse and recyclables twice each week.

Based on the results shown in [Table 4-6](#) the Commercial Bin Storage Area has been sized to accommodate:

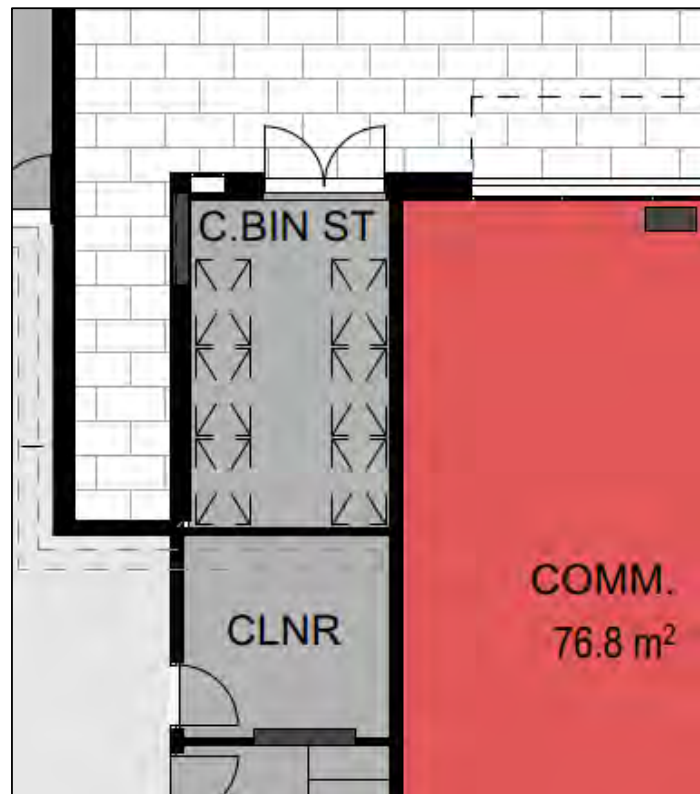
- Four 660L refuse bins; and
- Three 660L recycling bins.

Table 4-6: Bin Requirements for Bin Storage Area – Commercial

Waste Stream	Waste Generation (L/week)	Number of Bins Required		
		240L	660L	1,100L
Refuse	4,043	9	4	2
Recycling	2,734	6	3	2

The configuration of these bins within the Commercial Bin Storage Area is shown in [Diagram 6](#). It is worth noting that the number of bins and corresponding placement of bins shown in [Diagram 6](#) represents the maximum requirements assuming two collections each week of refuse and recyclables.

Diagram 6: Commercial Bin Storage Area



4.7 Bin Storage Area Design

The design of the Bin Storage Areas will take into consideration:

- Smooth impervious floor sloped to a drain connected to the sewer system;
- Taps for washing of bins and Bin Storage Areas;
- Adequate aisle width for easy manoeuvring of bins;
- Doors to the Bin Storage Areas self-closing and vermin proof;
- Doors to the Bin Storage Areas wide enough to fit bins through;
- Ventilated to a suitable standard;
- Appropriate signage;
- Undercover where possible and be designed to not permit stormwater to enter the drain;
- Located behind the building setback line;
- Bins not to be visible from the property boundary or areas trafficable by the public; and
- Bins are reasonably secured from theft and vandalism.

Bin numbers and storage space within the Bin Storage Areas will be monitored by the building manager/caretaker during the operation of the Proposal to ensure that the number of bins and collection frequency is sufficient.

5 Waste Collection

A private contractor will service the Proposal and provide the bins and collection frequencies outlined in Section 4 and discussed in the following sub-sections.

5.1 Residential Waste

A private contractor (likely Veolia) will collect residential refuse, recyclables and FOGO twice each week utilising a low-profile rear loader waste collection vehicle (2.4m height clearance).

The private contractor's rear loader waste collection vehicle will service the bins onsite, directly from the respective Tower's Bin Storage Area. The private contractor's rear loader waste collection vehicle will travel with left hand lane traffic flow on Subiaco Road, turn into the eastern access of the Upper Ground Floor in forward gear and reverse into the collection area adjacent to the Tower 4 Bin Storage Area for servicing. Upon completion, the vehicle will proceed forward and pull up adjacent to the Tower 3 and Tower 2 Bin Storage Areas for servicing. The private contractor's rear loader vehicle will then exit via the western access of the Upper Ground Floor, perform a turnaround at Court Place, and re-enter via the western access of the Upper Ground Floor to service the Tower 1 Bin Storage Area.

The Upper Ground Floor will have adequate height clearances for the rear loader waste collection vehicle to enter and exit the Proposal (3.2m height clearance).

It is proposed that servicing will be conducted during school operating hours to mitigate impacts on local traffic movements during peak hours from the nearby Bob Hawke College.

Private contractor's staff will ferry bins to and from the rear loader waste collection vehicle and the respective Tower's Bin Storage Area during servicing. The private contractor will be provided with key access to the Proposal and security access gates to facilitate servicing, if required.

Once servicing is complete the private contractor's rear loader waste collection vehicle will exit in a forward motion, turning onto Subiaco Road moving with traffic flow.

The above servicing method will preserve the amenity of the area by removing the requirement for bins to be presented to the street on collection days. In addition, servicing of bins onsite will reduce the noise generated in the area during collection. Noise from waste vehicles must comply with the Environmental Protection (Noise) Regulations and such vehicles should not service the site before 7.00am or after 7.00pm Monday to Saturday, or before 9.00am or after 7.00pm on Sundays and Public Holidays.

The ability for the private contractor's rear loader waste collection vehicle to access the Proposal in a safe manner has been assessed by PTG Consulting and further information will be included within their traffic impact statement.

5.2 Commercial Waste

A private contractor will also service the commercial tenancies at the Proposal twice each week utilising its rear loader waste collection vehicle.

The private contractor's rear loader waste collection vehicle will service the bins directly from the Commercial Bin Storage Area, from a stopping point on Court Place.

The private contractor will ferry bins to and from the rear loader waste collection vehicle and the Commercial Bin Storage Area during servicing. The travel path between the Commercial Bin Storage Area and the waste collection vehicle will be of flat surface and kept free of obstacles.

The private contractor will be provided with key access to the Proposal and security access gates to facilitate servicing, if required.

Once servicing is complete the private contractor's rear loader waste collection vehicle will exit in a forward motion, turning onto Court Place moving with traffic flow.

5.3 Bulk and Speciality Waste

To assist with the reduction of illegal dumping of bulk wastes, Bulk Waste Storage Areas have been provided on the Upper Ground Level of the Proposal for the temporary storage of bulk wastes. In addition, each resident has an allocated storage area which may also be used, if required.

The City offers two residential verge bulk waste collections each year. The building manager/caretaker will liaise with residents and tenants on procedures for bulky waste disposal within the Proposal, as required.

Residents are also entitled to two discounted tip passes per property each year to the West Metro Recycling Centre for disposal of additional bulk waste throughout the year. In addition, household hazardous and other recyclable items not accepted in the bin system can be disposed of free at the centre.

6 Waste Management

A building manager/caretaker will be engaged to complete the following tasks:

- Monitoring and maintenance of bins and the Bin Storage Areas;
- Cleaning of bins and Bin Storage Areas, when required;
- Exchange full bins with empty bins at the terminus of the waste chute in each Tower's Bin Storage Area, as required;
- Ensure all residents, tenants, staff, and cleaners at the Proposal are made aware of this WMP and their responsibilities thereunder;
- Monitor resident, tenant, staff, and cleaner behaviour and identify requirements for further education and/or signage;
- Monitor bulk and speciality waste accumulation and assist with its removal, as required;
- Regularly engage with residents, tenants, staff, and cleaners to develop opportunities to reduce waste volumes and increase resource recovery; and
- Regularly engage with the private contractors to ensure efficient and effective waste service is maintained.

7 Conclusion

As demonstrated within this WMP, the Proposal provides sufficiently sized Bin Storage Areas for storage of refuse, recyclables and FOGO, based on the estimated waste generation volumes and suitable configuration of bins. This indicates that adequately designed Bin Storage Areas have been provided, and collection of refuse, recyclables and FOGO can be completed from the Proposal.

A private contractor will service the residential waste onsite, directly from the respective Tower's Bin Storage Area. The private contractor's waste collection vehicle will enter and exit the Proposal in forward gear via Subiaco Road.

A private contractor will also service the commercial waste directly from the Commercial Bin Storage Area from a truck stopping point on Court Place.

A building manager/caretaker will oversee the relevant aspects of waste management at the Proposal.

DRAFT

Figures

Figure 1: Locality Plan

DRAFT

Subiaco LRC Meeting - Development Management

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LEGEND

- Site Boundary
- Cadastral**
- Crown Allotment
- Freehold
- Road
- Strata Plan or Lot
- Easement
- Reserve

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LOCALITY

0 5 10 15 20 km

LOCALITY

154 Subiaco Rd
Subiaco WA 6008

Community Housing (Subi East)
Ltd

0 10 20 30 40 m

Coordinate System: GDA2020 MGA Zone 50
Scale @ A3: 1:1,000

Prepared: A Barley	Date: 24/06/2025
Reviewed: C Andersen	Revision: A
Project: WMP25053	

Figure 01

Data source: Roads, Cadastre - Landgate, 2025; Imagery: Nearmap, 2025
Document Path: \\server\talis\SECTIONS\Waste\PROJECTS\WMP2025\WMP25053 - 154 Subiaco Road, Subiaco\GIS\Map\WMP25053 - 154 Subiaco Road, Subiaco\WMP25053 - 154 Subiaco Road, Subiaco.aprx



Assets | Engineering | Environment | Noise | Spatial | Waste

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COMMUNITY HOUSING (SUBI EAST) LIMITED

**154 SUBIACO ROAD
SUBIACO**

SPP 5.4 NOISE MANAGEMENT PLAN

JANUARY 2026

OUR REFERENCE: 34788-3-25214

Rochdale Holdings Pty Ltd A.B.N. 85 009 049 067 trading as:
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Herring Storer Acoustics

DOCUMENT CONTROL PAGE

SPP 5.4 NOISE MANAGEMENT PLAN
154 SUBIACO ROAD, SUBIACO

Job No: 25214

Document Reference: 34788-3-25214

FOR

COMMUNITY HOUSING (SUBI EAST) LIMITED

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Herring Storer Acoustics

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3.	MEASUREMENT	4
4.	MODELLING	5
5.	RESULTS	5
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APPENDICES

A	Plans
B	Glazing Requirements

1. INTRODUCTION

Herring Storer Acoustics were commissioned to carry out an acoustic study with regards to passenger rail noise for a residential development at 154 Subiaco Road, Subiaco. The purpose of the study was to assess the noise that would be received within the development area from the Perth to Fremantle Passenger Rail Line and to compare the results with accepted criteria and if exceedances exist, develop the framework for the management of noise.

A plan is attached in Appendix A.

2. ACOUSTIC CRITERIA

The Western Australian Planning Commission (WAPC) released on 6th September 2019 State Planning Policy 5.4 "Road and Rail Noise". The requirements of State Planning Policy 5.4 are outlined below.

POLICY APPLICATION (Section 4)

When and where it applies (Section 4.1)

SPP 5.4 applies to the preparation and assessment of planning instruments, including region and local planning schemes; planning strategies, structure plans; subdivision and development proposals in Western Australia, where there is proposed:

- a) *noise-sensitive land-use within the policy's trigger distance of a transport corridor as specified in **Table 1**;*
- b) *New or major upgrades of roads as specified in **Table 1** and maps (**Schedule 1, 2 and 3**); or*
- c) *New railways or major upgrades of railways as specified in maps (**Schedule 1, 2 and 3**); or any other works that increase capacity for rail vehicle storage or movement and will result in an increased level of noise.*

Policy trigger distances (Section 4.1.2)

Table 1 identifies the State's transport corridors and the trigger distances to which the policy applies.

*The designation of land within the trigger distances outlined in **Table 1** should not be interpreted to imply that land is affected by noise and/or that areas outside the trigger distances are unaffected by noise.*

*Where any part of the lot is within the specified trigger distance, an assessment against the policy is required to determine the likely level of transport noise and management/ mitigation required. An initial screening assessment (**guidelines: Table 2: noise exposure forecast**) will determine if the lot is affected and to what extent."*

TABLE 1: TRANSPORT CORRIDOR CLASSIFICATION AND TRIGGER DISTANCES

Transport corridor classification	Trigger distance	Distance measured from
Roads		
Strategic freight and major traffic routes <i>Roads as defined by Perth and Peel Planning Frameworks and/or roads with either 500 or more Class 7 to 12 Austroads vehicles per day, and/or 50,000 per day traffic volume</i>	300 metres	Road carriageway edge
Other significant freight/traffic routes <i>These are generally any State administered road and/or local government road identified as being a future State administered road (red road) and other roads that meet the criteria of either >=23,000 daily traffic count (averaged equivalent to 25,000 vehicles passenger car units under region schemes)</i>	200 metres	Road carriageway edge
Passenger railways		
	100 metres	Centreline of the closest track
Freight railways		
	200 metres	Centreline of the closest track

Proponents are advised to consult with the decision making authority as site specific conditions (significant differences in ground levels, extreme noise levels) may influence the noise mitigation measures required, that may extend beyond the trigger distance.

POLICY MEASURES (Section 6)

The policy applies a performance-based approach to the management and mitigation of transport noise. The policy measures and resultant noise mitigation will be influenced by the function of the transport corridor and the type and intensity of the land-use proposed. Where there is risk of future land-use conflict in close proximity to strategic freight routes, a precautionary approach should be applied. Planning should also consider other broader planning policies. This is to ensure a balanced approach takes into consideration reasonable and practical considerations.

Noise Targets (Section 6.1)

Table 2 sets out noise targets that are to be achieved by proposals under which the policy applies. Where exceeded, an assessment is required to determine the likely level of transport noise and management/mitigation required.

In the application of the noise targets the objective is to achieve:

- indoor noise levels as specified in **Table 2** in noise sensitive areas (for example, bedrooms and living rooms of houses, and school classrooms); and
- a reasonable degree of acoustic amenity for outdoor living areas on each residential lot. For non-residential noise-sensitive developments, for example schools and child care centres the design of outdoor areas should take into consideration the noise target.

It is recognised that in some instances, it may not be reasonable and/or practicable to meet the outdoor noise targets. Where transport noise is above the noise targets, measures are expected to be implemented that balance reasonable and practicable considerations with the need to achieve acceptable noise protection outcomes.

TABLE 2: NOISE TARGETS

Proposals	New/Upgrade	Noise Targets		
		Outdoor		Indoor
		Day ($L_{Aeq}(\text{Day})$ dB) (6 am-10 pm)	Night ($L_{Aeq}(\text{Night})$ dB) (10 pm-6 am)	(L_{Aeq} dB)
Noise-sensitive land-use and/or development	New noise sensitive land use and/or development within the trigger distance of an existing/proposed transport corridor	55	50	L_{Aeq} (Day) 40(Living and work areas) L_{Aeq} (Night) 35 (bedrooms)
Roads	New	55	50	N/A
	Upgrade	60	55	N/A
Railways	New	55	50	N/A
	Upgrade	60	55	N/A

Notes:

- The noise target is to be measured at one metre from the most exposed, habitable façade of the proposed building, which has the greatest exposure to the noise-source. A habitable room has the same meaning as defined in State Planning Policy 3.1 Residential Design Codes.
- For all noise-sensitive land-use and/or development, indoor noise targets for other room usages may be reasonably drawn from Table 1 of Australian Standard/New Zealand Standard AS/NZS 2107:2016 Acoustics – Recommended design sound levels and reverberation times for building interiors (as amended) for each relevant time period.
- The 5dB difference in the criteria between new and upgrade infrastructure proposals acknowledges the challenges in achieving noise level reduction where existing infrastructure is surrounded by existing noise-sensitive development.
- Outdoor targets are to be met at all outdoor areas as far as is reasonable and practical to do so using the various noise mitigation measures outlined in the guidelines. For example, it is likely unreasonable for a transport infrastructure provider to achieve the outdoor targets at more than 1 or 2 floors of an adjacent development with direct line of sight to the traffic.

Noise Exposure Forecast (Section 6.2)

When it is determined that SPP 5.4 applies to a planning proposal as outlined in Section 4, proponents and/or decision makers are required to undertake a preliminary assessment using **Table 2**: noise exposure forecast in the guidelines. This will provide an estimate of the potential noise impacts on noise-sensitive land-use and/or development within the trigger distance of a specified transport corridor. The outcomes of the initial assessment will determine whether:

- no further measures are required.
- noise-sensitive land-use and/or development is acceptable subject to deemed-to-comply mitigation measures; or
- noise-sensitive land-use and/or development is not recommended. Any noise-sensitive land-use and/or development is subject to mitigation measures outlined in a noise management plan.”

3. MEASUREMENT

A measurement of the noise emissions associated with passenger trains on the Fremantle line was undertaken at 11 am on the 16th of May 2025. The measurement location was on the footpath adjacent to the rail line north of the lot, approximately 12 m from the centre line of the nearest track, illustrated in Figure 3.1.

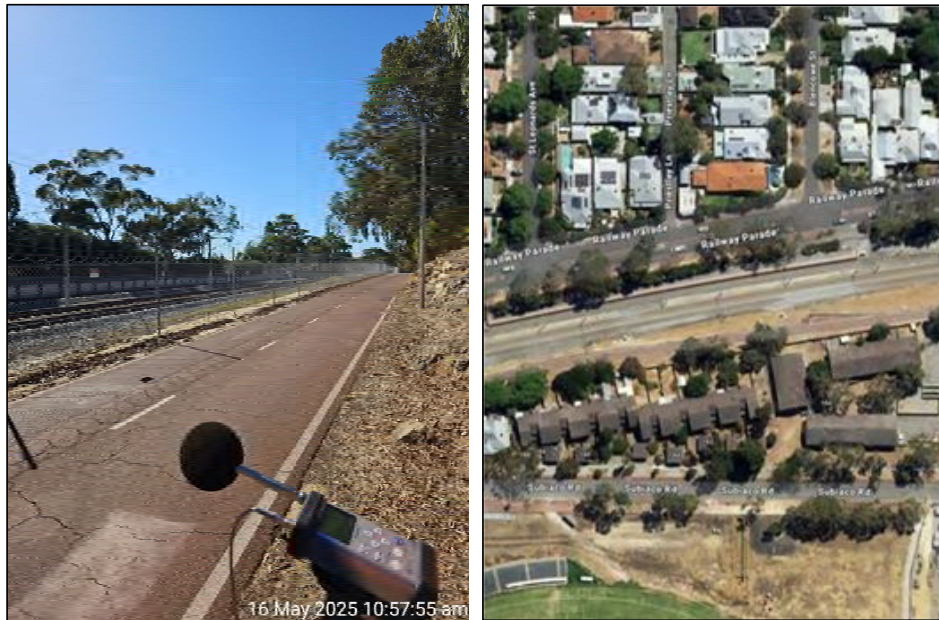


FIGURE 3.1 – MEASUREMENT LOCATION

TABLE 3.1 – PASSENGER TRAIN MEASUREMENT

Vehicle	Direction	Duration	L _{Amax} (dB(A))	L _{Aeq} (dB(A))
A-Series (4 car)	Westbound	13s	81	77
A Series (4 car)	Eastbound	13s	72	67

Using the L_{Aeq} of the highest train pass and current timetabled train movement volumes the L_{Aeq(day)} and L_{Aeq(night)} were determined. The calculated L_{Aeq(day)} was used to calibrate the noise model.

TABLE 3.2 – PASSENGER TRAIN MEASUREMENT

Period	Number of Trains	Duration	L _{Aeq(period)} (dB(A))
Day (6am to 10 pm)	110	13s	58
Night (10pm to 6am)	26	13s	52

4. MODELLING

Using the measured levels of the train as a basis for calibration, predictive noise modelling using Soundplan has been carried out.

The input data for the model included:

- Topographical data, with the ground level within the development based on natural ground levels as per Google Earth.
- Architectural plans (included in Appendix A)
- Current passenger rail volumes as per information contained on <https://www.transperth.wa.gov.au/timetables>.
- A +2.5 dB adjustment to allow for façade reflection.
- Calibrated noise levels for the $L_{Aeq(day)}$ and $L_{Aeq(night)}$ as per the measured noise levels, summarised in Table 4.2. Note calibration is against the current traffic flow.
- Existing residential housing / building throughout the study area.

5. RESULTS

Noise levels received at the façade of the development exceed the noise targets set out in *State Planning Policy 5.4*. The highest noise levels received for reach tower in the development is 61 dB(A) which places them in exposure category B. The requirements of the development are summarised in Table 5.1. It is noted that all requirements pertain only to the acoustic advice with respect to State Planning Policy 5.4 and may be superseded by other requirements (BAL, thermal, etc).

TABLE 5.1 – QUIET HOUSE DESIGN REQUIREMENTS

Quiet House Design Requirements	
Item	Requirements
Walls	$R_w + C_{tr}$ 50 dB
External Doors	Individual external door requirements to meet specifications as listed in the table of Appendix B.
Windows	Individual glazing requirements to meet specifications as listed in the table of Appendix B.
Roofs and Ceilings of Highest Floors	$R_w + C_{tr}$ 35 dB
Outdoor Living Area	Noise levels at the Outdoor Areas meet the criteria.
Mechanical Ventilation	Mechanical ventilation to not compromise the performance of any of the façade elements.
Notification on Title	Required

Noise levels received at the outdoor areas on the east and west podiums are 44 and 50 dB(A) respectively, which meets the outdoor noise criteria.

6. CONCLUSION

In accordance with the WAPC Planning Policy 5.4, an assessment of the noise that would be received within the development of 154 Subiaco Road, Subiaco, from future passenger rail travelling on the Perth to Armadale Train Line has been undertaken.

In accordance with the Policy, the following would be the acoustic criteria applicable to this project:

External

Day	55 dB(A) L_{Aeq}
Night	50 dB(A) L_{Aeq}

Internal

Sleeping Areas	35 dB(A) $L_{Aeq(night)}$
Living Areas	40 dB(A) $L_{Aeq(day)}$

The results of the acoustic assessment indicate that noise received at the development from future traffic volumes will exceed the external noise level criteria. The exposure category for all buildings in the development is category B. Glazing requirements to meet the target noise levels are listed in Appendix B.

It has been assumed that walls will be constructed of tilt up concrete panel or similar. If lightweight construction is to be considered, an investigation into the appropriateness of the proposed construction would be required.

APPENDIX A

PLANS

UPPER GROUND



1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE CITY OF SUBIACO ZONING ORDINANCE AND THE SUBIACO DEVELOPMENT APPLICATION.

DKO
 DESIGN KIM O'NEILL
 ARCHITECTS
 1000 N. 10TH AVE.
 SUITE 100
 COVINGTON, WA 98943
 TEL: 509.253.1000
 WWW.DKOARCHITECTS.COM

DEVELOPMENT APPLICATION
 Overall - Upper Ground Plan
 Project No: DA201
 Sheet No: 03

LEVEL 02 / TOP OF PODIUM



NO.	REV.	DATE	BY	CHKD.	DESCRIPTION
1	01	10/10/2023	DKO	DKO	ISSUED FOR PERMIT

1. ALL DIMENSIONS ARE IN METERS UNLESS OTHERWISE SPECIFIED.
 2. ALL ROOMS SHALL BE FINISHED TO THE FINISHES SPECIFIED IN THE SCHEDULE.
 3. ALL WALLS SHALL BE 100MM THICK UNLESS OTHERWISE SPECIFIED.
 4. ALL FLOORS SHALL BE FINISHED TO THE FINISHES SPECIFIED IN THE SCHEDULE.
 5. ALL CEILING SHALL BE FINISHED TO THE FINISHES SPECIFIED IN THE SCHEDULE.
 6. ALL ROOF SHALL BE FINISHED TO THE FINISHES SPECIFIED IN THE SCHEDULE.
 7. ALL SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE RELEVANT STANDARDS.
 8. ALL SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE RELEVANT STANDARDS.
 9. ALL SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE RELEVANT STANDARDS.
 10. ALL SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE RELEVANT STANDARDS.

DKO

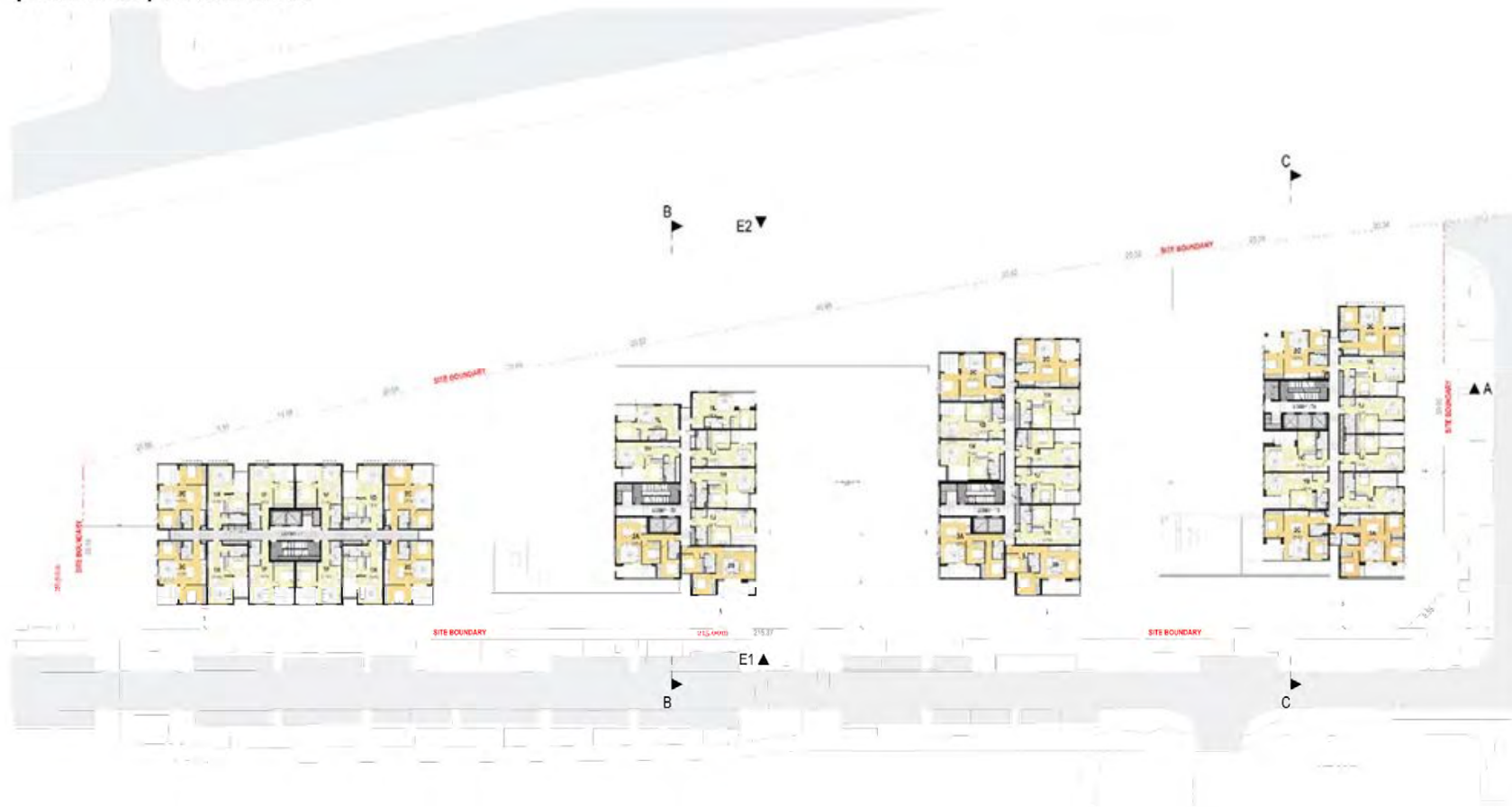
DKO ARCHITECTS
 10/10/2023
 10/10/2023
 10/10/2023

NOT FOR CONSTRUCTION
 DEVELOPMENT APPLICATION

Overall - Level 02 / Podium

DA203 03

LEVEL 03-06 / TYPICAL PLATES



NO.	REV.	DATE	DESCRIPTION
1	0	08/11/2023	ISSUED FOR PERMIT
2	1	08/11/2023	REVISIONS TO PERMIT
3	2	08/11/2023	REVISIONS TO PERMIT

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 DEVELOPMENT APPLICATION
 Project Name: Overall - Level 03-06 / Typical Plates
 Project No: DA204
 Date: 08/11/2023

APPENDIX B

GLAZING REQUIREMENTS

Subiaco LRC Meeting - Development Management

R_w+Ctr REQUIREMENTS									
Tower*	Facade	Floors up to	L _{Aeq}	Bedrooms Window to Floor Ratio			Living Room Window to Floor Ratio		
				0.3	0.5	0.8	0.3	0.5	0.8
1	N	6	61	28	30	32	23	30	32
	E	4	46	23	23	23	23	23	23
	E	6	55	23	24	26	23	24	26
	S	6	46	23	23	23	23	23	23
	W	4	52	23	23	23	23	23	23
	W	6	58	25	27	29	23	27	29
2	N	4	55	23	24	26	23	24	26
	N	8	61	28	30	32	23	30	32
	N	14	58	25	27	29	23	27	29
	E	14	52	23	23	23	23	23	23
	S	14	43	23	23	23	23	23	23
	W	5	52	23	23	23	23	23	23
3	W	14	55	23	24	26	23	24	26
	N	4	55	23	24	26	23	24	26
	N	10	61	28	30	32	23	30	32
	E	9	55	23	24	26	23	24	26
	E	10	52	23	23	23	23	23	23
	S	10	43	23	23	23	23	23	23
4	W	4	52	23	23	23	23	23	23
	W	10	58	25	27	29	23	27	29
	N	10	61	28	30	32	23	30	32
	N	17	58	25	27	29	23	27	29
	E	4	52	23	23	23	23	23	23
	E	17	55	23	24	26	23	24	26
4	S	17	40	23	23	23	23	23	23
	W	10	55	23	24	26	23	24	26
	W	17	52	23	23	23	23	23	23

*Towers numbered from west to east.

COURT PLACE

PERTH, WA

PEDESTRIAN WIND ASSESSMENT
PROJECT # 2512726
20 JANUARY 2026



SUBMITTED TO

**Community Housing (Subi
East) Limited**
19 Catherine Street
Subiaco, WA 6008

SUBMITTED BY

RWDI Australia Pty Ltd
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DOCUMENT CONTROL



Version	Status	Date	Prepared By	Reviewed By
A	Initial	18 December 2025	AMC	RL
B	Final – Review of DA Set	20 January 2026	AMC	RL

NOTE

The information contained in this document produced by RWDI is solely for the use of the client identified on the front page of this report. Our client becomes the owner of this document upon full payment of our **Tax Invoice** for its provision. This document must not be used for any purposes other than those of the document's owner. RWDI undertakes no duty to or accepts any responsibility to any third party who may rely upon this document.

QUALITY ASSURANCE

RWDI Australia Pty Ltd operates a Quality Management System which complies with the requirements of AS/NZS ISO 9001:2015. This management system has been externally certified by SAI Global and Licence No. QEC 13457 has been issued for the following scope: The provision of consultancy services in acoustic engineering, air quality and wind engineering; and the sale, service, support and installation of acoustic monitoring and related systems and technologies.



1. INTRODUCTION

RWDI Australia Pty Ltd (RWDI) was retained to undertake a pedestrian wind assessment of the Proposed Development, known as Court Place, located at 154 Subiaco Road in Subiaco, WA. The development site is situated to the north of Bob Hawke College Oval and is bounded by Subiaco Road to the south and Court Place to the east. Market Square lies to the south-west and Mueller Park to the south-east. The location of the site within its broader existing surrounding context is shown in Image 1.

The proposal comprises four residential towers, with heights ranging from 7 to 19 storeys, situated along Subiaco Road and the Railway corridor with a local rise in topography along the road from east to west. The towers are situated on a common podium sub-structure with communal amenity areas situated atop the 2-storey podium between the towers. Image 2 shows a layout plan for the development.

This desk-based report provides a review of the potential wind conditions around the site and offers conceptual wind control measures and design advice suitable for early design of the development. The key outdoor pedestrian accessible areas of interest associated with the development include the pedestrian footpaths around the site, the entrances to the development, and the amenity spaces on ground and upper levels.



Image 1: Project Location and Surrounding Context
Source: Nearmap

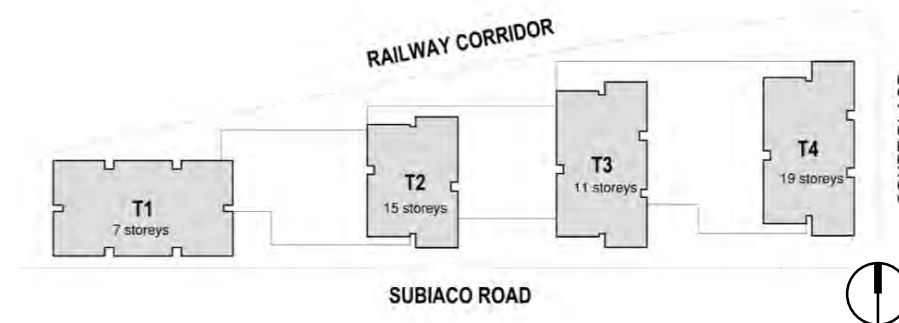


Image 2: Development Layout Plan

2. METHODOLOGY



Predicting wind speeds and occurrence frequencies around a building is a complex process, encompassing an assessment of factors such as building geometry, orientation, surrounding building heights and positions, terrain upstream, and the local wind climate. RWDI has amassed extensive expertise through conducting numerous wind tunnel studies and Computational Fluid Dynamics (CFD) assessments specifically focused on pedestrian wind conditions around buildings. This wealth of experience, complemented by comprehensive literature, facilitates a reliable and efficient desktop estimation of pedestrian wind conditions for concept designs without the need for wind-tunnel testing or detailed CFD studies.

Note that this approach delivers a preliminary assessment of potential wind conditions around the site. It offers a conceptual framework for wind control measures aimed at enhancing wind comfort, if and where necessary. To further validate and refine the predicted conditions discussed in this report or to fine-tune the suggested wind control measures, physical scale model tests in a boundary layer wind tunnel or CFD simulations will be required. These detailed studies offer a quantitative validation and enable a more detailed assessment, ensuring the accuracy and effectiveness of proposed wind control strategies.

Therefore, RWDI's assessment is based on the following:

- A review of the regional long-term meteorological data;
- Drawings and information received by RWDI in January 2026.
- Wind-tunnel studies, CFD simulations, and desktop assessments undertaken by the microclimate team for projects in the region;
- RWDI's engineering judgement, experience, and expert knowledge of wind flows around buildings^{1,2}; and,
- Assessment against a suitable criteria for pedestrian wind comfort and safety.

Note that other microclimate issues such as those relating to cladding and structural wind loads, door operability, building air quality, noise, vibration, etc. are not part of the scope of this assessment.

-
1. H. Wu and F. Kriksic (2012). "Designing for Pedestrian Comfort in Response to Local Climate", *Journal of Wind Engineering and Industrial Aerodynamics*, vol.104-106, pp.397-407.
 2. C.J. Williams, H. Wu, W.F. Waechter and H.A. Baker (1999), "Experience with Remedial Solutions to Control Pedestrian Wind Problems", 10th International Conference on Wind Engineering, Copenhagen, Denmark.



3. METEOROLOGICAL DATA

Meteorological data recorded at Perth International Airport for the period from 1998 to 2023 were used as a reference for wind conditions in the study area.

Image 3 depicts the directional distributions of wind frequencies and speeds recorded at the station. The records indicate that strong winds typically originate from the east. These are an inland wind and are generally common during the summer months. Other regional prevailing winds originate from the northeast and from the south to southwest sectors (the Fremantle Doctor).

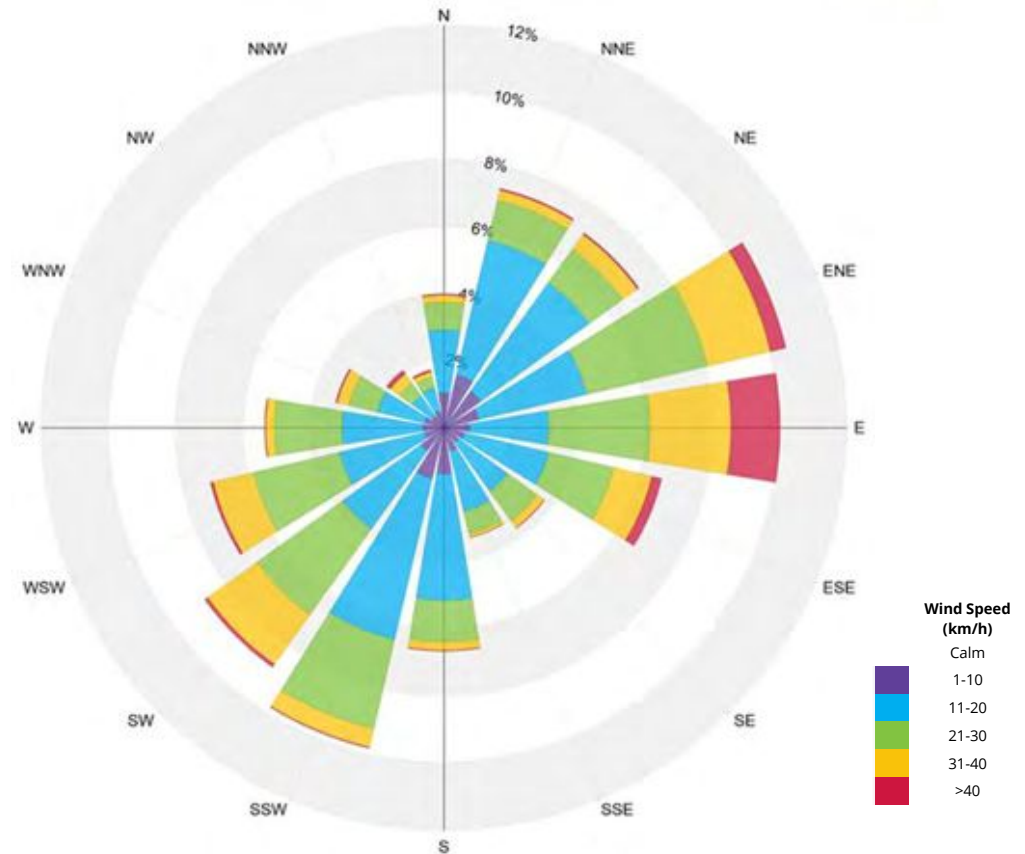


Image 3: Directional Distribution of Winds Approaching Perth International Airport (1998 - 2023)



4. PEDESTRIAN WIND CRITERIA

4.1 Pedestrian Wind Safety Criterion

Pedestrian wind safety is associated with excessive gusts that can adversely affect a person's balance and footing. The *Australasian Wind Engineering Society Guidelines for Pedestrian Wind Effects Criteria (2024)* recommends assessing wind safety using a threshold of 3-second gusts exceeding 83 km/h for more than 0.1% (or 9 hours) per year. Wind conditions surpassing this limit are considered severe and likely to impact pedestrian stability. This has been assessed qualitatively in the current report.

4.2 Pedestrian Wind Comfort Criteria

Pedestrian wind comfort is assessed using the criteria developed by Soligo et al.^{1,2} which are illustrated in Image 4. These criteria, established at RWDI through extensive research and consulting since 1974, are based on gust-equivalent mean wind speeds. Wind comfort levels are categorised according to the typical/intended pedestrian activity such that higher the activity level, higher the wind speed tolerable while engaged in the activity. Wind conditions are assessed at a typical pedestrian chest height and are considered suitable for the intended use of the space if the associated winds for the category are expected for at least 80% of the time. Wind control measures are

1. M.J. Soligo, P.A. Irwin, and C.J. Williams (1993). "Pedestrian Comfort Including Wind and Thermal Effects," Third Asia-Pacific Symposium on Wind Engineering, Hong Kong.
2. M.J. Soligo, P.A. Irwin, C.J. Williams, and G.D. Schuyler (1998). "A Comprehensive Assessment of Pedestrian Comfort Including Thermal Effects," Journal of Wind Engineering and Industrial Aerodynamics, vol. 77&78, pp. 753-766.

recommended for areas where the occurrence frequencies of wind speeds are expected to exceed the threshold values for specific pedestrian activities.

It should be noted that these criteria for wind forces represent average wind tolerance and can be subjective with regional differences in wind climate and thermal conditions as well as variations in age, health, clothing, etc. also impacting and individual's perception of the wind climate. For a full assessment of comfort, a thermal comfort study is required.

<p>Sitting ≤ 10 km/h</p>		<p>Calm or light breezes desired for outdoor seating areas where one can read a paper without having it blown away</p>
<p>Standing ≤ 14 km/h</p>		<p>Gentle breezes suitable for main building entrances, bus stops and locations where pedestrians may linger such as communal terraces</p>
<p>Strolling ≤ 17 km/h</p>		<p>Moderate winds that would be appropriate for strolling along a downtown street, plaza or park and where the objective is not to linger</p>
<p>Walking ≤ 20 km/h</p>		<p>High Winds that can be tolerated if one's objective is to walk, run or cycle without lingering - Also suitable for certain sporting activities</p>
<p>Uncomfortable > 20 km/h</p>		<p>None of comfort categories above are met - Represents conditions that might be dangerous to the elderly and children and are of a considerable discomfort to others</p>

Image 4: RWDI Pedestrian Wind Comfort Criteria

5. RESULTS AND DISCUSSION



5.1 General Wind Flow around Buildings

In the discussion of wind conditions on and around the Proposed Development, reference may be made to the following generalised wind flows (see Image 5). If these building / wind combinations occur for prevailing winds, there is a greater potential for increased wind activity and uncomfortable or potentially unsafe conditions. Design details such as setting back a tower from the edges of a podium for a prevailing wind direction, deep canopies close to ground level, wind screens / tall trees with dense landscaping, etc. can help reduce high wind activity. The choice and effectiveness of these measures would depend on the exposure and orientation of the site with respect to the

prevailing wind directions and the size and massing of the proposed buildings.

Conversely, in areas where higher wind velocities are desired, design measures can be implemented to enhance wind flow. For instance, channels aligned with prevailing wind directions can be integrated into the design to promote increased wind infiltration in regions prone to stagnant conditions. Such measures are particularly beneficial in areas with generally milder climates and high humidity levels, such as those closer to the equator.

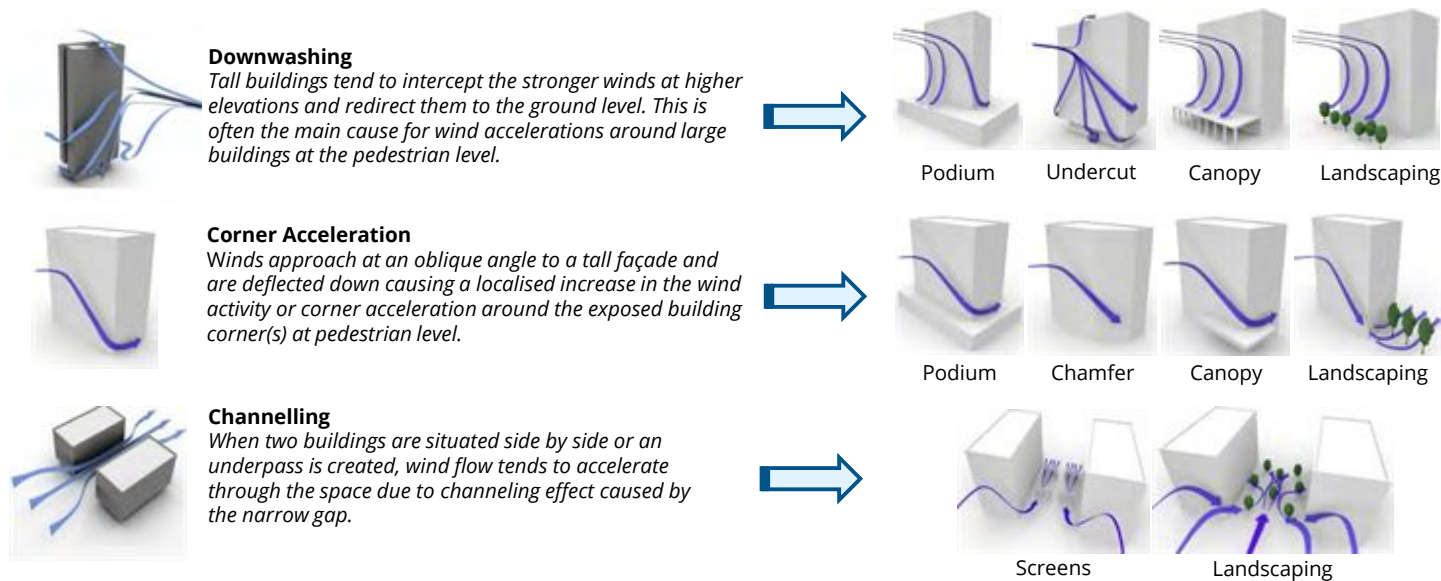


Image 5: General Wind Flow around Buildings with Examples of Common Wind Measures

5. RESULTS AND DISCUSSION



5.2 Site Exposure and Existing Site Conditions

The exposure of the site to regional prevailing winds is illustrated in Image 6. The upwind terrain for all key wind directions comprises of low-rise suburban terrain. This is expected to provide reduction in surface-level winds. However, any exposed structures are likely to impact local wind conditions. The open terrain from parkland and the oval to the south-east, south and south-west provide no obstruction to the winds which are likely to be stronger from these directions.

The existing site is currently empty and not likely to impact local wind microclimate. Wind conditions are likely to be governed by the wind exposure discussed above. Hence, wind conditions along Court Place are likely to be comfortable for sitting to standing use throughout the year. The alignment of Subiaco Road with the primary east winds and the rise in topography from east to west is likely to increase the impact of these winds. Hence, conditions along Subiaco Road are likely to be comfortable for standing to strolling use during the year. High winds exceeding the comfort or safety limits are not expected within and around the existing site areas.

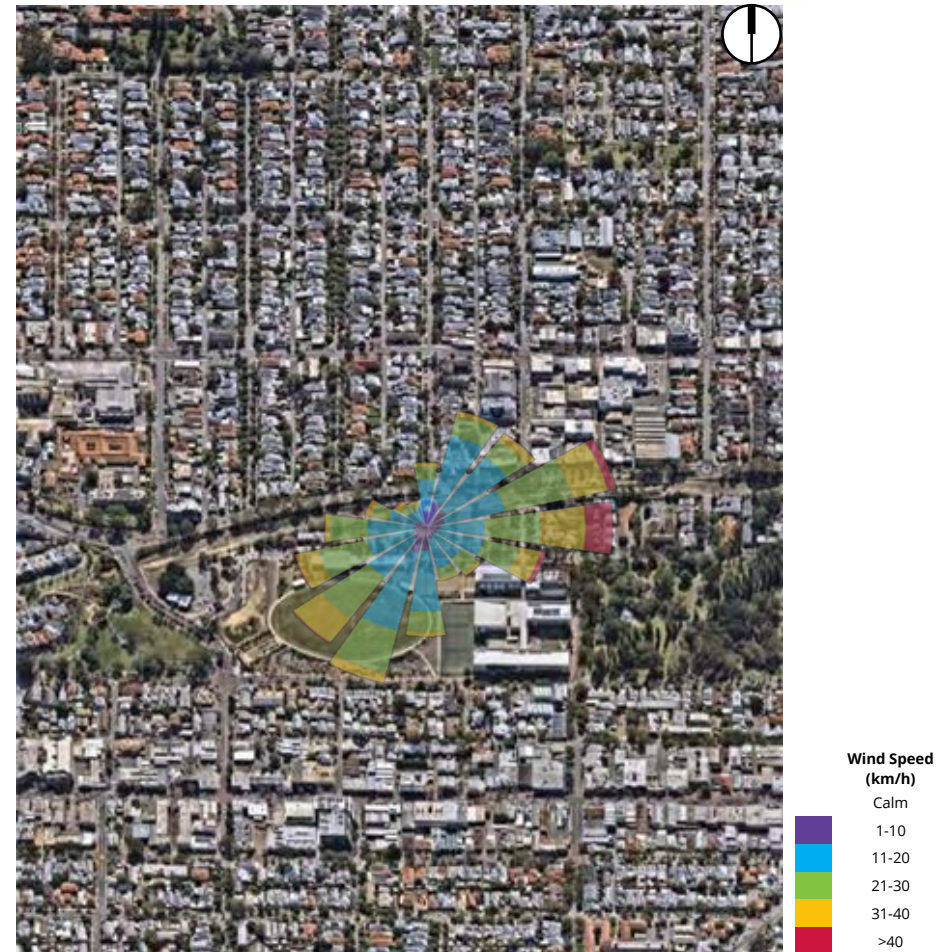


Image 6: Exposure of Site to Regional Prevailing Winds

Source: Nearmap

Pedestrian Wind Assessment

8

5. RESULTS AND DISCUSSION



5.3 Proposed Site Conditions

The Proposed Development comprises of four residential towers that are significantly taller than the surrounding context. Hence, there is a high potential for prevailing winds to be redirected into the street corridors. A summary of wind conditions within and around key areas is provided below:

- Surrounding Streets:** Winds from the east are likely to be captured by Tower 4 and redirected towards the ground (downwash). These winds can then accelerate around the eastern corners of the site. Current design shows planters and an awning along the southeast corner of the site which will likely reduce these wind effects. The east winds can also increase along Subiaco Road leading to conditions typically comfortable for strolling use. Increased winds are also expected along Court Place with conditions likely to range from standing to strolling use due to redirected north-easterly and south-westerly winds.
- Primary Lobby Entrances:** The primary lobby entrances to Towers 1 – 3 are located along Subiaco Road and are recessed into the building form with dense surrounding landscape. This is likely to reduce wind impacts around these spaces with conditions expected to be comfortable for standing use. The primary lobby entrance to Tower 4 is located approximately at the middle of the eastern façade along Court Place and includes a recess of approximately 1.5 m with an overhead awning. Winds here are expected to be suitable for standing use.
- Level 2 Communal Spaces:** Two communal terraces are located on Level 2 of the development above the podium between Towers 1 & 2 and between Towers 3 & 4. The terraces are generally sheltered from prevailing winds from the east due to the proposed towers and benefit from strategic landscape interventions to reduce buildup of winds. Overall wind impacts are likely to be small with conditions expected to be suitable for the intended use of the spaces. Wind conditions will be further assessed during the detailed design stage.
- Private Balconies:** Private balconies that are inset within the planform of the buildings benefit from a single aspect design and are expected to have comfortable conditions throughout the year. In contrast, balconies at the corners are likely to be exposed to windier conditions due to potential for wind acceleration through the balcony around the corner of the building. For Towers 1–3, corner balconies are partially sheltered from prevailing easterly winds. However, other regional wind directions may still influence overall conditions. Due to the exposed location of the development with little shelter from upwind terrain, strong winds are likely to persist on the eastern corner balconies of Tower 4. Similarly, upper-level corner balconies of Towers 1–3 may be impacted by higher wind speeds compared to mid- or lower-level balconies due to an increase in wind speeds with height above ground.

5. RESULTS AND DISCUSSION



5.4 Mitigation Measures

Based on the discussions provided, the following design advice and wind control measures should be considered to improve wind conditions around the site (examples shown in Images 7a-7c):

- **Ground Level Landscape/Awning:** It is recommended to include densely foliating trees with interlocking canopies along the eastern boundary of the site to mitigate the downwash of easterly winds. It should be ensured that the gap between the tree foliage and the façade is kept to a minimum. Alternatively, a continuous awning extending from Level 1 slab can be included along the eastern aspect to reduce downwash. Other landscape elements, such as trees and dense shrubs, should be retained along Subiaco Road to reduce the build-up of east winds up the slope.
- **Level 2 Communal Spaces:** The current design of the Level 2 communal spaces provides ample opportunity to integrate wind mitigation strategies which will be further explored during wind tunnel testing at detailed design stages. It is advised to consider densely foliating trees along the perimeter of the terraces. Additional mitigation strategies such as screening or landscape elements around outdoor seating areas can also provide a local shelter from channelling winds.
- **Private Balconies:** It is noted that the southeast corner balconies of Tower 4 include impermeable screening for half the width of the corner balconies. This design feature should be maintained to

provide comfortable conditions within these balconies. Similar screening is recommended for the northeastern corner balconies of Tower 4 and all corner balconies above Level 10 within the development site.

All corner balconies should also include impermeable balustrades to reduce wind impacts (Towers 1-4).

5. RESULTS AND DISCUSSION

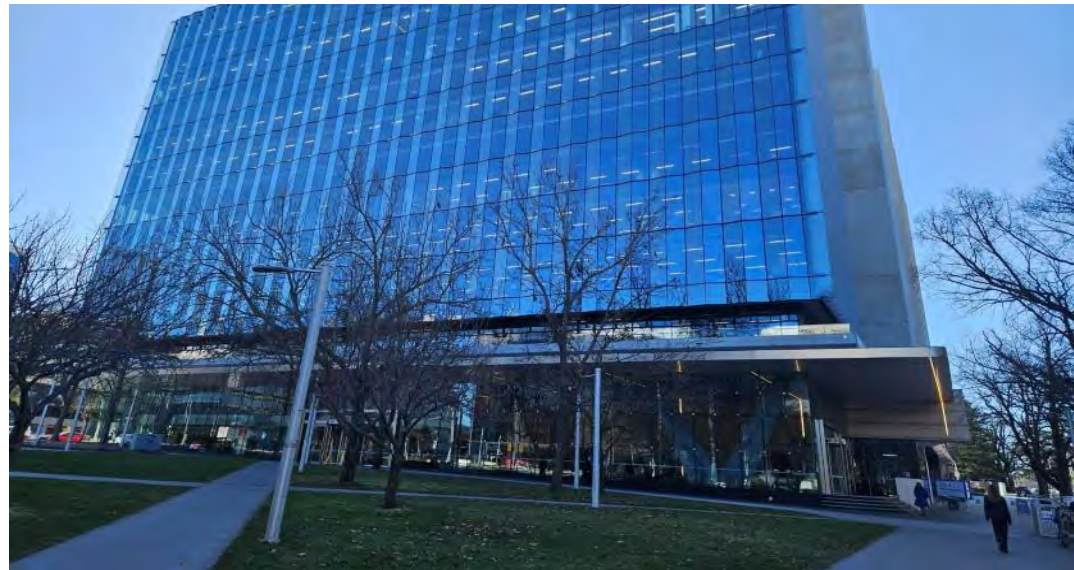


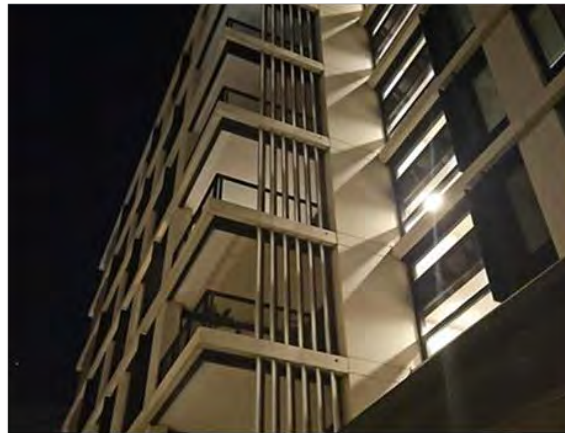
Image 7a: Examples of Wind Control Measures
Ground Level Awnings and Landscape for Downwash

5. RESULTS AND DISCUSSION



Image 7b: Examples of Wind Control Measures
Level 2 Podium Terrace

5. RESULTS AND DISCUSSION



**Image 7c: Examples of Wind Control Measures
Private Balconies**

6. SUMMARY



Wind conditions on and around the Proposed Development, known as Court Place, located at 154 Subiaco Road in Subiaco, WA are discussed in this report. The qualitative assessment is based on the review of local wind climate and the current design of the proposed development. The impact of the surrounding buildings and the local land topography has also been considered. The assessment is based on RWDI's experience with wind tunnel testing and CFD analysis of similar buildings within the region.

Conceptual wind flows around the Proposed Development are discussed in the report for the prevailing wind directions to identify key wind sensitive areas. The review indicates that wind conditions within and around the development site are likely to be impacted by the exposure of the development to regional winds. Hence, higher wind speeds can occur near the eastern corners of the site at the ground level and within the northeast corner balconies. Level 2 may be exposed to some channelling winds, but the current design provides ample opportunity to implement dense vegetation and other measures to improve overall comfort. Preliminary advice has been provided and can be considered further at a more detailed design stage.

This qualitative assessment provides a screening-level estimation of potential wind conditions across the development site and outlines conceptual wind control strategies appropriate for early-stage design. The integration of these measures is expected to support wind conditions that align with the intended use of various outdoor spaces within and around the proposed development. To validate and refine these preliminary findings, wind tunnel testing will be undertaken during the detailed design phase. This will enable quantification of wind effects and confirmation of the predicted comfort levels. The conceptual mitigation strategies presented herein can be further optimised based on the outcomes of these detailed studies.

6. SUMMARY



The assessment discussed in this report pertains to the Proposed Development in accordance with the drawings and information received in January 2026. In the event of any significant changes to the design, construction or operation of the building or addition of surroundings in the future, RWDI could provide an assessment of their impact on the wind conditions discussed in this report. It is the responsibility of others to contact RWDI to initiate this process.

Statement of Limitations

This report entitled ' *Court Place: Pedestrian Wind Assessment*', dated 20 January 2026, was prepared by RWDI Australia Pty Ltd ("RWDI"). The findings and conclusions presented in this report have been prepared for the Client and are specific to the project described herein ("Project"). The conclusions and recommendations contained in this report are based on the information available to RWDI when this report was prepared. Because the contents of this report may not reflect the final design of the Project or subsequent changes made after the date of this report, RWDI recommends that it be retained by Client during the final stages of the project to verify that the results and recommendations provided in this report have been correctly interpreted in the final design of the Project.

The conclusions and recommendations contained in this report have also been made for the specific purpose(s) set out herein. Should the Client or any other third party utilise the report and/or implement the conclusions and recommendations contained therein for any other purpose or project without the involvement of RWDI, the Client or such third party assumes any and all risk of any and all consequences arising from such use and RWDI accepts no responsibility for any liability, loss, or damage of any kind suffered by Client or any other third party arising therefrom.

Finally, it is imperative that the Client and/or any party relying on the conclusions and recommendations in this report carefully review the stated assumptions contained herein and to understand the different factors which may impact the conclusions and recommendations provide.

Access Review of Plans for Development Approval

**Community Housing Limited c/o Corbel Property
Court Place**

Court Place 154 Subiaco Road, Subiaco WA 6008

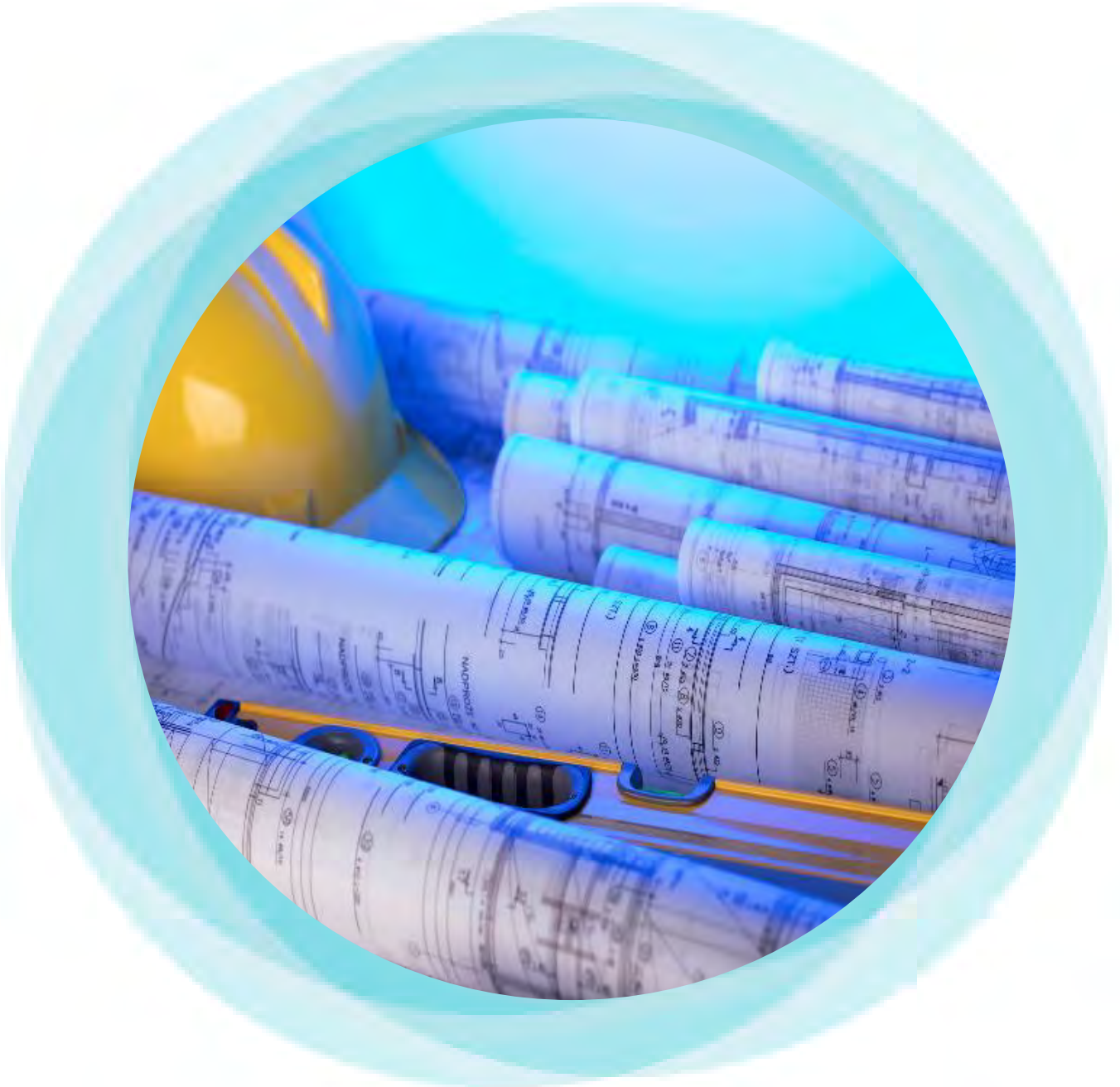




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EXECUTIVE SUMMARY

The proposed Court Place development comprises predominantly of Class 2 apartments, along with a Class 5 office and Class 6 commercial tenancies on the Ground Floor. The proposed Class 2 works consist of four towers – Tower 1 with 6 storeys, Tower 2 with 14 storeys, Tower 3 with 10 storeys, and Tower 4 with 17 storeys.

At this stage of design and documentation, the accessibility requirements relating to door clear open width, accessible path of travel, turning space, provision and dimensions of accessible car parking spaces and lifts, and access to common-use areas are confirmed to be achieved.

The following issues have been identified:

- Insufficient doorway circulation space, and
- Unclear path of travel from the accessible carparking bay at the eastern portion of the Upper Ground car park to the lift and commercial tenancies.

All dwellings within the Court Place development are designed to meet LHDG Silver Level requirements at the minimum and as such, the LHDG requirements have been used as the basis for assessment of all Class 2 dwellings.

All proposed dwellings are confirmed to achieve the LHDG Silver Level requirements.



1. INTRODUCTION

1.1 SOCIAL AND AFFORDABLE HOUSING DELIVERY

A new social and affordable housing [S&AH] development proposed at 154 Subiaco Road Subiaco aims to deliver a high-quality, community integrated residential outcome that meets both the social and affordable housing needs and long term operational requirements for Community Housing Limited (CHL) as a registered Community Housing Provider.

This housing initiative is a Class 2 development comprising a mix of social and affordable housing with dwellings designed to the Silver Level of the Livable Housing Design Guidelines [LHDG].

1.2 INTENT OF REVIEW

This Development Application (DA) review has included an assessment of the provided drawings as detailed below to determine compliance with the following, as they pertain to access for people with disability to this Class 2 development:

- National Construction Code (NCC) Building Code of Australia (BCA) Volume One 2022 Amendment 2
- Disability (Access to Premises – Buildings) Amendment (2024 Measures No1) Standards 2024
- Applicable Australian Standards on access and mobility
- Livable Housing Design Guidelines [Fourth Edition] [LHDG]

The assessment has been completed to the extent appropriate for this DA stage. The report does not address elements for which design detail is not yet developed.

The assessment undertaken when preparing this report has considered all areas that are mandatory under legislation, to meet the access requirements of people with disability, plus the additional requirement to achieve Silver Level of the Livable Housing Design Guideline [LHDG], including external access, car parking, entrances, internal accessways, doorways and circulation space, vertical access including stairs, lifts and ramps, statutory signage, hearing augmentation, application of tactile ground surface indicators, treatment of glazing, the provision and design of the Silver Level apartments, application of luminance contrast requirements, and any accessible sanitary facilities.



1.3 GLOSSARY

Accessible [NCC 2022 Glossary]: Having features to enable use by people with a disability.

Accessway [NCC 2022 Glossary]: A continuous accessible path of travel (as defined in AS 1428.1) to, into or within a building.

Livable Housing Design [LHDG] A livable home is designed and built to meet the changing needs of occupants across their lifetime. Livable homes include key easy living features that make them easier and safer to use for all occupants including; people with disability, ageing Australians, people with temporary injuries, and families with young children. A livable home is designed to:

- be easy to enter
- be easy to navigate in and around
- be capable of easy and cost-effective adaptation, and
- be responsive to the changing needs of home occupants

Silver Level [LHDG]

Silver Level of the LHDG includes the seven core livable housing design elements and focuses on the key structural and spatial elements that are critical to ensure future flexibility and adaptability of the home. The seven core design features elements in the silver level are:

- (1) A safe continuous and step free path of travel from the street entrance and / or parking area to a dwelling entrance that is level.
- (2) At least one, level (step-free) entrance into the dwelling.
- (3) Internal doors and corridors that facilitate comfortable and unimpeded movement between spaces.
- (4) A toilet on the ground (or entry) level that provides easy access.
- (5) A bathroom that contains a hobless shower recess.
- (6) Reinforced walls around the toilet, shower and bath to support the safe installation of grabrails at a later date.
- (7) Stairways are designed to reduce the likelihood of injury and also enable future adaptation.

1.4 REFERENCES

- Disability (Access to Premises – Buildings) Standard 2010
- National Construction Code Volume One, Building Code of Australia 2022 Class 2 to Class 9 Buildings [Amendment 2]
- AS1428.1 2021 – Design for Access and Mobility – General Requirements for Access – New Building Works



- AS2890.6 2009 – Parking Facilities – Off-street parking for people with disabilities
- AS1735.12 1999 – Lifts, escalators and moving walks – Facilities for persons with disabilities
- Livable Housing Design Guidelines (Fourth Edition)

1.5 DRAWINGS ASSESSED



Sheet Number	Sheet Name	Issue date	Drawing Revision
DA200	Overall – Lower Ground Plan	19/12/2025	02
DA201	Overall – Upper Ground Plan	19/12/2025	02
DA202	Overall – Level 01	19/12/2025	02
DA203	Overall – Level 02/Podium	19/12/2025	02
DA204	Overall – Level 03-06/Typical Plates	19/12/2025	02
DA205	Overall – Level 07/T1 Roof Services	19/12/2025	02
DA206	Overall – Level 07/T1 Overrun	19/12/2025	01
DA207	Overall – Level 11/T3 Roof Services	19/12/2025	02
DA208	Overall – Level 11/T3 Overrun	19/12/2025	01
DA209	Overall – Level 15/T2 Roof Services	19/12/2025	02
DA210	T1&T2 – Upper Ground Plan	19/12/2025	01
DA211	T1 & T2 – Level 01	19/12/2025	01
DA212	T1 & T2 – Top of Podium	19/12/2025	01
DA213	T1 & T2 – Typical Tower	19/12/2025	01
DA214	T1 – Roof Services Plan	19/12/2025	01
DA220	T3&T4 – Lower Ground Plan	19/12/2025	01
DA221	T3&T4 – Upper Ground Plan	19/12/2025	01
DA222	T3&T4 – Level 01	19/12/2025	01
DA223	T3&T4 – Top of Podium	19/12/2025	01
DA224	T3&T4 – Typical Tower	19/12/2025	01
DA225	T3 – Roof Services Plan	19/12/2025	01
DA300	South Elevation	19/12/2025	03
DA301	North Elevation	19/12/2025	03
DA310	T1 & T2 – South Elevation	19/12/2025	01
DA311	T1 & T2 North Elevation	19/12/2025	01
DA312	T1 – West & East Elevation	19/12/2025	01
DA313	T2 – West & East Elevation	19/12/2025	01
DA320	T3&T4 – South Elevation	19/12/2025	01
DA321	T3&T4 – North Elevation	19/12/2025	01
DA322	T3 – West & East Elevation	19/12/2025	01
DA323	T4 – West & East Elevation	19/12/2025	01
DA2010	Overall – Level 16/T2 Overrun	19/12/2025	01



1.6 ASSESSMENT AND DIMENSIONING

Unless dimensioned on the plans, O’Brien Harrop Access has reviewed dimensions and distances using the PDF reviewing tool, Bluebeam Revu.

1.7 REPORT TRACKING AND REVISIONS

Report title	Access Review of Plans for Development Approval	
Date report issued	19 January 2026	
Report issued to	Community Housing Limited c/o Corbel Property	
Report prepared by:	Silvia Rossi Occupational Therapist BSc (OT) Associate member ACA No. 510	
Report reviewed by:	Anita Harrop Occupational Therapist BAppSc (OT) (Hons) Diploma of Access Consulting Access Consultant, Accredited member ACA No. 147 Livable Housing Design Guideline Assessor #LHDG099 Changing Places Assessor #CP011 Accredited SDA Assessor #SDA00070	

Development Approval (DA) Design Review for Court Place



OHA



1. EXEMPT AREAS

1.1 INTRODUCTION

NCC D4D5 states that the following areas are not required to be accessible:

- (a) an area where access would be inappropriate because of the particular purpose for which the area is used.*
- (b) an area that would pose a health or safety risk for people with a disability.*
- (c) any path of travel providing access only to an area exempted by (a) or (b).*

The Guideline on the Application of the Premises Standards [Version 2 February 2013] [Clause D3.4 Exemptions] provides additional clarity by the following examples:

“These areas could include cleaners store rooms, commercial kitchens, staff serving areas behind bars, cool rooms, rigging lofts, waste-containment areas, foundry floors, abattoir animal processing areas, railway shunting yards, electrical switch rooms, chemical and hazardous materials store areas, loading docks, fire lookouts, plant and equipment rooms and other similar areas.”

1.2 EXEMPT AREAS

Areas considered exempt for provision of access for people with disability and not considered as part of this drawing review were those spaces generally considered where access would be inappropriate because of the purpose for which the area is used, or an area that would pose a health or safety risk for people with a disability, or the area is solely used by staff undertaking such duties, as is pertinent to this development.

Areas considered **exempt** for the purpose of this report are therefore considered to be:

- R.ST Storage spaces
- Mech rooms
- Comms, Mech, Solar, Electrical, SV and other service areas
- Fire Control Room, Fire Tank and Pump rooms
- Hyd Pump room

1.3 EXEMPT AREAS TO BE CONFIRMED

To ensure that any declaration of exemption is not based on inappropriate assumptions about the ability of people with a disability or people with a particular type of disability to undertake work or activities in the above-mentioned spaces, further clarification/consultation as to the purpose of the areas listed below, may be necessary. A clear determination must be sought as to whether the following areas are to meet access requirements for people with disability.



- R.Bin store, R.HD Waste & R.Fogo as residents of the Silver Level dwellings on the Ground Floor may require access to these areas

2. ACCESSIBLE CARPARKING

2.1 CLASS 7A CAR PARK

For a Class 7a [car park] building as per the NCC D4D2(1)(7):

(1) Buildings and parts of buildings must be accessible as required by this clause, unless exempted by D4D5.

(7) For a Class 7a building, access must be provided to and within any level containing accessible carparking spaces.

Implications

Within this proposed development, access for people with disability must therefore be provided to and within the Upper Ground Floor car park, as it contains accessible carparking spaces.

2.2 PROVISION

In accordance with NCC D4D6:

(1) Accessible carparking spaces—

(a) subject to (b), must be provided in accordance with (2) in—

(i) a Class 7a building required to be accessible; and

(ii) a carparking area on the same allotment as a building required to be accessible; and

(c) subject to (d), must comply with AS/NZS 2890.6; and

(d) need not be identified with signage where there is a total of not more than 5 carparking spaces, so as to restrict the use of the carparking space only for people with a disability.

(2) For each class of building to which the carpark or carparking area is associated, the number of accessible carparking spaces required is as follows:

(b) Class 5, 7, 8 or 9c buildings — 1 accessible space for every 100 carparking spaces or part thereof.

(c) Class 6 buildings—

(i) with up to 1000 carparking spaces — 1 accessible space for every 50 carparking spaces or part thereof; and

(ii) for each additional 100 carparking spaces or part thereof in excess of 1000 carparking spaces — 1 accessible space.



Interpretation

For this Class 2, 5, and 6 building, accessible car parking is not required for the Class 2 residential parking area but for the carparking associated with the Class 5/6 component of the allocated parking. With a total of 30 visitor carparking bays, one accessible parking space is required.

Confirmation

It is confirmed that two accessible visitor carparking bays are proposed, which satisfies the requirement of NCC D4D6(1)(a)(2).

2.3 DESIGN

NCC D4D6(1)(c) states that accessible carparking spaces must be designed to AS2890.6 2009.

Confirmation

On assessment of drawing [DA201], the accessible carparking bays and the adjacent shared area meet the general dimensional requirements as per AS2890.6 2009 Clause 2.2.1 and Figure 2.1.

Recommendations

Ensure that the accessible carparking bay and the adjacent shared area are at the same level, in accordance with AS2890.6 2009 Clause 2.2.1(d).

Provide a minimum of 2500mm headroom above the accessible carparking bays and adjacent shared area as per AS2890.6 2009 Clause 2.4 and Figure 2.7.

3. GENERAL BUILDING ACCESS REQUIREMENTS FOR PEOPLE WITH DISABILITY

3.1 CLASS 2

The NCC 2022 D4D2(1)(4) states for a Class 2 [apartment building] that common areas are to be accessible as described:

(1) Buildings and parts of buildings must be accessible as required by this clause, unless exempted by D4D5.

(4) For a Class 2 building, common areas are to be accessible as follows:

(a) From a pedestrian entrance required to be accessible to at least 1 floor containing sole-occupancy units and to the entrance doorway of each sole-occupancy unit located on that level.



- (b) To and within not less than 1 of each type of room or space for use in common by the residents, including a cooking facility, sauna, gymnasium, swimming pool, common laundry, games room, individual shop, eating area, or the like.*
- (c) Where a ramp complying with AS 1428.1 or a passenger lift is installed—
 - (i) to the entrance doorway of each sole-occupancy unit; and*
 - (ii) to and within rooms or spaces for use in common by the residents.**
- (d) The requirements of (c) only apply where the space referred to in (c)(i) or (ii) is located on the levels served by the lift or ramp*

Confirmation

On assessment of the DA drawings [DA200 & DA201], it is confirmed that access has been provided from the main pedestrian entrances off Subiaco Road on the Upper Ground Floor to the T1, T2 and T3 Lobbies, and from Court Place on the Lower Ground Floor to the T4 Lobby.

Two lifts in Towers 1, 2 and 3, and three lifts in Tower 4 provide vertical access to all floors containing sole-occupancy units. Access is achieved to most of the apartments on each level from the lift lobbies.

On assessment of the Class 2 common areas in accordance with NCC 2022 D4D2(1)(4), it is confirmed that access has been provided to the bin chutes, courtyards and amenity areas on Level 02, the two R. Bicycle stores on the Upper Ground Level and the R. Bin store, R. HD Waste & R. Fogo on the Ground Floor.

Refer to Section 6 of this report for discussion on doorways.

3.1.1 Livable Housing Design Guideline requirements – Silver Level

All dwellings are designed to the Silver Level of the Livable Housing Design Guidelines [LHDG]. All seven core living requirements pertain to the internal of apartments only and not the external path of travel to the apartments.

Thus, in considering all aspects of the Class 2 building, this report addresses both the Class 2 building requirements, and the Silver Level Livable Housing requirements pertaining to the private spaces within the apartments.

A summary of the outcome of the Silver Level Level Livable Housing assessment is provided in Section 9 of this report.

3.2 CLASS 5 OFFICE & CLASS 6 RETAIL

The NCC D4D2(1)(6) states that access requirements for a Class 5 office and Class 6 commercial building/tenancy are as follows:

- (1) Buildings and parts of buildings must be accessible as required by this clause, unless exempted by D4D5.*



(6) For Class 5 and 6 buildings, access must be provided to and within all areas normally used by the occupants.

Comment/Recommendations

Ensure flush transitions from the footpath into the commercial tenancies and the CHL Office as per AS1428.1 2021 Clause 3.1 and Clause 4.2.

As the plans develop, given the varying topography and gradients of the footpaths, align flat landings at the entrance doors to the commercial tenancies and CHL Office as per AS1428.1 2021 Clause 7.1(b) & (c), 10.3.1 & Figure 31 (swing) or 32 (sliding), to enable a wheelchair user to make a 90° turn to enter.

4. ACCESS TO BUILDINGS

4.1 PROVISION OF AN ACCESSWAY TO A BUILDING

NCC D4D3(1) requires that:

- (1) An accessway must be provided to a building required to be accessible-*
- (a) from the main points of a pedestrian entry at the allotment boundary; and*
 - (b) from another accessible building connected by a pedestrian link; and*
 - (c) from any required accessible carparking space on the allotment.*

Confirmation

In this proposed development an accessway is confirmed:

- From the allotment boundary on Subiaco Road and Court Place to the principal pedestrian entrances via footpaths, and
- Between the towers, commercial tenancies and CHL Office via the footpaths surrounding the building and via the carpark.

On assessment of access to and within the car park on the Upper Ground Floor, it is confirmed that one out of two accessible car parking bays is located in proximity to the Tower 1 Lobby.

Findings/Consideration

The second accessible carparking bay on the Upper Ground Floor is near Tower 4. However, at this stage of design, the path of travel from the accessible carparking bay to the lift or commercial tenancies is yet to be detailed.

Ensure the accessible carparking bay near Tower 4 is connected to the lift and/or commercial tenancies by an accessible and safe path of travel.



4.2 ACCESS VIA THE PRINCIPAL PEDESTRIAN ENTRANCE

In regards the principal pedestrian entrance(s) to the building, NCC D4D3(2) necessitates that:

- (2) *In a building required to be accessible, an accessway must be provided through the principal pedestrian entrance, and-*
 - (a) *through not less than 50% of all pedestrian entrances including the principal pedestrian entrance; and*
 - (b) *in a building with a total floor area more than 500m², a pedestrian entrance which is not accessible must not be located more than 50m from an accessible pedestrian entrance, except for pedestrian entrances serving only areas exempted by clause D4D5.*

Confirmation

On assessment of drawings [DA200 & DA201], an accessway is provided through pedestrian entrances to the lift lobbies of all towers via the Lower and Upper Ground floors.

It is assumed that these pedestrian entrance doorways into the lift lobbies are automatic doors with flush thresholds. If they are manual, ensure that the requirements doors (including clear open door width, door hardware, door force, and door circulation space) are achieved to AS1428.1 2021 Clause 10.2 and Figure 31.

5. PARTS OF A BUILDING REQUIRED TO BE ACCESSIBLE – VERTICAL ACCESS

5.1 RAMPS

As per NCC D4D4(a)(i) in a building required to be accessible every ramp, except for ramps in areas exempted by D4D5 as listed above, and fire-isolated ramps, must comply with Clause 7 of AS1428.1 2021.

Confirmation

No pedestrian ramps have been documented within this development. While walkways within the development comprise varying topography, their gradients do not exceed the maximum allowable 1:20 gradient for a walkway.

Recommendation

Provide walkways with landings, as specified in Clause 7.8, at intervals according to AS1428.1 2021 Clause 7.2(b).



5.2 STAIRWAYS

As per NCC D4D4(a)(ii) in a building required to be accessible every stairway, except for stairways in areas exempted by D4D5 as listed above, must comply with Clause 8 of AS1428.1 2021.

Interpretation/Implications

At this stage of design, the indicative common stairways (e.g., the stairway adjacent the Mail room in Tower 3 [Figure 1] appears sufficiently wide to accommodate compliant handrails on both sides. The stair set back may need further resolution to ensure the required handrail end extensions and warning TGSIs on the bottom landing of the stair can be contained within the confines of the partition wall and the door circulation space requirements (where clouded in blue).

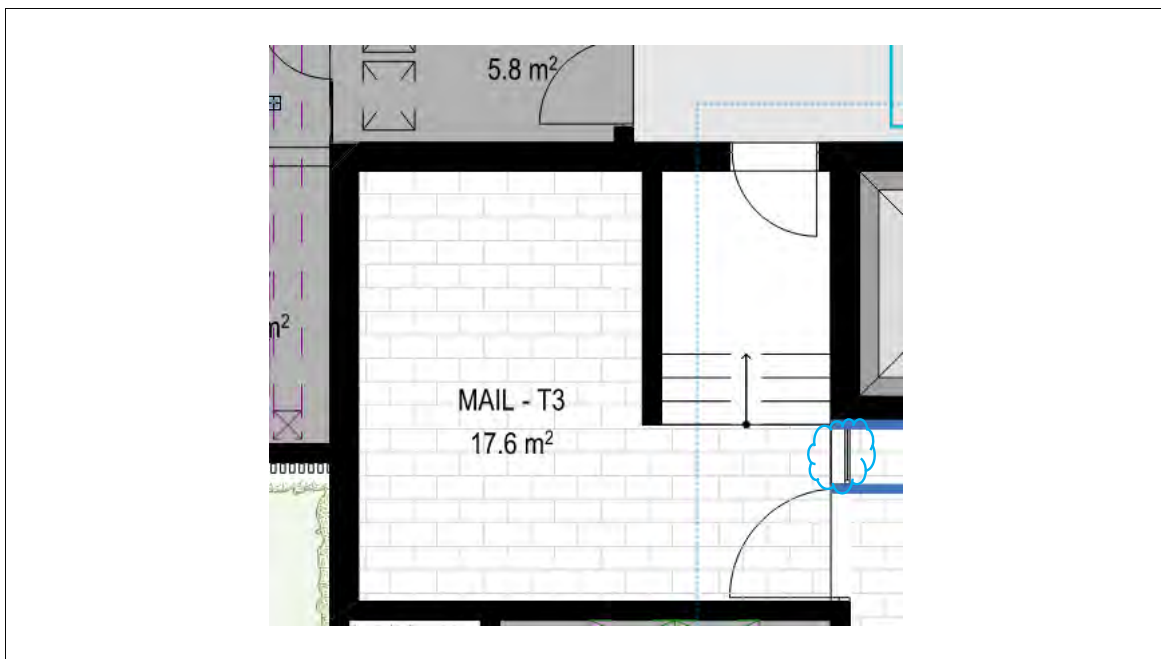


Figure 1: Extract of drawing [DA201], showing the T3 Mail Room stairway

Recommendations

At the appropriate design stage of this project ensure stairs are designed to the full requirements of AS1428.1 2021 Clause 8.1, 8.2 and 9 and warning tactile ground surface indicators (TGSIs) in accordance with AS1428.4.1 2009 [Amt 2] Clauses 2.1, 2.2, 2.3 & 2.4.



5.3 FIRE-ISOLATED STAIRWAYS

As per NCC D4D4(a)(iii) in a building required to be accessible, fire-isolated stairways must comply with Clause 8 (f) and (g) of AS1428.1 2021.

Confirmation

A fire isolated stairway is located within each of the four towers, which provides vertical access to all floors of each tower. At this early stage of design, there is insufficient detail for assessment of the stairways.

Recommendations

At the appropriate design stage of this project ensure a warning strip 50-75mm deep in 30% luminance contrast is documented to the tread of steps in fire-isolated stairways [AS1428.1 2021 Clause 8.1(f) and (g)].

5.4 LIFTS

As required by NCC D4D4(b), all passenger lifts must comply with E3D7 and E3D8. When specified, the lift must be of a type identified in E3D7(1), subject to the limitations on use specified in this same table.

Further, the lift must have accessible features in accordance with Clause E3D8 and not rely on a constant pressure device for its operation if the lift car is fully enclosed as per Clause E3D7(2).

Confirmation

On review of drawings [DA203 to DA209], a bank of two lifts is proposed within Towers 1, 2 and 3, and a bank of three lifts is proposed within Tower 4. All lifts indicatively achieve the general dimensional requirements for the lift car size and the minimum clear width of car door openings. The lift landings on each level achieves the minimum 1500mm wide by 1500mm long circulation space required for a wheelchair user to make a 90° turn.

Recommendation

When the manufacturer/supplier is confirmed, ensure the lift car dimensions, fittings and fixtures as specified by AS1735.12 1999 are achieved.

6. PARTS OF A BUILDING REQUIRED TO BE ACCESSIBLE: HORIZONTAL ACCESS

6.1 DOORWAYS

The NCC Glossary defines an **accessway** as a **continuous accessible path of travel** (as defined in AS1428.1) to, into or within a building, and AS1428.1 2021 Definition 1.4.6 states that this continuous accessible path of travel is “*an uninterrupted path of travel to, into or within a building providing access to all accessible facilities.*”



It is the interpretation of O'Brien Harrop Access that the provisions for:

- Luminance contrast requirements at doorways;
- Clear opening width of the active leaf of doorways;
- Circulation spaces at doorways;
- Distances between successive doorways;
- The force required to operate a door, and
- Door controls, including manual controls to automate a door;

are to be achieved on all accessible paths of travel through the proposed apartment, commercial and office building.

Australian Standard AS1428.1 2021 Clauses 3 [continuous accessible path of travel] and 10 [doorways] pertain to doorways.

Confirmation

At this early stage of design and documentation, the doorways along the accessible paths of travel are assessed to achieve:

- Compliant clear open widths of more than the required 850mm.
- Insufficient door circulation space has been identified at the following doors as per AS1428.1 2021 Figure 31:
 - The corridor entrance doorways connecting a block of apartments to the T1 and T2 Lobbies on the Upper Ground Floor [Figures 2 & 3]
 - Door between T2 Lobby Level 01 [Figure 4].
 - The R. Bicycle store doorway within Tower 4 [Figure 5]
 - The T1 Lobby doorway connecting to the courtyard on Level 02 [Figure 6]



Figure 2: Extract of Drawing [DA201], showing insufficient latch side clearance at corridor entrance doorway in Tower 1

Figure 3: Extract of Drawing [DA201], showing insufficient latch side clearance at corridor entrance doorway in Tower 2

Figure 4: Extract of Drawing [DA202], showing insufficient latch side clearance at the T2 Lobby doorway on Level 01

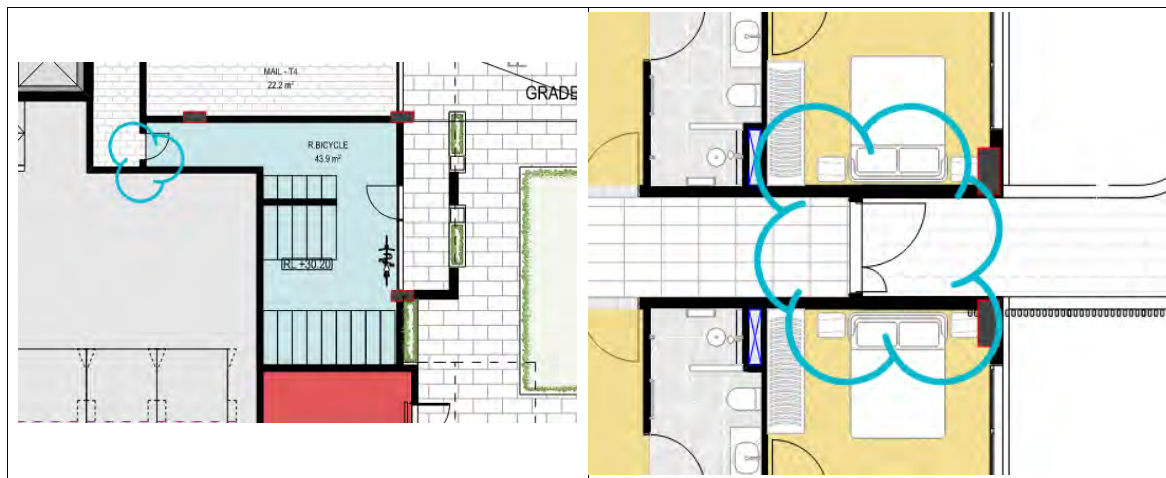


Figure 5: Extract of drawing [DA200], showing insufficient doorway circulation space at the internal doorway of R.Bicycle within Tower 4 on the Lower Ground Floor.

Figure 6: Extract of drawing [DA203], showing insufficient doorway circulation space at the T1 Lobby doorway connecting to the courtyard on Level 02

Recommendations

As the design progresses, ensure that all doorways along the accessible paths of travel achieve the circulation spaces in accordance with AS1428.1 2021 Clause 10.3, Figures 31 or 32 and are compliant with the requirements of:

- Luminance contrast requirements at doorways in accordance with AS1428.1 2021 Clause 10.1
- Distances between successive doorways in accordance with AS1428.1 2021 Clause 10.3.5 (if applicable)
- The force required to operate a door in accordance with AS1428.1 2021 Clause 10.4.2(e) noting fire and smoke doors are exempted from this requirement



- Door controls, including manual controls to automate a door in accordance with AS1428.1 2021 Clauses 10.4.3 & 10.4.4

6.2 DOORWAYS WITH MULTIPLE LEAVES

NCC D4D3(5) states that where a doorway on an accessway has multiple leaves, (except an automatic opening door) one of those leaves must have a clear opening width of not less than 850mm in accordance with AS1428.1. AS1428.1 2021 Clause 10.2 requires that where double doors are used, the 850mm minimum clear open width must apply to the active leaf of the doorway.

Confirmation

The active leaf of the Lobby double doorway on Level 02 achieves the minimum 850mm clear open width required.

6.3 ACCESSWAYS - PASSING AND TURNING SPACES

Part D4D4(c)(i), (ii)(A) and (ii)(B) of the NCC requires that accessways must have:

- **passing spaces** complying with AS1428.1 2021 Clause 3.4 and Figure 3 at maximum 20 metre intervals on those parts of an accessway where a direct line of sight is not available; and
- **turning spaces** complying with AS1428.1 2021 Clause 3.5 and Figures 4 and 5 within 2 metres of the end of accessways where it is not possible to continue travelling along the accessway and at maximum 20 metre intervals along the accessway.

Confirmation

It is confirmed that non-exempted corridors indicatively achieve the minimum width requirements for turning and passing spaces

6.4 GLAZING ON AN ACCESSWAY

As per the requirement of the NCC D4D13 and AS1428.1 2021 Clause 3.6, any door (and / or side light) without a transom that is fully glazed or frameless that could be mistaken for an open door or opening shall be clearly marked for the full width of the glazing panel with an unbroken, solid, and opaque contrasting line.

Findings

On review of the elevation drawings [DA300 to DA301; DA310 to DA 313; DA320 to DA323], glazing is documented to the apartment, commercial and office building, however at this early stage of design visual indicators are not detailed.



Recommendation

As the design is progressed, ensure treatment for glazing as per the requirement of the NCC D4D13 and AS1428.1 2021 Clause 3.6 is achieved.

7. SURFACES

7.1 INTRODUCTION

AS1428.1 2021 Section 4 addresses floor or ground surfaces on continuous accessible paths of travel and circulation spaces, and the general requirement is that a continuous accessible path of travel and any circulation spaces shall have a slip-resistant surface. The texture of the surface shall be traversable by people who use a wheelchair and those with an ambulant or sensory disability.

NOTE 1 Information relevant to slip resistance can be found in AS 4586, AS 4663, HB 197, and SA HB 198.

Findings

At this early stage of design, details for ground (landscape) and floor coverings have yet to be developed for assessment.

Recommendation

As the design progresses OHA will continue to work with the design team to ensure floor and ground surfaces meet accessibility requirements.

8. SANITARY FACILITIES

8.1 UNISEX ACCESSIBLE AND AMBULANT SANITARY FACILITIES

One unisex accessible toilet is documented adjacent to the commercial tenancy that is located northeast of the building. At this early stage of design there is insufficient detail for assessment of sanitary facilities against accessibility requirements.

Recommendation

OHA will be pleased to review the sanitary facility design when it has been developed to ensure accessibility requirements are met, further, OHA would be pleased to review the statutory signage package.



9. LIVABLE HOUSING DESIGN ASSESSMENT

9.1 INTRODUCTION

As per the Livable Housing Design Guideline [LHDG] [Fourth Edition], a livable home is designed and built to meet the changing needs of occupants across their lifetime. Livable homes include key easy living features that make them easier and safer to use for all occupants including: people with disability, ageing Australians, people with temporary injuries, and families with young children.

As required for this Court Place Project, all dwellings are required to meet the LHDG Silver Level requirements [also called the seven core livable housing design elements].

The seven core design features elements in the silver level are:

- (1) A safe continuous and step free path of travel from the street entrance and / or parking area to a dwelling entrance that is level.
- (2) At least one, level (step-free) entrance into the dwelling.
- (3) Internal doors and corridors that facilitate comfortable and unimpeded movement between spaces.
- (4) A toilet on the ground (or entry) level that provides easy access.
- (5) A bathroom that contains a hobless shower recess.
- (6) Reinforced walls around the toilet, shower and bath to support the safe installation of grabrails at a later date.
- (7) Stairways are designed to reduce the likelihood of injury and also enable future adaptation.

9.2 FINDINGS

9.2.1 LHDG Silver Level

Confirmation

At this stage of design, the apartment design types 1B 1b, 2B 1b, and 3B 2b across all floors within all towers have been assessed to and are confirmed to achieve the LHDG Silver Level requirements.

Recommendation

As design progresses, ensure:

- Doorways achieve a level transition and threshold (maximum vertical tolerance of 5mm between abutting surfaces is allowable provided the lip is rounded or beveled),
- Walls around the shower and toilet reinforced to provide the safe installation of grabrails as specified in the LHDG Section 6.



Department of Planning,
Lands and Heritage



20 April 2025

Court Place Subiaco

GAWA DESIGN ADVICE

PROPOSED DEVELOPMENT NAME Court Place

ADDRESS OF PROPOSED DEVELOPMENT 154 Subiaco Road Subiaco

PROJECT NUMBER 2528

OBJECTIVE ID qA694924

OFFICIAL

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BACKGROUND

Development WA have requested comments from the Government Architect as Chair of the Design Review Panel for the social housing proposal at Court Place Subiaco.

This advice has been prepared to specifically address Design Excellence as it related to the proposal and the refinements that have been made since Design Review 3.

DESIGN EXCELLENCE

Design Excellence as it applies to this development is defined within the Subiaco Oval and Railway Precinct Design Guidelines. Design Excellence requires achieving an exceptional outcome by addressing all of the SPP7.0 principles of good design and then going above and beyond. Guidance is provided within Appendix 2 of the Design Guidelines.

Design Excellence is required to be achieved for landmark sites identified under the Design Guidelines, which includes Lot 11.

- Demonstrate the way in which the proposal goes above and beyond when addressing **amenity, sustainability, durability and maintenance, and safety and security across the entire development.**
- Demonstrate the way in which the proposal goes above and beyond in **the way in which the Lot 11 portion of the development responds to its prominent, public location.**

COMMENTS

Amenity Issues identified in DR3 report

- Improved amenity of lower-level dwellings, including further consideration of privacy, interaction with the street, stoop depth, and planting interfaces.
- Greater amenity within the ground floor public realm through prioritisation of shared, welcoming spaces with improved comfort, shade, seating, and integration of public art.

Chair comments in relation to revised proposal

The proposal has developed to largely address concerns raised by the Panel and the achievement of Design Excellence. Minor modifications are required to meet this threshold.

The revised design addresses the comments made in relation to the amenity of dwellings on ground with consideration to levels, stairs and planting locations however some concern remains in relation to the potential negative impacts to amenity of the apartments adjacent to vehicle entry points and bicycle stores. Consideration should be given to ways in which these interfaces can be screened to balance connection and privacy, ensuring resident amenity is maintained.

The central forecourt area has been further developed to demonstrate the integration of seating and shade within the landscape, offering an invitation for use to residents and the public. Ensuring the proposed shade structures are maintained through the development and delivery of the design is critical to the demonstration of excellent amenity.

The amenity that has been provided through the refinement of the commercial tenancy at the corner of Court and Subiaco Road has addressed the Panel comments. The integration of street facing seating with the planters is an important component of the design.

The integration of the commercial tenancy with the pocket park and Court Place is supported.

The location and scale of public art has not been resolved in the information provided. Although important to the proposal overall this is not considered necessary for the determination of design excellence.

Sustainability Issues identified in DR3 report

- Greater information regarding the independent verification process.
- Integration of ESD measures with façade design and material selection.

Chair comments in relation to revised proposal

It is not currently possible to determine if the threshold of Design Excellence has been met based on the information supplied. The approach to integrated ESD measures in the façade has not been reflected in the DA set. The Design Review pack contains variations between the renders of the proposal (with not shading to north facing glazing) and the small vignette of a north facing window detail on page 44.

Confirmation is sought in relation to the integration of awnings on the northern elevation of the development.

Confirmation is sought from Development WA that the independent verification process proposed, and the degree to which this has been explained by the applicant, meets Development WA standards.

Durability and maintenance issues identified in DR3 report

- Further details of the intended material strategy, particularly for the tower forms, including contingency plans should the preferred material/construction methodology not be considered viable. Greater confidence is required that the intended material quality and durability will be delivered, particularly if construction methodology changes (e.g. precast versus lightweight systems).

Chair comments in relation to revised proposal

The proposed material palette is supported and considered to meet the threshold of Design Excellence. The Panel expressed concern in relation to the impact of an alternate construction methodology should this be required as part of the VE process. The alternate lightweight system proposed in the supplied documents, that changes from concrete to aluminium cladding, is not supported. Exploration of lightweight options that maintain both the colour and textural quality of the precast panels is considered important to maintaining a contextually appropriate response. There are proprietary compressed fibre cement cladding systems that could be explored to address this.

The introduction of the lightweight arbour to the commercial tenancies along Court Place is supported. The materiality and finish of these is not described in the information provided.

Safety and Security issues identified in DR3 report

- A consolidated, independent CPTED report is required to clearly demonstrate how safety principles are embedded in the design. An internally prepared assessment is not considered to be sufficient for a development of this scale and use.

Chair comments in relation to revised proposal

Confirmation that an external CPTED consultant will be engaged to undertake a review of the proposal and provide a report is supported. There are two specific areas to note in the current proposal, that could be easily resolved to more fully address safety and security.

The bike stores to Tower 1 and Tower 2 have limited passive surveillance, these should be maximised to capitalise on visual connection to the street and lobby.

OFFICIAL

The level change between the upper and lower terraces on the corner tenancy at Court Place may warrant balustrading and handrails. Should this be the case these elements will need to be sensitively integrated into the design.

With Development WA approval it is considered that, by addressing the above, in combination with the improvements that have been made to passive surveillance and lighting to public landscape areas, the proposal would meet the threshold of design excellence.

Lot 11 landmark site

The changes to the articulation and massing to the corner of Court Place and Subiaco Road are considered to sufficiently meet the threshold required for a landmark site.

Conclusion

It is possible for the proposal to meet the threshold of Design Excellence subject to the issues raised being met or conditioned.

SIGNATURE



NAME

Emma Williamson

POSITION


Government Architect

DATE

20 April 2026



DESIGN REVIEW PANEL ADVICE NOTE (DRP 3)

Review Attendance	
Subject	Review of proposed 7-18 Storey Mixed Use Development at Lot 1 (No. 154) Subiaco Road, Subiaco (Third Design Review)
Date	17 February 2026
Time	2.00pm – 4.15pm
Location	DevelopmentWA – Mia Yellagonga Tower 2, Level 7/5 Spring St, Perth
Panel Members	Emma Williamson (Chair) WA Government Architect - DPLH
	Robin Burnage Plan E
	Bret White Cox Architecture
	Marion Fredriksson Marion Fredriksson Design
Proponent Team	Corey Owen Community Housing Ltd
	Dennis Chew DKO
	Lionel Mawhinney DKO
	Ross Catalano Corbel Property
	David Klein Corbel Property
	Joel Baker See Design Studio
	Daniel Hollingworth Lateral Planning
Observers from DevelopmentWA	Emily Grindrod* Head of Planning, DevelopmentWA
	Owen Thomas* Program Manager, DevelopmentWA
	Gavin Casey Manager Planning, DevelopmentWA
	Matthew Andrews Senior Planner, DevelopmentWA
Declarations	
None	
Briefings	
	Design presentation from the project architects: <ul style="list-style-type: none"> • Lionel Mawhinney – DKO • Joel Baker – See Design Studio (Landscape)
Design Review Report endorsement	
Chair Signature	
	Emma Williamson, Government Architect
Executive Summary	
Overall	<p>The Panel thanks the project team for their continued engagement with the design review process.</p> <p>The Panel acknowledged the substantial work undertaken since DRP2, noting that the proposal demonstrates clear improvement, design advice being considered and incorporated into the current design.</p> <p>The Panel supported the shift away from a commercial expression toward a more domestic, residential quality, particularly through façade articulation, balcony proportions, and material intent across the towers. The landscape approach is also strongly supported including the extent of deep soil, retention of existing trees and use of landscaping as a key design driver rather than an afterthought.</p> <p>The Panel agreed that the proposal has substantially improved since DRP2 and is close to meeting Design Excellence, however identified design elements that require further refinement, including reconsideration of the central forecourt and the Subiaco Road / Court Place corner treatments, clarification regarding the construction methodology, material selection, and entry legibility and wayfinding.</p> <p>Accordingly, this additional detail on amenity, legibility, safety, and materiality is considered necessary before the required confirmation of Design Excellence.</p>

<p>Summary</p>	<p>The Panel support the overall direction of the proposal. However, further refinement is required in the following key areas to demonstrate Design Excellence:</p> <ul style="list-style-type: none"> • Further design development of the central forecourt to better contribute to public amenity, attraction and activation, and spatial purpose, particularly consideration for after hours. • Further design development of the Subiaco Road / Court Place corner treatment recognising its significance as a key focal location for both the development and the broader Subi Oval precinct. This location should demonstrate a high quality design which strengthens the sense of community, wayfinding and sense of place, positively supporting external food and beverage activation and the overall public realm experience. • Further details of the intended material strategy, particularly for the tower forms, including contingency plans should the preferred material/construction methodology not be considered viable. Greater confidence is required that the intended material quality and durability will be delivered, particularly if construction methodology changes (e.g. precast versus lightweight systems). • A consolidated, independent CPTED report is required to clearly demonstrate how safety principles are embedded in the design. An internally prepared assessment is not considered to be sufficient for a development of this scale and use. • Improved entry legibility and architectural expression to residential lobbies. • Improved amenity of lower-level dwellings, including further consideration of privacy, interaction with the street, stoop depth, and planting interfaces. • Greater amenity within the ground floor public realm through prioritisation of shared, welcoming spaces with improved comfort, shade, seating, and integration of public art.
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<p>Design Excellence Framework</p>	
<p>The Panel considers Design Quality in social and affordable housing to mean delivering cost-effective, inclusive, and accessible environments that meet the needs of vulnerable communities while fostering and strengthening social cohesion. It recognises the importance of affordability delivered through climate-responsive design that minimises energy use, promotes the use of durable materials that minimise long-term maintenance and operational costs, and a design approach that encourages safety, inclusivity, and integration with the wider community.</p> <p>Design Excellence as it applies to this development is defined within the Subiaco Oval and Railway Precinct Design Guidelines. Design Excellence requires achieving an exceptional outcome by addressing all of the SPP7.0 principles of good design and then going above and beyond. Guidance is provided within Appendix 2 of the Design Guidelines.</p> <p>Design Excellence is required to be achieved for landmark sites identified under the Design Guidelines, which includes Lot 11.</p> <p>To achieve Design Excellence the proposal must:</p> <ol style="list-style-type: none"> 1. Demonstrate the way in which the proposal goes above and beyond when addressing amenity, sustainability, durability and maintenance, and safety and security across the entire development. 2. Demonstrate the way in which the proposal goes above and beyond the way in which the Lot 11 3. portion of the development responds to its prominent, public location. 	

Design Quality Evaluation
<p>Principle 1: Context and character <i>Good design responds to and enhances the distinctive characteristics of a local area, contributing to a sense of place.</i></p> <ol style="list-style-type: none"> 1. Further refinement of the Subiaco Road / Court Place corner to ensure that it recognises and celebrates this key location, positively contributes to the local streetscape character and neighbourhood scale and acts as a successful wayfinding component for both the development and greater precinct. 2. Strengthen the architectural expression at key interfaces and entrances to improve legibility and wayfinding without the need for excessive signage. 3. Clarify how the proposal balances landmark scale with local, human-scale qualities. 4. The landmark role of Tower 4 should be reinforced through human-scale responses at street level, not solely through building height and a tower to ground form.
<p>Principle 2: Landscape quality <i>Good design recognises that together landscape and buildings operate as an integrated and sustainable system, within a broader ecological context.</i></p> <ol style="list-style-type: none"> 1. Retain the strong landscape narrative while ensuring planting does not unintentionally reduce activation or surveillance. 2. Review maintenance access and long-term management of raised planters and podium landscapes. 3. Ensure landscape design supports passive surveillance to and from the PSP and avoids creating concealed or inactive areas. 4. Planting on structure and the connection of the 'green link' through the site heading north-south is a positive change. 5. Strengthen the central forecourt as a meaningful gathering space through softening of the utilitarian feel being generated by the Community Housing Provider office. 6. Ensure public spaces remain welcoming and safe outside of office hours. 7. Throughout the public realm interface increase seating variety, shade, and incidental gathering opportunities to support lingering and informal use.
<p>Principle 3: Built form and scale <i>Good design ensures that the massing and height of development is appropriate to its setting and successfully negotiates between existing built form and the intended future character of the local area.</i></p> <ol style="list-style-type: none"> 1. Re-examine the ground-to-tower transition, particularly where tower forms meet public spaces. 2. The removal of the former colonnade on Lot 11 has improved openness; however, the corner condition now lacks sufficient architectural structure and purpose and has significantly lost the human scale. Improve articulation and spatial definition at the corner condition, potentially through lighter architectural elements. 3. Consider use of articulation at key public interfaces to avoid overly flat or a more corporate architectural expression.
<p>Principle 4: Functionality and build quality <i>Good design meets the needs of users efficiently and effectively, balancing functional requirements to perform well and deliver optimum benefit over the full life-cycle.</i></p> <ol style="list-style-type: none"> 1. The Panel requested further assurance that construction methodologies will deliver the intended quality and durability outcomes, particularly if materials change. 2. Demonstrate durability of alternative lightweight systems, particularly at podium and public interfaces. 3. Address long-term maintenance strategies for facades, glazing, shading devices, and landscape elements. 4. Confirm that materials align with the project's role as a long-term retained asset with low maintenance requirements/costs.
<p>Principle 5: Sustainability <i>Good design optimises the sustainability of the built environment, delivering positive, environmental, social and economic outcomes.</i></p> <ol style="list-style-type: none"> 1. The Panel generally supports the sustainability approach, including passive design and shading strategies. This should continue to be developed in the next stages of the design. 2. Greater clarity is required regarding independent verification processes and how performance outcomes will be demonstrated. 3. Sustainability initiatives should continue to prioritise resident comfort and operational cost benefits, rather than solely meeting minimum targets. 4. Integration between ESD measures, facade design, and material selection should be clearly demonstrated.
<p>Principle 6: Amenity <i>Good design provides successful places that offer a variety of uses and activities while optimising internal and external amenity for occupants, visitors and neighbours, providing environments that are comfortable productive and healthy.</i></p> <ol style="list-style-type: none"> 1. The central forecourt requires further design development to provide a more comfortable, inviting and genuinely shared space of high amenity.

2. Address the street interface for the terrace style dwellings to the west on Subiaco St, to ensure a balance of privacy with the opportunity for incidental social interactions. Consider levels, stair and entry locations, planting location and height-
3. The relationship between indoor communal facilities and outdoor spaces should be strengthened.

Principle 7: Legibility *Good design results in buildings and places that are legible, with clear connections and easily identifiable elements to help people find their way around.*

1. Residential entries lack sufficient architectural emphasis and should be made more visually apparent without reliance on signage alone.
2. Improve wayfinding to each tower/lobby, Community Housing Provider offices and commercial tenancies without reliance on signage alone (e.g. use of colour, canopy, form and/or architectural language).
3. Increase weather protection at entries, particularly for pedestrians and cyclists.
4. Internal lobby sightlines within Tower 2 to be reviewed to allow for passive surveillance of the central forecourt.

Principle 8: Safety *Good design optimises safety and security, minimising the risk of personal harm and supporting safe behaviour and use.*

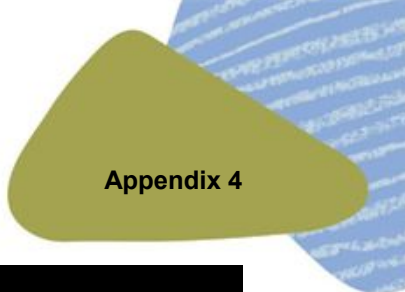
1. The balance of passive surveillance with privacy requires further consideration in relation to the terrace style housing to the street.
2. The Panel requests a consolidated, independent CPTED report demonstrating how safety principles are embedded throughout the design.
3. Address after-hours safety of public and semi-public spaces, particularly the central forecourt.
4. Landscape and built elements should avoid creating concealed, inactive or poorly overlooked areas.

Principle 9: Community *Good design responds to local community needs as well as the wider social context, providing environments that support a diverse range of people and facilitate social interaction.*

1. Public and semi-public spaces should better support informal community interaction, not just circulation or servicing.
2. The central forecourt should function as a welcoming shared space with a stronger sense of civic purpose.
3. Ground-level design should support incidental interaction between residents and the wider community.
4. Opportunities to reinforce community identity through public art, seating and spatial design should be further explored.
5. Consider maximising informal seating in the public realm to support incidental engagement

Principle 10: Aesthetics *Good design is the product of a skilled, judicious design process that results in attractive and inviting buildings and places that engage the senses.*

1. The Panel supports the aesthetic direction including the move away from the overly corporate architectural expression of the towers.
2. Public art integration requires reconsideration in terms of location, scale and emphasis, particularly along the northern interface when viewed at speed from the train and the PSP.
3. Tower 4 should not be defined as a landmark solely through the use of public art on the eastern façade, with greater focus and stronger definition at the ground floor, at the human scale, sought.



Appendix 4

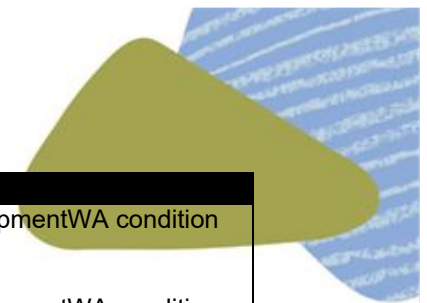
Summary of Stakeholder Comments

Stakeholder	Comments	Consideration
<p>City of Subiaco</p>	<p>In response to the development application referred to the City of Subiaco on 9 February 2026 for the construction of a 7 to 18 storey mixed use development including 447 Multiple Dwellings (Social and Affordable housing), Office, and 2 x Restaurant/Cafes at No. 154 (Lot 1) Subiaco Road, Subiaco, DevelopmentWA is advised that in accordance with the Metropolitan Redevelopment Authority Act 2011, the City of Subiaco SUPPORTS the application subject to the matters for consideration outlined in Attachment 1 being addressed</p> <p>Attachment 1 – Matters for consideration, without prejudice conditions and advice notes</p> <p><u>Matters for consideration</u></p> <ol style="list-style-type: none"> 1. It is recommended that DevelopmentWA reviews the ‘typical floorplate’ drawings to ensure that they correspond to the section and elevation drawings in relation to the overall heights. 2. That DevelopmentWA requires the applicant to submit further details on the criteria for assessing the eligibility of tenants for the affordable and social housing, in order to address Development Policy 9 – Affordable Housing. 3. Confirm or increase the size of storerooms to comply with the minimum storage area requirements in Element 4.8 of the Residential Design Codes WA Volume 2. 4. Ensure that balcony areas proposed are of adequate proportions to address Element 4.4 of the Residential Design Codes WA Volume 2. 5. Include additional bicycle storage noting the large shortfall proposed and to capitalise on access to the Fremantle Principal Shared Path. 6. Ensure the resident bicycle store and visitor bicycle parking are designed to provide secure, weather-protected, fit-for-purpose storage that accommodates: 	<p>The City’s support for the application is acknowledged, and responses to the specific matters raised are provided below.</p> <ol style="list-style-type: none"> 1. Typical floorplates will be reviewed through the detailed design to ensure that the construction plans are consistent with the approved development. Minor variation in dimensions and heights are expected as detailed design is undertaken. 2. The Proposal is Housing Australia Funded and 100% of the dwellings are to be affordable or social housing meeting the requirements of Development Policy 9. Community Housing Limited is a Registered Community Housing Provider and will own and manage the units. Eligibility for tenants is controlled through various acts and regulations outside of the planning framework. A condition is recommended to confirm that the proposal meets DP9 requirement prior to occupancy. 3. Storeroom size and dimensions are generally compliant with the

Stakeholder	Comments	Consideration
	<ul style="list-style-type: none"> a) e-bikes (space/handling needs and reasonable charging management). b) e-scooters/micromobility devices (secure storage and management expectations); and c) a variety of bicycle types, including non-standard bicycles (rack types/layout, aisle widths, and access arrangements that do not disadvantage heavier or larger bikes). <p>7. It is recommended that DevelopmentWA considers and investigates the Engineering matters raised in Attachment 4 – Internal referral comments, specifically having regard to crossovers, waste truck collection, on-street parking, TIA updates, intersection capacity analysis and ramps</p> <p>8. It is recommended that DevelopmentWA considers and investigates the Parks matter raised in Attachment 4 – Internal referral comments, specifically guidance on tree planting and preferred species, along with the interface to the Fremantle Principal Shared Path.</p> <p>9. The Community Housing Provider prepares and implements a Green Travel Plan prior to occupation to support sustainable transport alternatives to private vehicle use and to minimise potential impacts on the local parking environment. The plan should:</p> <ul style="list-style-type: none"> a) outline measures to encourage walking, cycling and public transport use. b) address micromobility expectations (including appropriate storage/parking behaviour). c) provide resident information (routes, nearby facilities, public transport options); and d) include building management measures to reduce the likelihood of overflow parking pressures and related tensions on surrounding streets. 	<p>acceptable outcomes of the R-Codes Volume 2.</p> <p>4. Balcony size and dimensions are generally compliant with the acceptable outcomes of the R-Codes Volume 2.</p> <p>5. Separate bicycle storage is considered to be sufficient for each tower. Additional storage is available for each dwelling within storerooms located within the podium. Conditions and Advice Notes are recommended to ensure that bicycle storage is provided within storerooms as required by tenants at the cost of the CHP managing the facility.</p> <p>6. Bicycle storerooms are internal to the building being weatherproof and secure. Internal layouts are subject to further detailed design and engagement with the community housing provider.</p> <p>7. Comments on the vehicle access, crossovers and swept paths are noted and will be considered as part of the detailed design. Modelling and outcomes from the TIA are considered to be sufficient for this stage of the development noting that built form outcomes on other sites</p>

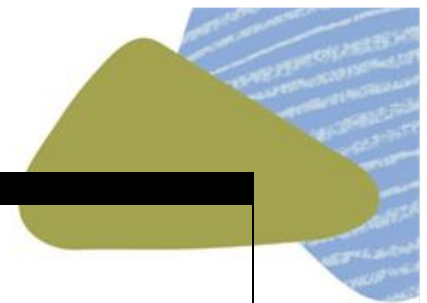
Stakeholder	Comments	Consideration
		<p>within the precinct are still unknown and based on advice from Main Roads. Bicycle parking provision and design are considered appropriate for the development. Final design will be considered further during the planning condition clearance stage. Conditions are recommended to ensure that drainage is contained on-site and that a CMP is provided.</p> <p>8. Comments from the parks team are noted. Design matters relating to the northeast and adjacent to commercial tenancies have been considered through the design review process and are considered satisfactory. Final details on tree planting density and species will be provided during the detailed design phase.</p> <p>9. The site will be managed by a community housing provider located on the property. Support will be provided to tenants such as local maps, transport options and benefits of public and active transport uses. The on-site community housing provider will also ensure that parking on site and within the immediate area is managed appropriately, including engagement with local schools and City of Subiaco. Car bay allocation</p>

Stakeholder	Comments	Consideration
		and management will be addressed via a condition of approval.
	<p><u>Without prejudice conditions</u> Should DevelopmentWA approve the application, the following conditions and advice notes are recommended:</p> <ol style="list-style-type: none"> 1. All stormwater generated on site is to be retained on site. No stormwater will be permitted to enter the City of Subiaco’s stormwater drainage system unless otherwise approved. 2. Prior to the issue of a building permit, the applicant is to submit a Wind Tunnel Testing Assessment (WTTA). Any recommendations made in the WTTA are to be implemented and incorporated in the design to mitigate the impact of potential wind tunnelling on the development, adjacent sites and streets, to the satisfaction of DevelopmentWA. 3. Prior to the issue of a building permit, the applicant shall submit and have approved by the City of Subiaco, a detailed noise management plan demonstrating that potential noise from the proposed development will comply with the requirements of the Environmental Protection (Noise) Regulations 1997. The plan is to be prepared by a suitably qualified consultant and is to address matters including, but not limited to: <ol style="list-style-type: none"> a) Type and specifications of selected mechanical plant and equipment and screening; b) Delivery and on site operations associated with the restaurant/café; c) Predictions of noise levels; d) Sound proofing measures used in the design and construction of the development; e) Control measures to be undertaken (including monitoring procedures); and f) A complaint response procedure. 	<ol style="list-style-type: none"> 1. Standard DevelopmentWA condition recommended. 2. Standard DevelopmentWA condition recommended requiring a final wind assessment to be prepared. 3. Standard DevelopmentWA condition recommended. 4. Standard DevelopmentWA condition recommended. Additional requirements within the CMP will include protection of the rail reserve as recommended by PTA. 5. Final details of the bicycle parking areas will be considered through detailed design to ensure that they address the requirements of the Design Guidelines. 6. Standard DevelopmentWA condition recommended. 7. Standard DevelopmentWA condition recommended.

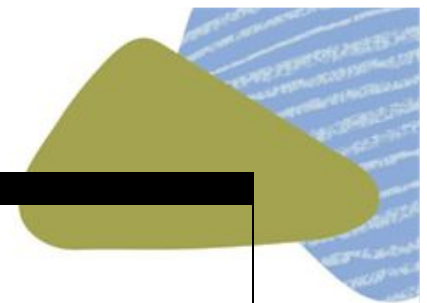


Stakeholder	Comments	Consideration
	<p>The approved plan and its recommendations, as well as any additional requirements by the City, shall be implemented prior to occupancy of the development and adhered to at all times, to the satisfaction of DevelopmentWA on advice from the City of Subiaco.</p> <p>4. Prior to the issue of a building permit, a Construction Management Plan shall be prepared and submitted to the satisfaction of DevelopmentWA. This plan is to address:</p> <ul style="list-style-type: none"> a) construction noise; b) hours of construction; c) dust; d) vibration; e) waste; f) sand; g) temporary fencing; h) scaffolding; i) hoarding; j) gantries; k) site access and egress; l) site deliveries; m) traffic management; n) parking management; o) management of loading and unloading of vehicles; p) heavy vehicle access; q) protection of verge trees; and r) any other relevant matters. <p>The requirements of this plan are to be observed at all times during the construction process to the satisfaction of DevelopmentWA on advice from the City of Subiaco.</p> <p>5. Prior to the issue of a building permit, the applicant shall submit a detailed bicycle storage layout, rack specifications, access control, lighting, charging facilities, and management approach to the</p>	<p>8. Standard DevelopmentWA condition recommended.</p> <p>9. Standard DevelopmentWA condition recommended.</p> <p>10. Condition supported.</p> <p>11. Standard DevelopmentWA condition recommended.</p> <p>12. Standard DevelopmentWA condition recommended.</p> <p>13. Standard DevelopmentWA condition recommended.</p> <p>14. Standard DevelopmentWA condition recommended.</p> <p>15. Condition supported.</p> <p>16. Condition supported. Standard DevelopmentWA condition recommended.</p> <p>17. Standard DevelopmentWA condition recommended.</p> <p>18. Standard DevelopmentWA condition recommended.</p> <p>19. Standard DevelopmentWA condition recommended.</p>

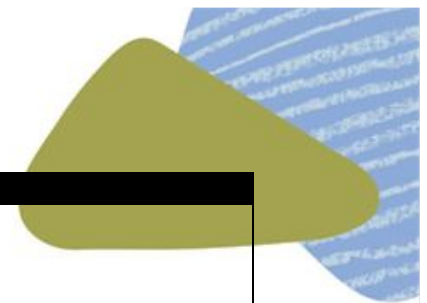
Stakeholder	Comments	Consideration
	<p>satisfaction of DevelopmentWA, in consultation with the City of Subiaco.</p> <ol style="list-style-type: none"> 6. Prior to the issue of a building permit, the applicant shall submit a schedule of materials, colours, finishes and textures for the development to the satisfaction of DevelopmentWA. 7. All existing street trees shall be protected and maintained with no pruning permitted without the prior approval of the City of Subiaco. 8. Prior to the commencement of physical works, Tree Protection Zones (TPZ) are to be established and maintained around each existing street tree for the duration of the construction. The TPZ is to be a minimum of two (2) metres extending out from the base of the tree. 9. Prior to the occupation of the development, an updated waste management plan shall be submitted to the DevelopmentWA for approval on advice from the City of Subiaco. The plan shall be implemented and adhered to throughout the life of the development. 10. Prior to occupation of the development any new or modified crossover associated with the hereby approved development must receive separate approval from the City of Subiaco. 11. Public art is to be provided in accordance with DevelopmentWA's Development Policy 4 – Providing Public Art, details of which are to be provided at working drawings stage to the satisfaction of DevelopmentWA, in consultation with the City of Subiaco. 12. Prior to occupation, all parking bays are to be clearly line marked and maintained thereafter to the satisfaction of DevelopmentWA. 13. All car parking dimensions, manoeuvring areas, crossovers and driveways shall accord with the Australian Standard AS2890.1 (as amended). 14. Prior to occupation, the Community Housing Provider is to submit an operational management plan for endorsement by the DevelopmentWA, covering: <ol style="list-style-type: none"> a) Procedures for handling complaints b) Management of tenant behaviour c) Use and maintenance of common facilities d) On-site or off-site management arrangements 	<ol style="list-style-type: none"> 20. Included under recommended condition 10 21. The existing verge areas include landscaping and tree planting delivered by DevelopmentWA. Additional street trees are not proposed. 22. Standard DevelopmentWA condition recommended. 23. Standard DevelopmentWA condition recommended. 24. Standard DevelopmentWA condition recommended. 25. Standard DevelopmentWA condition recommended. 26. Standard DevelopmentWA condition recommended. Dilapidation reports for dwellings will be provided as necessary within an appropriate radius.



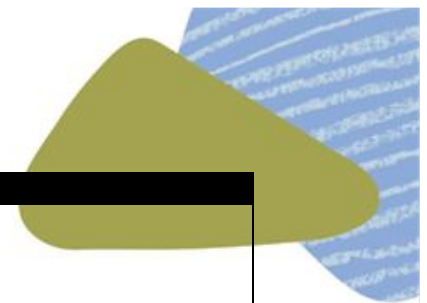
Stakeholder	Comments	Consideration
	<p>e) Any other relevant operational considerations</p> <p>The endorsed operational management plan shall be adhered to for the life of the development to the satisfaction of DevelopmentWA, in consultation with the City of Subiaco.</p> <p>15. Prior to the issue of a building permit, the applicant shall submit a statement of compliance prepared by a Green Star Accredited Professional confirming that the development can achieve the targeted informal (equivalency) 5 star Green Star Buildings rating performance, to the satisfaction of DevelopmentWA, in consultation with the City of Subiaco. The Statement of Compliance must include a formal peer review by an independent Green Star Accredited Professional.</p> <p>16. Prior to occupation of the development, the applicant shall submit to DevelopmentWA a Statement of Compliance completed by an independent peer reviewer confirming that the project has delivered upon the sustainability initiatives claimed to contribute to the sustainability performance target for the project. Any outstanding initiatives or deliverables are to be clearly outlined with confirmation of when works will be completed, to the satisfaction of DevelopmentWA on advice from the City of Subiaco.</p> <p>17. Within 12 months of practical completion of the development, all outstanding sustainability initiatives and deliverables must be completed. The applicant is to submit to DevelopmentWA documentation confirming as such, to the satisfaction of DevelopmentWA on advice from the City of Subiaco.</p> <p>18. Prior to occupation of the development, all external fixtures including, but not limited to TV and radio antennae, satellite dishes, plumbing vents and pipes, solar panels, air conditioners and hot water systems shall be integrated into the design of the building and not be visible from the primary street and secondary street to the satisfaction of DevelopmentWA.</p> <p>19. Prior to the issue of a building permit, the applicant shall submit a final acoustic report demonstrating how suitable noise attenuation</p>	



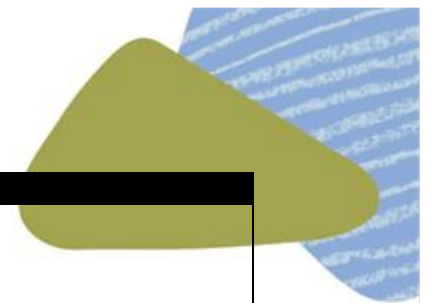
Stakeholder	Comments	Consideration
	<p>measures have been incorporated into the development to address State Planning Policy 5.4 – Road and Rail Noise, to the satisfaction of DevelopmentWA. Prior to occupation, all endorsed noise attenuation measures shall be implemented into the design and maintained thereafter, to the satisfaction of DevelopmentWA.</p> <p>20. Prior to issue of a building permit, the applicant is to make suitable arrangements with the City of Subiaco to relocate any existing infrastructure within the verge in order to accommodate crossover(s), and approval must be sought from the City of Subiaco prior to its relocation. All costs associated with relocation of the infrastructure are the responsibility of the property owner(s).</p> <p>21. Prior to issue of a Building Permit, the applicant shall prepare a street tree planting plan in consultation with the City of Subiaco, detailing proposed street tree locations and species, ongoing maintenance and any other relevant matters. The plan shall be approved by the City of Subiaco and implemented prior to occupation of the development.</p> <p>22. Prior to the issue of a building permit, the applicant is to submit a final landscaping plan which shall demonstrate how all landscaped areas can be accessed and maintained, to the satisfaction of DevelopmentWA on advice from the City of Subiaco.</p> <p>23. Prior to occupation, landscaping shall be completed in accordance with the approved plans or any approved modifications thereto, to the satisfaction of the City of Subiaco. All landscaped areas are to be maintained on an ongoing basis for the life of the development on the site, to the satisfaction of DevelopmentWA on advice from the City of Subiaco.</p> <p>24. Prior to the issue of a building permit, the applicant shall provide details on a minimum of 20% of all vehicle parking spaces being design with capacity for electric vehicle charging. Prior to occupation, the electric vehicle charging capacity shall be implemented to all required parking bays, to the satisfaction of DevelopmentWA.</p> <p>25. Prior to the issue of a building permit, the applicant shall provide details on the final number of apartments to meet the silver and gold level of apartment design in accordance with Livable Housing</p>	



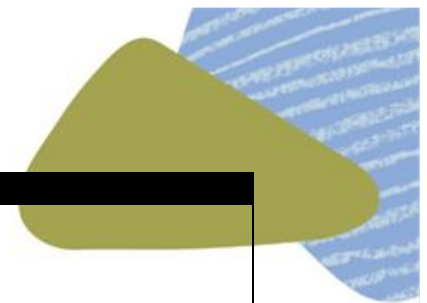
Stakeholder	Comments	Consideration
	<p>Australia, in order to address Development Policy 10, to the satisfaction of DevelopmentWA.</p> <p>26. Prior to the issue of a building permit, a dilapidation report prepared by a suitably qualified professional shall be submitted to the City of Subiaco for approval, and the owners of the adjoining properties listed below detailing the current condition and status of all buildings (both internal and external together with surrounding paved areas and rights of ways), including ancillary structures located on these properties/structures:</p> <ul style="list-style-type: none"> a. 192 Subiaco Road SUBIACO b. 194 Subiaco Road SUBIACO c. 194A Subiaco Road SUBIACO d. 1-1/196 Subiaco Road SUBIACO e. 1-4/196-200 Subiaco Road SUBIACO f. 1-5/2 Court Place SUBIACO g. 1-12/148 Subiaco Road SUBIACO h. 150 Subiaco Road SUBIACO i. 200 Roberts Road (Bob Hawke College) SUBIACO j. Public realm directly to the south of 154 Subiaco Road, including footpaths, public open space and community facilities adjoining the site at Subiaco Oval. <p>If the dilapidation survey is denied by an adjoining owner, the applicant must demonstrate in writing to the satisfaction of the City of Subiaco, that all reasonable steps have been taken to obtain access and advise the affected property owner of the reason for the survey and that these steps have failed. Dilapidation reports shall be prepared to the satisfaction of the City of Subiaco.</p>	
	<p><u>Advice Notes</u></p> <ol style="list-style-type: none"> 1. A building permit is required to be obtained for the proposed building work. The building permit must be issued prior to commencing any works on site. Please contact the City's Building Services on 9237 9222 for further information. 2. The need to comply with the requirements of Part D3 of the Building Code of Australia - Access for People with Disabilities, including 	<p>Advice notes are recommended as necessary to address the recommended conditions.</p>



Stakeholder	Comments	Consideration
	<p>parking, sanitary facilities and tactile indicators in accordance with AS 1428.1, AS 1428.4, AS 1428.5 and AS/NZS 2890.6.</p> <ol style="list-style-type: none"> 3. Ensure that all proposed tree species are not prone to Shot-Hole Borer insect attack. Please contact the City's Parks Services on 9237 9222 for further information. 4. In relation to the dilapidation report(s), these should address the following matters to satisfy the City of Subiaco: <ol style="list-style-type: none"> a) Dilapidation reports are to clearly identify the name of the organisation and person(s) undertaking the inspection and shall include their relevant qualifications; b) Dilapidation reports are to be prepared in accordance with relevant legislation and standards, including Australian Standard AS4349.1 (or equivalent); c) A calibration gauge should be used for measurement, in combination with high resolution photos for accurate record keeping; d) Landowners of properties requiring dilapidation reports are to be afforded the opportunity by the applicant to identify any existing areas of concern that can be visually identified and recorded in any dilapidation report; e) The applicant, in consultation with the owner of the property requiring the dilapidation report, is to ensure that every reasonable effort is made to ensure that they can obtain safe and reasonable access to any and all areas of a property requiring dilapidation reports; f) Landowners of properties requiring dilapidation reports are to be provided the same copy and version of the dilapidation report as any submitted to the City of Subiaco as part of any request to clear conditions of development approval as required; g) For the City to be fully satisfied, consideration will be given to written feedback from landowners up to seven days after receipt of the dilapidation report; and h) In the event that access for undertaking the dilapidation survey is denied by an adjoining owner, the applicant must demonstrate in 	

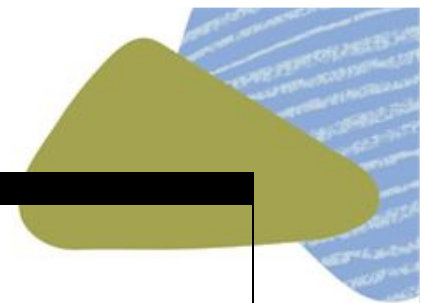


Stakeholder	Comments	Consideration
	<p>writing to the satisfaction of the City of Subiaco, that all reasonable steps have been taken to obtain access and advise the affected property owner of the reason for the survey and that these steps have failed. Dilapidation reports shall be prepared to the satisfaction of the City of Subiaco.</p> <ol style="list-style-type: none"> 5. A tree protection zone is a specified area above and below ground level that is set aside for the protection of the trees structural root system. The installation of a TPZ around all City trees adjacent the development site must be installed during a development that may impact on the verge trees. Please contact the City's Parks Services on 9237 9222 for further information, prior to the commencement of works. 6. The updated waste management plan should note the city does not provide 660l bins for the collection of FOGO, general waste is collected fortnightly not weekly, the City provides 2 bulk hard waste collections per year. It should also be noted that the city will require a minimum of 8 weeks' notice prior to occupation to allow for the procurement of the bins required for this proposal. Please contact the City's Waste Services on 9237 9222 for further information. 7. In relation to public art, the provision of public art to a minimum value of 1% of the construction is to be incorporated into the development in accordance with Development Policy 4 – Providing Public Art. 8. An independent Green Star Accredited Professional means a third-party consultant that is otherwise not part of the project team (or an associated subsidiary of an engaged entity) and is engaged to verify that the claimed sustainability initiatives and associated deliverables have been completed. They must be suitably qualified, namely a practicing Green Star Accredited Professional. The City of Subiaco encourages early engagement with the peer reviewer to ensure the proposed approach is appropriate for the project, to the satisfaction of the peer reviewer. Any alternative methodologies or initiatives (i.e. based on project scale or complexity) is to be agreed with the peer reviewer and the City of Subiaco. 	



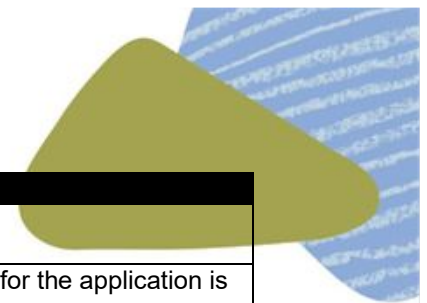
Stakeholder	Comments	Consideration
	<p>9. The applicant is advised that the Green Star Certification or Equivalency pathway should address the following considerations and deliverables:</p> <ul style="list-style-type: none"> a) Green Star Energy Modelling b) Upfront carbon emissions calculations c) LCA Modelling d) Heat Resilience calculations e) Climate change risk assessment and adaptation report f) Ventilation rates schedule confirming percentage improvement in outside air g) Provide Zero Carbon Action Plan where Minimum Expectation standard is claimed h) Prior to Occupancy Stage: <ul style="list-style-type: none"> a. Energy supply contract for 100% renewable power for base building for minimum 5 years b. Commissioning reporting including air tightness and acoustic testing (where relevant) results demonstrating compliance with target. i) Suitable evidence for all other Green Star credits claimed should be agreed upon with the independent peer reviewer. Note, all testing and validation must be in accordance with Green Star methodologies. <p>10. Please be advised:</p> <ul style="list-style-type: none"> a) the exterior fixture associated with any air-conditioning unit or hot water system is considered an appropriate location where it is positioned: <ul style="list-style-type: none"> a. outside of balcony/verandah areas (if applicable); and below the height of a standard dividing fence within a side or rear setback area; or b. within a screened rooftop plant area or nook c. the exterior fixture associated with any TV and radio antennae, satellite dishes, essential plumbing vents above the roof line, down pipes and solar panels are considered minor fixtures and deemed-to-comply. 	

Stakeholder	Comments	Consideration
<p>Town of Cambridge</p>	<p>The Town supports the proposed development, as it broadly aligns with the strategic and statutory planning framework applicable to the site and the surrounding locality for the following reasons:</p> <ol style="list-style-type: none"> 1. The site is included within the Subiaco East Project Area under DevelopmentWA's Subiaco Redevelopment Scheme No. 2 (SRS 2). Within the Subiaco Oval Precinct, residential and commercial uses are identified as preferred land uses along the railway interface, with culture, creative industry, retail, dining and entertainment contemplated. The Proposed Development is largely consistent with this framework, comprising 447 multiple dwellings across four towers as the primary use, a 165.5m² office tenancy fronting the Subiaco Road forecourt, and two dining and entertainment tenancies (76.8m² and 115.7m²) fronting Court Place — all of which align with the preferred and contemplated land use categories under SRS 2. 2. The proposed building heights are consistent with the maximum heights permitted under the Subiaco Oval and Railway Precinct Design Guidelines, with Towers 1-3 averaging 11 storeys and Tower 4 achieving the 18-storey maximum designated for the landmark site at Lot 11. The Town notes that Tower 4, being the tallest element at 18 storeys, is positioned at the eastern end of the site, in close proximity to the Rosslyn Quarter on the opposite side of the railway corridor within the Town, which itself contemplates development of up to 18 storeys under the recently approved West Leederville Precinct Structure Plan (WLPSP, approved by the WAPC on 2 February 2026). In this context, the proposed height at this end of the site is considered consistent with the broader strategic intent for the locality on both sides of the boundary. 3. The proposed development is physically being buffered by two reserves, two roads, and a rail line from the Town of Cambridge. Having regard to this separation, the visual and amenity impacts of the proposed development on the Town's area are considered negligible. 	<p>The City's support for the application is acknowledged.</p> <p>Comments on the compliant aspects of the proposal as noted.</p> <p>Support for this proposal at a height of 18 storeys is noted as being appropriate for the site having regard to the applicable framework and that any development within the Town of Cambridge will be assessed on its merits against the relevant framework.</p>

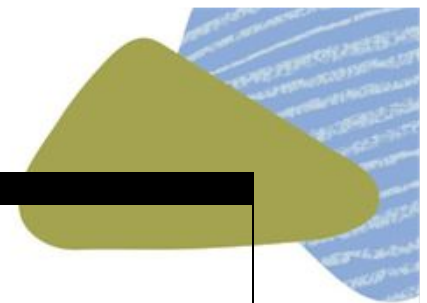


Stakeholder	Comments	Consideration
	<p>4. Notwithstanding the above, the Town wishes to expressly place on record that this referral response is not to be construed as support of any future proposal seeking 18 storeys or greater on land within the Town of Cambridge's jurisdiction. The WLPSP has now been approved and provides clear and specific height controls for the West Leederville Activity Centre. The Town will assess all future development applications within its jurisdiction against the WLPSP and its Local Planning Scheme No. 1. The assessment and determination of any Town of Cambridge applications should not be based on the outcomes within the adjacent Subiaco East redevelopment area.</p>	
<p>Department of Education</p>	<p>Given the proximity of the Bob Hawke College, careful planning of any future development and further sensitive design consideration needs to be undertaken on the subject site to protect the safety, wellbeing and privacy of students and operation and amenity of the College. The Department notes that the proposed mixed use development is required to conform with the Subiaco Oval Precinct's development objectives and the precinct-specific provisions contained in the Subiaco Oval and Railway Precinct Design Guidelines (Design Guidelines).</p> <p><u>Building interface</u> Of relevance, section 4.6.3 of the Design Guidelines states the following: "Development will be oriented in a manner which discourages overlooking of school open space in recognition of the significance of Bob Hawke College. Notwithstanding, upper floors will be sited to maximise views to surrounding regional landmarks and open views through development sites." However, the south elevation façades of proposed residential Tower 3 and 4 feature habitable room windows and balconies which have direct interface with Bob Hawke College. The supporting Planning Report is silent on addressing this development objective. In addition, section 4.6.9 of the of the Design Guidelines states that the minimum setback from the upper floor level to Subiaco Road is 6 metres. However, Tower 3 upper floor levels have a minimum setback of 5.2 metres.</p>	<p>The Department's support for the application is acknowledged, and responses to the specific matters raises are provided below.</p> <p><u>Building interface</u> The minor variation to the upper level setback is not considered to result in adverse amenity impacts. The combination of deeper setbacks at the lower levels, slim tower form and reduced building envelopes provide sufficient separation from the street and surrounding properties.</p> <p><u>Land Use Compatibility</u> The preferred uses for the site are Shop and Restaurant/Café. The final land use will be considered during the planning condition clearance process with due regard given to the compatibility of the land use with the surrounding land uses,</p>

Stakeholder	Comments	Consideration
	<p><u>Land use compatibility</u> Schools are deemed to be sensitive land uses and it is imperative to ensure that school sites are located amongst or adjacent to compatible land uses to support education, health, and wellbeing outcomes of students. There are certain land uses which are not considered to be compatible near school sites which include, but are not limited to, fast food outlets, service stations, smoke/vape shops and liquor licensing premises.</p> <p>Whilst the proposed Office component is supported, it is noted in the section 6.3.2 of the Planning Report that the intended use for Tenancies 1 and 2 is likely to include café, bakery, ice cream shop, etc and is not intended for these tenancies to apply for liquor licencing or extended trading permits. Further, the development acknowledges the need for the commercial tenancies to 'remain complimentary with the development and broader community'. In this regard, the Department is supportive of this approach. However, should there be any change of land use in the future on the subject site, it is requested that the proposal be referred to the Department for review and comments.</p> <p><u>Car parking</u> The proposed development consists of 308 on-site car bays whilst the required number of on site car bays is a minimum of 436. This presents a parking shortfall of approximately 128 car bays. Notwithstanding the applicant's 'recognition of the availability of alternative transport options' to justify the parking shortfall, the Department expects that there would not be significant reliance on the on-street car bays along Subiaco Road, particularly during the school peak periods, as these bays are utilised for drop-off and pick-up of students from the adjoining College.</p> <p>In view of the above, the Department has no in-principle objections to the proposed development subject to the above matters being addressed. The Department welcomes the opportunity to discuss with the DevelopmentWA on the above matter and consider any alternative design proposals to ensure</p>	<p>in particular the school and adjacent residential land uses.</p> <p><u>Car parking</u> Management of on-street carparking will be an ongoing discussion with the City of Subiaco, with the aim to ensure that short term parking is available during peak pick-up and drop-off times for nearby schools. Consideration to parking availability during the construction phase will also be managed through a Construction Management Plan, intended to be prepared in consultation with impacts stakeholders.</p>



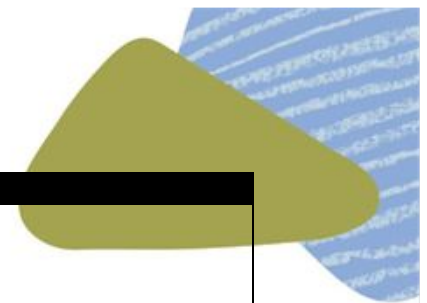
Stakeholder	Comments	Consideration
	<p>that the proposed development aligns with the intent of the Design Guidelines and WAPC's Operational Policy 2.4 – Planning for School Sites.</p>	
<p>Main Roads</p>	<p>In response to correspondence received on 12 February 2026, Main Roads has no objections to the proposal and provides the following conditions for the DevelopmentWA's consideration:</p> <p><u>Conditions</u></p> <ol style="list-style-type: none"> 1. Prior to the submission of a building permit application, this noise sensitive development adjacent to a major transport corridor must implement measures to ameliorate the impact of transport noise. The development is to comply and implement the Noise Management Plan prepared by Herring Storer Acoustics, Ref 34788-3 25214, revision 2 and dated 21 January 2026 with the following amendments: <ol style="list-style-type: none"> a) Specify the rail-specific noise model or numerical code used for the modelling e.g. Nordic Rail Prediction Method (Kilde 130-1984), Nord2000, Schall 03, UK Calculation of Railway Noise (CRN) or CNOSSUS algorithm – examples from the State Planning Policy 5.4 guidelines; b) Include the train speed used in the noise modelling; and c) Indicate that noise predictions are assumed to be applicable to 2046, compliant with the State Planning Policy 5.4 requirement for a 20-year planning horizon. <p>The amended acoustic report shall be to the satisfaction of the City of Subiaco and must be implemented at all times.</p> <p><u>Justification for Condition</u> To ensure the acoustic requirements are implemented in accordance with State Planning Policy 5.4 – Road and Rail Noise.</p> <ol style="list-style-type: none"> 2. Prior to occupation of the development, certification from a qualified acoustic consultant be submitted, confirming that the recommendations of the approved acoustic report prepared by 	<p>Main Roads support for the application is acknowledged.</p> <ol style="list-style-type: none"> 1. Standard DevelopmentWA condition is recommended to ensure that an updated Acoustic Attenuation/Vibration Report is provided 2. Standard DevelopmentWA condition is recommended the ensure that the recommendations of the Acoustic Attenuation/Vibration Report are included in the final design. 3. Standard DevelopmentWA condition is recommended to ensure notice is provided on the Certificate of Title.



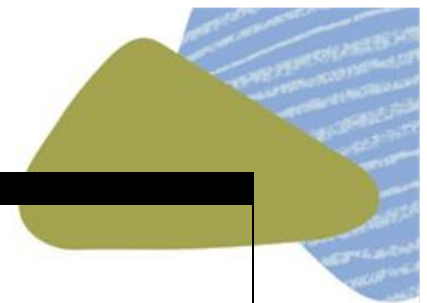
Stakeholder	Comments	Consideration
	<p>Herring Storer Acoustics have been implemented to the satisfaction of the City of Subiaco.</p> <p><u>Justification for Condition</u> To ensure the acoustic requirements are implemented in accordance with State Planning Policy 5.4 – Road and Rail Noise.</p> <p>3. A notification, pursuant to Section 70A of the Transfer of Land Act 1893 is to be placed on the certificates of the proposed lots. The notification is to state as follows: <i>'The lots are situated in the vicinity of a transport corridor and are currently affected, or may in future be affected by transport noise.'</i></p> <p><u>Justification for Condition</u> To ensure the acoustic requirements are implemented in accordance with State Planning Policy 5.4 – Road and Rail Noise.</p>	
<p>Public Transport Authority (PTA)</p>	<p>The Public Transport Authority is strongly supportive of the objectives of the development proposed, with the provision of well-located housing, and excellent accessibility to a high level of public transport services, noting the site's close proximity to West Leederville Station. Moreover, the Public Transport Authority is supportive of efforts to increase housing supply during this period of heightened housing demand, and in particular the delivery of housing at higher densities, infill development, and social and affordable housing.</p> <p>Notwithstanding, as can be expected with a development of the scale proposed, the Public Transport Authority has several potential points of concern regarding the potential for impacts of the development on the integrity of Public Transport Authority operations and infrastructure. Therefore, while the Public Transport Authority is supportive of the development, this support is contingent on the inclusion of the following conditions and supporting advice notes on Development WA's decision letter:</p> <p>Conditions:</p>	<p>PTA support for the development is noted.</p> <ol style="list-style-type: none"> 1. Condition recommended requiring that all PTA requirements are addressed with specific requirements captured within advice notes. Condition will be addressed in consultation with PTA to ensure necessary design modifications and clearances are obtained. 2. Condition recommended requiring that all PTA requirements are addressed with specific requirements captured within advice notes. Condition will be addressed in consultation with PTA to ensure

Stakeholder	Comments	Consideration
	<p><u>Interface with the Railway Corridor</u></p> <ol style="list-style-type: none"> 1. Prior to the application for a building permit, updated development plans being submitted to and approved by the Public Transport Authority, and thereafter implemented in full, demonstrating: <ol style="list-style-type: none"> a. All windows facing the rail corridor and located on floors with a floor height of no less than 2 metres above ground level being protected with a suitable device or screen which does not permit a 125mm sphere to pass through the opening or screen; and b. All openings, including balconies, facing the rail corridor and located on floors with a floor height of no less than 2 metres above ground level, being protected with screening of a minimum height of 2.4 metres so as to not permit a 125 mm sphere to pass through. <p>Justification for Condition: To prevent any objects from being dropped, thrown, or blown into the Public Transport Authority rail reserve, ensuring the safety and integrity of rail operations.</p> <ol style="list-style-type: none"> 2. Prior to the application for a building permit, engineering drawings and technical reports being submitted to and approved by the Public Transport Authority, and thereafter implemented in full, demonstrating: <ol style="list-style-type: none"> a. the compliance of the development with Public Transport Authority Specification <i>8880-450-083 Design for Rail Collisions</i>; and b. details of proposed crane operations, including exclusion zones, control measures, and engineering documentation, including demonstrating that tower cranes are not to be 	<p>necessary design modifications and clearances are obtained. Operation of Cranes is captured within the CMP Condition.</p> <ol style="list-style-type: none"> 3. Condition recommended requiring that all PTA requirements are addressed with specific requirements captured within advice notes. Condition will be addressed in consultation with PTA to ensure necessary design modifications and clearances are obtained. 4. Standard DevelopmentWA condition recommended requiring that a Construction Management Plan is prepared, in consultation with PTA. Comments from PTA are recommended to be included in the Advice Note to ensure that are captured in the CMP. 5. Recommended to be included as an Advice Note. 6. Standard DevelopmentWA condition relating to stormwater being retained on site is recommended. 7. Standard DevelopmentWA condition is recommended to ensure notice is provided on the Certificate of Title.

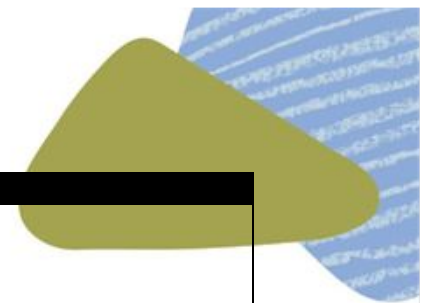
Stakeholder	Comments	Consideration
	<p>permitted to sail over the Public Transport Authority rail reserve, including as a result of free-slewing during periods of non-use.</p> <p>Justification for Condition:</p> <ul style="list-style-type: none"> a. To mitigate the consequences of a rail collision or derailment event on people and property by ensuring adjacent structures are designed to resist or safely absorb impact loads without catastrophic failure. b. To prevent safety, operational, and structural risks to the PTA Rail Reserve and associated assets resulting from the use of cranes, including hazards associated with electromagnetic induction and arcing from high voltage rail traction systems. <p><u>Construction Management</u></p> <ul style="list-style-type: none"> 3. Prior to the application for a building permit, the applicant/owner is to obtain approval from the Public Transport Authority for works in and around an operating railway corridor in accordance with the Public Transport Authority's Procedure 8810-450-003 – <i>Working In and Around the PTA Rail Corridor, Assets and Infrastructure</i>. All supporting documentation required to be submitted as part of this application is to be prepared and certified by a suitably qualified professional, in alignment with Public Transport Authority Specifications and relevant Australian Standards. <p>Justification for Condition: The proposed development is adjacent to an active passenger rail corridor and therefore within the Public Transport Authority's Asset Protection Zone. In accordance with PTA procedure, all works within the PTA's Asset Protection Zone are required to be approved by the Public Transport Authority to ensure the safety of railway and construction personnel, and to minimise the potential for service disruption.</p>	<p>8. Standard DevelopmentWA condition is recommended the ensure that the recommendations of the Acoustic Attenuation/Vibration Report are included in the final design.</p>



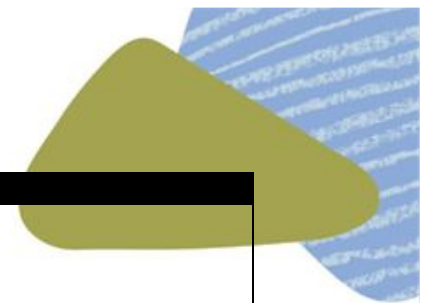
Stakeholder	Comments	Consideration
	<p>4. Prior to the application for a building permit, a Construction Interface Rail Safety Management Plan (CI-RSMP), specifically addressing the risks associated with constructing a multi-storey building in close proximity to an operating electrified rail corridor, is to be submitted and approved to the satisfaction of the Public Transport Authority. This plan is to thereafter be implemented in full. Refer advice note (b)</p> <p>Justification for Condition: Construction of a tall building immediately adjacent to the operating rail corridor poses elevated risks—including falling objects, wind-borne debris, and high-level construction activities—that are not fully addressed by a standard construction safety management plan. PTA Safeworking Rules for working near the rail corridor may apply in certain situations. A dedicated Construction Interface Rail Safety Management Plan ensures these unique height related risks are identified and appropriately controlled to protect rail operations, workers, and the public.</p> <p>5. All railway access tracks and gates are to remain unobstructed at all times during construction and occupancy of the development.</p> <p>Justification for Condition: The Public Transport Authority requires that railway access gates and tracks remain unobstructed at all times to ensure that maintenance and incident response operations are not inhibited by the development.</p> <p><u>Stormwater</u></p> <p>6. Stormwater from all roofed, paved and awning areas shall be collected and contained on site. Stormwater must not affect or be allowed to flow onto or into any other property or the adjoining railway reserve.</p> <p>Justification for Condition: Stormwater runoff represents a significant, but mitigable potential risk to the safety of railway infrastructure and operations.</p>	



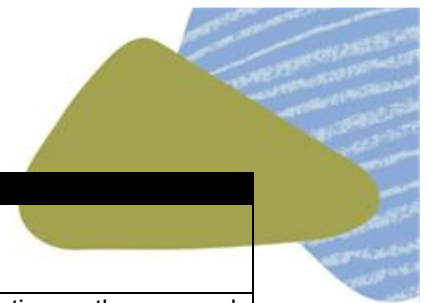
Stakeholder	Comments	Consideration
	<p>This condition ensures the development does not result in the runoff of stormwater into the railway corridor.</p> <p><u>Rail Noise</u></p> <p>7. Prior to the occupancy or use of the development, a notification, pursuant to Section 70A of the <i>Transfer of Land Act 1893</i> is to be placed on the Certificate(s) of Title of the lot(s) which are the subject of this development. Notice of this Notification is to be included on the diagram or plan of survey (Deposited Plan). The Notification is to state as follows:</p> <p><i>'This lot is in the vicinity of a transport corridor and is currently affected, or may in the future be affected, by road and rail noise and/or rail vibrations. Road and rail transport noise levels may rise or fall over time depending on the type and volume of traffic.'</i></p> <p>Justification for Condition: In accordance with State Planning Policy 5.4. The proposed development is located directly adjacent to a major railway corridor, including rail turnbacks and track switches. These turnbacks/switches can be required to be used without notice as a part of regular passenger operations, and use of these may result in significant rail noise impacts beyond that of standard line operations. Rail traffic on this corridor may also increase in the future without warning. Occupants should be made aware of the potential for rail noise and vibration impacts.</p> <p>8. Prior to the occupancy of the development, the recommendations outlined in Table 5-1 and Appendix B of the provided <i>SPP5.4 Noise Management Plan</i> (Prepared by Herring Storer Acoustics, reference 34788-3-25214, Version 3, dated 18 June 2025) being implemented in full at the cost of the landowner/applicant.</p> <p>Justification for Condition: As mentioned above, the subject site directly adjoins a major railway corridor. The Public Transport Authority has reviewed</p>	



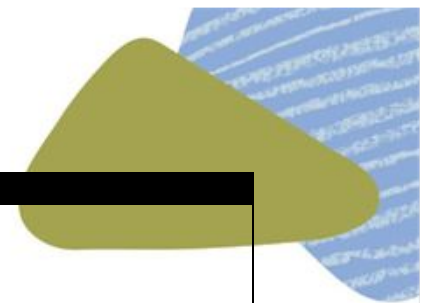
Stakeholder	Comments	Consideration
	<p>the abovementioned Noise Management Plan, and is satisfied that its full implementation would satisfy the requirements and/or objectives of State Planning Policy 5.4 – Road and Rail Noise.</p> <p><u>Advice:</u></p> <ul style="list-style-type: none"> a. With regards to Condition 3, this development is within 50 metres of the Public Transport Authority’s Railway Corridor and therefore lies within the Public Transport Authority’s Asset Protection Zone. To manage safety, operational continuity, and risk to critical infrastructure, works within this zone require Public Transport Authority assessment and approval in accordance with Public Transport Authority procedures. Information about this procedure and required documentation can be found in the Public Transport Authority’s Procedure <i>8810-450-003 – Working In and Around the Public Transport Authority Rail Corridor, Assets and Infrastructure</i>. For further information, and to submit an application, please email PTAThirdPartyCivil@pta.wa.gov.au. <p>Justification for Advice: As per Condition 3.</p> <ul style="list-style-type: none"> b. With regards to Condition 4, the Public Transport Authority advises that the Construction Interface Rail Safety Management Plan (CI-RSMP) should address the following matters: <ul style="list-style-type: none"> a. Controls for working at height adjacent to the rail reserve, including measures to prevent dropped objects, materials, tools, or debris entering the rail corridor. b. Management of façade construction, formwork systems, screens, and temporary edge protection for elevated works facing the rail reserve. 	



Stakeholder	Comments	Consideration
	<ul style="list-style-type: none"> c. Wind-related hazard management, including assessment of wind loading on materials, plant, and temporary works at height. d. Engineering controls, exclusion zones, monitoring requirements, and any additional controls that address construction activities occurring above the PTA Protection Zone. e. Procedures for rail-adjacent emergency response, including communication protocols with PTA Network Control and escalation requirements. <p>Justification for Advice: As per Condition 4.</p> <p>Should Development WA disagree with, or resolve not to include on its decision letter, any of the above conditions and/or advice notes, the Public Transport Authority requests to opportunity to meet to discuss the application further, prior to a final determination being made.</p>	
<p>Department of Planning, Lands and Heritage (DPLH)</p>	<p>It is understood that the subject site is located within the Subiaco East Project Area under the SRS 2. It is also acknowledged that the subject site is located approximately 60m south of the West Leederville Precinct Structure Plan (PSP) boundary, however, there are limited to no implications arising from the proposal on the PSP and the PSP is unlikely to affect the proposal. As such, no advice from our team is necessary</p> <p>After reviewing the proposal I can confirm that the Metro Central North team has no comments to provide regarding the development.</p> <p>For your reference, the Statutory Planning Committee (SPC) meeting minutes, including the WAPC's recommended modifications to the West Leederville PSP, are available here. The Town of Cambridge updated the</p>	<p>Comments from DPLH are noted.</p>



Stakeholder	Comments	Consideration
	<p>PSP which was adopted at the 10 December 2025 SPC meeting. The WAPC granted final approval on 2 February 2026.</p>	
<p>Perth Airport</p>	<p><u>Perth Airport position</u> No objection</p> <p><u>Comments</u> <i>Building Heights</i> Perth Airport has assessed the proposal against the Perth Airport airspace protected under the Airports (Protection of Airspace) Regulations 1996 (C'th). The provided plans indicate that the proposed development will not exceed 97m AHD (based on a finished ground floor level of 30.8m AHD and building elevation up to 64.2m AGL and a small allowance for ancillary structures located on the roof). At this elevation the proposed development will not infringe Perth Airport's current of future airspace or windshear surfaces .</p> <p>This position of no objection is on the basis that Perth Airport considers the maximum elevation of the structure to be the absolute highest point (top of monopole, antennae, facade etc.) and not just the highest point of the roof.</p> <p><i>Crane Use</i> Cranes used in the construction are also a height consideration. Cranes are assessed separately, closer to construction, when accurate information regarding crane operating heights and locations is available. A separate assessment of cranes involved in construction will likely be required from a number of authorities, including Perth Airport, Airservices Australia, CASA and potentially the Commonwealth Department of Infrastructure, Transport, Regional Development, Communications, Sport and the Arts. Information regarding the Perth Airport crane assessment and permit process is available on our website perthairport.com.au.</p> <p>Perth Airport has assessed the proposal following conditions</p>	<p>Perth Airport no objection on the proposal is noted.</p> <p>Comments on building heights and use of cranes are noted and will be addressed through the recommended conditions and advice notes.</p> <ol style="list-style-type: none"> 1. Condition supported 2. Standard DevelopmentWA conditions and advice notes are recommended to ensure that the proposed development does not unduly impact on Perth Airport's Communication, Surveillance and Navigation Facilities



Stakeholder	Comments	Consideration
	<p><u>Condition 1:</u> The development, including the ancillary structures such as roof-based plant or antenna, shall not exceed a maximum elevation of 97m AHD.</p> <p><u>Condition 2:</u> the applicant or responsible contractor shall lodge an online application to Perth Airport's Protected Airspace Assessment Tool (PAAT) prior to the erection of a crane on the subject site during both the construction and operation of the subject site in accordance with the obligations under the <i>Airports (Protection of Airspace) Regulations 1996 (C'th)</i></p> <p><i>Advice i :</i> In relation to Condition 2, applications are to be made online at https://portal.perthairport.com.au/ . Queries can be directed to Perth Airport's airspace line or inbox at 6278 8122 or airspace@perthairport.com.au.</p>	
Western Power	No comments received	-
Water Corporation	No comments received	-

Summary of Public Comments

Table 1: Key themes and responses

No.	Theme	Response
1	<p><u>Traffic</u></p> <p>The development will increase congestion on already congested roads, especially during peak hours such as school pick-up and drop-off.</p>	<p>The site is well-serviced by public transport and established pedestrian and cycling networks. Convenient access to these encourages walking and cycling, reducing reliance on private vehicles and, in turn, the demand for on-site parking.</p> <p>A Transport Impact Assessment has been submitted in support of the proposed development which concludes that the traffic generated will be relatively low and is unlikely to have any major impact to the surrounding land uses including residential dwellings and local schools.</p>
2	<p><u>Parking</u></p> <p>Parking provisions is too low to meet the expected demand generated from 447 dwellings</p> <p>Concerns that street parking will not be available during key times such as school pick-up and drop-off.</p>	<p>The proposed development results in a shortfall of residential and visitor parking, however the site is located within a highly accessible, transit-oriented area. Convenient access to a nearby train station, high-frequency bus services, and a well-established pedestrian and cycling network is expected to reduce reliance on private vehicles, thereby mitigating demand for on-site parking.</p> <p>Parking bay allocation and management will be addressed via conditions of approval and managed by the Community Housing Provider.</p>
3	<p><u>Height</u></p> <p>18 storeys is excessive and inconsistent with the existing character.</p> <p>Height should be located away from Bob Hawke College and existing residential homes.</p>	<p>The height of the development is considered appropriate and generally in keeping with the outcome envisaged by the Design Guidelines. The Design Guidelines allow for heights of 11 to 18-storeys to serve as a landmark for the area and facilitate increased residential density within close proximity to high-frequency public transport and local amenities. The proposal is a high quality architecturally designed development which has been supported by the Design Review Panel.</p>
4	<p><u>Impact on nearby schools</u></p>	<p>The proposal has been designed to appropriately consider and manage amenity impacts to Bob Hawk College, with particular regard to overlooking and student privacy. A deeper street setback to Tower 4 has been incorporated along Subiaco Road, significantly increasing separation between the development and school</p>

No.	Theme	Response
	<p>Concern regarding overlooking of school grounds resulting from balconies oriented towards the school.</p> <p>Increased traffic will create safety issues for students walking to and from school.</p> <p>Land uses of commercial tenancies may be incompatible with the school.</p> <p>Concern regarding construction impacts of the project on the school safety and operations.</p> <p>Existing schools in the area are already at capacity which will be exacerbated by increased density.</p>	<p>grounds. In addition, the building envelope has been carefully orientated to control building extent near the school interface, further limiting potential overlooking from balconies and upper-level dwellings.</p> <p>Dwellings are primarily oriented east, north and west minimising actual and perceived overlooking while remaining aligned with <i>DPLH Operational Policy 2.4 - Planning for School Sites</i> which identifies some incidental surveillance from surrounding development as an intended design outcome for school sites to support safety outcomes.</p> <p>The proposed reduced parking provision and highly accessible, transit-oriented location are expected to limit traffic generation and reduce reliance on private vehicles, as out lines in the Traffic Impact Assessment, a significant proportion of trips are anticipated to be made by walking and public transport, minimising impacts on surrounding streets and maintaining a safe environment for students travelling.</p> <p>The proposal includes two small non-residential tenancies to activate Court Place and provide café/retail offerings to benefit the local community. The tenants will be selected and managed by the CHP and the uses are expected to be low intensity and compatible with the surrounding context, including the adjoining school. The applicable Design Guidelines limit the types of uses that are possible in proximity to the school and require any proposed non-preferred uses to be advertised to surrounding landowners and for evidence of compatibility to be submitted.</p> <p>Impact on students and the school itself will be carefully managed through construction, including heavy vehicle movement, access to on-street parking during peak school times and impacts from noise and dust. Conditions are recommended to ensure all works are undertaken in consultation with DevelopmentWA, City of Subiaco and Department of Education.</p> <p>The proposed development is in keeping with planned densities for the area under the approved Subiaco East Master Plan and Design Guidelines.</p>

No.	Theme	Response
5	<p><u>Amenity</u></p> <p>The development will overshadow existing homes, streets and open spaces.</p> <p>The light and noise impacts from commercial tenancies and residential entry lobby on Court Place will impact existing residents on the street.</p>	<p>The proposal was subject to a Design Review Panel process, chaired by the Government Architect of WA, which carefully considered the potential impacts from overshadowing, noise and light on the public realm and surrounding land uses, confirming that a Design Excellence outcome will be delivered.</p> <p>The design reduces overshadowing by approximately 28% when compared to a design that is strictly in accordance with the Design Guidelines, through reduced podium heights and slender tower design. Overshadowing is predominantly limited to community infrastructure at Subiaco Oval.</p> <p>Noise impacts associated with the commercial and communal areas are not anticipated to be unreasonable and will be managed through design measures and implementation of an Operational Management Plan which will include procedures for managing tenant behaviour and responding to any amenity issues should they arise.</p>
6	<p><u>Social Housing</u></p> <p><i>Opposed</i> The previous community housing development on the site created issues with crime and anti-social behaviour that will return as a result of this development.</p> <p><i>Supportive</i> Urgent need for social and affordable housing</p>	<p>A shortage of Affordable and Social Housing has been identified by Federal and State Governments.</p> <p>The subject site is well-located being serviced by high quality public transport, the cycling network and pedestrian connectivity. The proposal will be managed by a Community Housing Provider (CHP). The Community Housing Provider will provide ongoing onsite management to reduce any risk of antisocial behaviour or amenity issues arising, and to ensure any matters are identified and addressed promptly</p> <p>Tenants are required to meet the same legal responsibilities as any private renter under the Residential Tenancies Act, this includes paying rent, maintaining the property, and being respectful of neighbours.</p> <p>The development will provide security systems, including a CCTV system along with lightning control measures to ensure that only authorised individuals can access certain areas.</p>

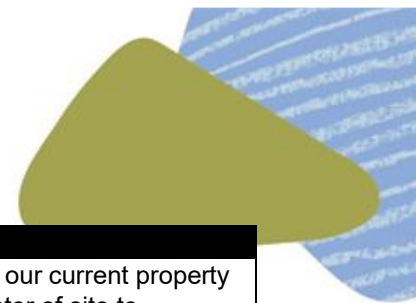


Table 2: Public Comments Received

No.	Address	Comment
1	Subiaco Road, Subiaco	No consideration given to existing residents on adjoining streets. Drawings indication that our current property is "Future Development" with no indication what that means. Taller tower should be in center of site to minimize impact to adjoining properties. Having a huge tower overlooking a high school makes no logical sense.
2	Tate St, West Leederville	<p>I object to the proposed development at Lot 1, 154 Subiaco Road, Subiaco. The proposed scale, density, and height of up to 18 storeys appear inconsistent with the existing built form and amenity of the locality and raise concerns under State Planning Policy 7.0, which requires development to respond positively to local context and protect surrounding amenity.</p> <p>The intensity of 447 dwellings on a single site will significantly increase traffic generation and pedestrian movements in an area already experiencing high demand due to the immediate proximity of Bob Hawke College and nearby Perth Modern School. This raises concerns regarding pedestrian safety, access, and whether the proposal appropriately addresses movement networks as required under State Planning Policy 7.0.</p> <p>Furthermore, the scale of the development raises questions as to whether local infrastructure and services can adequately accommodate the additional population, consistent with the objectives of the City of Subiaco Local Planning Scheme and orderly and proper planning principles under the Planning and Development Act 2005. I respectfully request that the responsible authority carefully assess whether the proposal appropriately satisfies State and Local Planning Policy, particularly in relation to built form, infrastructure capacity, and protection of local amenity.</p>
3	Unknown, Doubleview	The development seems to consider overshadowing and building separation very well and has a far better utilisation of the ground plane than what was initially set out in the Oval Master Plan. My only reservation is that the aesthetics seem to have been downgraded from what we saw in initial renders that the State released when they made their announcement about pursuing a project at this site. The aesthetics looked pretty fancy and now they are reminiscent of 1960s flats. The material treatments can probably accommodate greater detail and depth which would really improve the development. Overall though I still am very supportive.
4	Flinders Street, Yokine	I'm concerned about social housing. This area was previously social housing and it caused a lot of headache for residents living in the area. But this new proposal does improve the streetscape and could potentially add value.

No.	Address	Comment
5	Subiaco Road, Subiaco	While the project seems overall well constructed, I am a bit worried about the amount of parking space available. The current rate is of 0.63 parking spot per apartment. While the train station and ease of access to public transport may increase the likelihood of some family units to not have a car, I struggle to imagine that this will be true for a third of them. This will in return overcrowd the street parking on Subiaco Road and around, which combined with school drop off and pick up twice a day will likely render the circulation a lot more difficult. Is there any plan to adjust the number of parking spot?
6	Railway Pde, West Leederville	I have been broken into 3 times in 2 years, every offender lived nearby in State Housing. My wife and I live in fear that another break-in will happen, this proposed development will be the end of us.
7	Cambridge St, West Leederville	<p>Traffic Impacts</p> <p>The proposed high-density development will significantly worsen traffic conditions on the already congested surrounding roads, including Railway Parade, Cambridge Street, Haydn Bunton Drive and Roberts Road. These roads are currently under considerable pressure, particularly during school times and peak-hour periods.</p> <p>The proposal does not include any plans to create new access roads or upgrade existing road infrastructure to accommodate the increased traffic volume. Additionally, the number of parking bays provided is insufficient for a development of this scale, which will inevitably push overflow parking into neighbouring streets and further strain local traffic flow.</p>
8	Subiaco Road, Subiaco	<p>The Developments Plan do not cover in detail the following -</p> <p>1) Local area security monitoring - a local area security plan should be included and dovetailed with that of Bob Hawke College and the West Leederville train station</p> <p>2) Public Conveniences - for visitors to the development, the provision of public conveniences is unclear. While the oval in front has public conveniences, these are opened at particular times and therefore not 24 hours. Consideration is requested therefore to include specifications for PC to be within the Development boundary and to be accessible 24/7 for use by guest and commuters at the West Leederville train station, where there are no PC's. For noting there is a single PC in Mueller Park which is overused and requires daily cleaning at the expense of the Subiaco City Council.</p>
9	Blencowe St, West Leederville	The North-facing views of all apartments is looking over an ugly 30m wide Railway trench . There is a lot of Noise from passing trains. Sink the railway, as they have done from the Haydn Bunton Bridge to the Subiaco Station. This area is aesthetically very pleasing and provides an open green area with trees. Why cant the Subi-East Devt have the same? This is the question future generations will ask
10	NA	Support this development, will allow much needed social housing for Perth

No.	Address	Comment
11	Subiaco Road, Subiaco	<p>East Subiaco a once residential area has already been subjected to massive destruction and redevelopment. This area already has two massive high rises being built and now the government is proposing four more buildings. This strategy of saturating every last available square inch of land in this area to solve a public housing shortage should not target certain areas on mass and leave other more 'leafy' areas without over building and over crowding.</p> <p>My family have owned and lived on this property for well over 100 yrs , not only does Subiaco not have the facilities to provide schooling etc to the outrageous number of units and apartments planned for Subiaco oval , you now want to flood the area with more apartments putting extreme pressure on the area's facilities. These proposals are turning a Suburban area into Inner city living which is not what residents owning homes in the area want. Local residents have paid hard earned money to live in areas that suit their families needs.</p> <p>Development WA is making decisions solely on the governments inability to provide affordable housing. This does not give the government the power to overdevelop targeted areas, completely ignoring the home owners rights to live in a peaceful area that we pay an extremely high price to live in. Over crowding and bringing in a large number of social housing is proved all over the world to bring prices down, bring crime and other social issues to the area. Bob Hawke College and Shelton College are both at capacity. It is well known that people from lower socioeconomic backgrounds rent cheap housing in the area, get their children enrolled and then move out of the area once the enrolment is secured. Where does the Government propose all 1000's of people of which I imagine many will have children are going to go to school? All the local schools are at capacity, the area simply does not have the infrastructure to accommodate these massive numbers of people you are proposing to live in the area. Parking will become problematic and the area will be noisy , more crime etc and once again ghettos will be end result with home owners selling due to the area becoming overcrowded and once the government realises they can not maintain the parking, rubbish collection, road maintenance etc etc that will all increase massively and with high use , restoration etc will be constantly required. The abundance of high rises in this area is completely unfair and unsustainable. I and my family could not be more against this proposal which is not only going to change the long term character of the area but you are going to put residents through the very invasive construction of these buildings which completely affects our daily living. I have had to live through the demonashipment of Subiaco oval, trucks raking out rubble night after night after night for months. I then had Bob hawke being built across the road for approximately 5 yrs in total . The impacting of the ground has structurally done immense damage to this Federation home, dust constantly flooding into our home and cars constantly covered with dust day after day, I've had my gas line cut through, had my carport blocked for weeks not to mention work on weekends, and frequently at night on construction by the building company. It is quite simply completely unacceptable to expect residents to live through these conditions again, especially as no compensation was or has been offered to residents for having their daily lives become barely tolerable with the constant noise from building materials being delivered not to mention</p>

No.	Address	Comment
		<p>the tradies in driving in and out in the early hours of the morning revving up their engines of their hotted up 4 wheel drives as they find a park. Quite simply we will not stay in the area as I refuse to live through any more building near my home. Will be requesting compensation to find the same type of home in Subiaco at the governments cost as compensation for the impact on our property and standard of living. I already now of people selling their homes solely on the impact this build will have on their lives, it is simply unfair to expect the people living in the areas you have chosen to place high rises suffer again and again, we have has our lives impacted enough. It has just quietened down after years of noise , dust mess, loss of any peace and calm and now you are going to do it all again. The buildings will impact our shade on our homes which effects our heating, light in our houses, wild life etc etc. I am so upset that the Government has not given any thought to how these decisions impact residents and I will be doing all I can to stop unfair and impractical developments continuing in the State.</p>
12	Tate Street, West Leederville	<p>I have significant concerns regarding the development associated with Application DA2600006 situated on Subiaco Road, Subiaco. The scale and density for such a site is significant given the proximity to private housing and Bob Hawke College.</p> <p>Being 98 parking bays short is a significant number and once again bringing out the reasoning that being close to public transport will mitigate this. The reality is that the car bays will be needed by residents as Perth is a car oriented city, public transport is great but people moving will have a vehicle to be parked.</p> <p>The discussion centres around the close secondary schools, however no mention of primary schooling has been made. Both West Leederville and Subiaco primary schools are basically full and do not have the infrastructure to take many more students. Again, the expectation is that some residents will have young children who will need schooling. How will such an issue be dealt with, or is this something else that the presumption will be that there will be alot of space in these schools. The development process will not deal with the tenant demographic so knowledge of needs cannot be known.</p> <p>Another issue will be the building process and the covering of neighbourhood damage to buildings. There was damage to homes during the Subiaco Oval demolition/ground work. Nearby residents will need to be a consideration within the proposal.</p> <p>At all costs, the application of such a large construction must make the close proximity to a school a priority. High traffic loads already exist in drop off and pickup times. Towering over the school and proximity to school windows creates further issues, as does the highrise across the Subiaco Oval. The need for housing is well known but the situation and surrounding area is vital to such builds and needs to be sympathetic to these.</p>

No.	Address	Comment
13	Subiaco Road, Subiaco	While I am supportive of some aspects of the development I object to Parking and Traffic Impact resolutions for the reasons outlined in the attached document. As a longtime resident of Subiaco Rd I can advised the report contains inaccuracies regarding impact which need to be corrected,
14	Court Place, Subiaco	<p>All re Tower 4 /Court Place - Lobby/entrance is directly opposite private residents. Please consider high volume noise/traffic in and out of doors, intercom system, walkways, stairwells etc. Wouldn't access be more suitable on Subiaco Rd where there are no direct residents?</p> <p>Lighting - LED street lighting is already an issue. Residents do not want to live next to a glowing tower/street - please be smart about this please.</p> <p>Overshadowing Court Place - there is no overshadowing plan for Court Place. This WILL affect us. Please consider MORE and EARLY planting of MATURE trees as some sort of buffer at least please.</p> <p>Construction - 2 years to build 4 towers? It took almost 6 months to pave 40-50 meters of Court Place. Please be transparent and honest about construction times.</p>
15	Subiaco Road, Subiaco	The plans for an 18 storey are just too much. The area deserves something that works into the surrounding area and doesn't block the sun for the current residents.
16	Blencowe Street, West Leederville	I am worried how much extra traffic will be generated by this development on surrounding roads such as Railway Parade in West Leederville. This road is already massively congested at am and pm peak times. This also creates rat runs thru adjoining residential streets such as ours. Add this to the One Oval Subi East development with over 400+ car bays which will also cause additional congestion on the aforementioned roads. Railway Parade has no potential for geometric redesign and upgrading due to existing residential and commercial properties on the north side and the railway reserve on the south side. Can someone please explain why this is not considered an issue for our concern? Thank you.
17	Greenock Ave, Como	Modest set of apartments against taller private development in the area. Buildings have a nice built form (spread out) that allows sunlight and air to pass through. Would like to see and support conditions on tree retention. I support the high quality design for social/affordable housing. Defensible Space / Crime Prevention Through Environmental Design is important and giving residents defined shared spaces gives a better sense of ownership of your surroundings. Fully support- looks amazing
18	Court Place, Subiaco	<p>My concerns are:</p> <ul style="list-style-type: none"> •Is the entrance on Court Place the best location? Would it be more appropriate for the entrance to be on Subiaco Road?

No.	Address	Comment
		<ul style="list-style-type: none"> •Will trees be planted on Court Place, or will some form of landscaping be undertaken to minimise the sterile appearance of the tower? •Lighting on Court Place can be excessively bright and may impact residents opposite. •Could cleaning vouchers be provided during the construction period, rather than only at the end of the build? •Will work hours be strictly controlled to ensure that noise.
19	Roberts Road, Subiaco	<p>Dear Members of DevelopmentWA, Mr Andrews and Mr Garvey,</p> <p>I write to formally object to the proposed development at Lot 1 154 Subiaco Road Subiaco WA 6008 located behind Bob Hawke School and Subiaco Football Oval.</p> <p>Firstly, I am pretty certain that this development has not been informed to the common residents of Subiaco. I have only come to know from a friend. I cannot imagine that such a crucial matter could be so hastily progressed. It's not fair at all. This precinct is an established low-density residential area with a defined heritage character and limited supporting infrastructure. The proposal represents a significant intensification of land use that is inconsistent with the existing planning intent for the suburb.</p> <p>Of particular concern are safety impacts. The site is already subject to heavy congestion during school drop-off and pick-up times, as well as major sporting events. Increased occupancy will likely exacerbate:</p> <ul style="list-style-type: none"> - Traffic congestion and unsafe turning movements - Pedestrian risk to school children - Parking overflow into residential streets - Reduced passive surveillance and neighbourhood amenity - ?safety concerns for the surrounding residents <p>The cumulative pressure on roads, footpaths, and local infrastructure has not been adequately addressed. This location is not supported by the transport capacity or strategic planning framework necessary for higher-density or specialised accommodation.</p> <p>Further, there is no mention of increased police presence or security for the residents in the area. We do not want this to be another East Perth.</p>

No.	Address	Comment
		<p>There has not been sufficient time allowed for public consultation to allow the community members to raise their concerns and it appears this project has been proposed by the WA Government in haste, with purely political motives of LabourWA.</p> <p>For these reasons, I respectfully but sternly object for this development to proceed as a long standing, community member, homeowner and resident of Subiaco.</p> <p>I do hope my concerns are genuinely addressed by the relevant members and do raise / make this application not alone but echo the sentiments of everyone living near Subiaco Road and Roberts Road.</p>
20	Subiaco Road, Subiaco	<p>Dear Members of DevelopmentWA, Mr Andrews and Mr Garvey,</p> <p>I write to formally object to the proposed development at Lot 1 154 Subiaco Road Subiaco WA 6008 located behind Bob Hawke School and Subiaco Football Oval.</p> <p>Firstly, I am pretty certain that this development has not been informed to the common residents of Subiaco. I have only come to know from a friend. I cannot imagine that such a crucial matter could be so hastily progressed. It's not fair at all. This precinct is an established low-density residential area with a defined heritage character and limited supporting infrastructure. The proposal represents a significant intensification of land use that is inconsistent with the existing planning intent for the suburb.</p> <p>Of particular concern are safety impacts. The site is already subject to heavy congestion during school drop-off and pick-up times, as well as major sporting events. Increased occupancy will likely exacerbate:</p> <ul style="list-style-type: none"> - Traffic congestion and unsafe turning movements - Pedestrian risk to school children - Parking overflow into residential streets - Reduced passive surveillance and neighbourhood amenity - ?safety concerns for the surrounding residents <p>The cumulative pressure on roads, footpaths, and local infrastructure has not been adequately addressed. This location is not supported by the transport capacity or strategic planning framework necessary for higher-density or specialised accommodation.</p>

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		<p>Further, there is no mention of increased police presence or security for the residents in the area. We do not want this to be another East Perth.</p> <p>There has not been sufficient time allowed for public consultation to allow the community members to raise their concerns and it appears this project has been proposed by the WA Government in haste, with purely political motives of LabourWA.</p> <p>For these reasons, I respectfully but sternly object for this development to proceed as a long standing, community member, homeowner and resident of Subiaco.</p> <p>I do hope my concerns are genuinely addressed by the relevant members and do raise / make this application not alone but echo the sentiments of everyone living near Subiaco Road and Roberts Road.</p>
21	Subiaco Road, Subiaco	<p>Dear Members of DevelopmentWA, Mr Andrews and Mr Garvey,</p> <p>I write to formally object to the proposed development at Lot 1 154 Subiaco Road Subiaco WA 6008 located behind Bob Hawke School and Subiaco Football Oval.</p> <p>Firstly, I am pretty certain that this development has not been informed to the common residents of Subiaco. I have only come to know from a friend. I cannot imagine that such a crucial matter could be so hastily progressed. It's not fair at all. This precinct is an established low-density residential area with a defined heritage character and limited supporting infrastructure. The proposal represents a significant intensification of land use that is inconsistent with the existing planning intent for the suburb.</p> <p>Of particular concern are safety impacts. The site is already subject to heavy congestion during school drop-off and pick-up times, as well as major sporting events. Increased occupancy will likely exacerbate:</p> <ul style="list-style-type: none"> - Traffic congestion and unsafe turning movements - Pedestrian risk to school children - Parking overflow into residential streets - Reduced passive surveillance and neighbourhood amenity - ?safety concerns for the surrounding residents

No.	Address	Comment
		<p>The cumulative pressure on roads, footpaths, and local infrastructure has not been adequately addressed. This location is not supported by the transport capacity or strategic planning framework necessary for higher-density or specialised accommodation.</p> <p>Further, there is no mention of increased police presence or security for the residents in the area. We do not want this to be another East Perth.</p> <p>There has not been sufficient time allowed for public consultation to allow the community members to raise their concerns and it appears this project has been proposed by the WA Government in haste, with purely political motives of LabourWA.</p> <p>For these reasons, I respectfully but sternly object for this development to proceed as a long standing, community member, homeowner and resident of Subiaco.</p> <p>I do hope my concerns are genuinely addressed by the relevant members and do raise / make this application not alone but echo the sentiments of everyone living near Subiaco Road and Roberts Road.</p>
22	Railway Parade, West Leederville	<p>Parking is seriously underrated in this area. Where are the workers to park and then where the residents park? Does not seem to have much parking and the area is already extremely busy. Are the schools in the area able to expand to meet a bigger demand? Already there is a problem with the local schools being too small. The infrastructure in the area is old and the streets already narrow so I see problems with sheer numbers of people using it.</p>
23	Subiaco Road, Subiaco	<p>I have lived in Subiaco Rd for 10 years. We had a terrible time when less than 100 housing commission units were in this block. During that time we had drug addicts in the park, walking down our driveway (we installed a gate to stop This) people abusing walkers going to the train station. The units were knocked down the school was built and peace came to our neighborhood that you wish to make this a HUGE complex and turn our quiet street is ridiculous. The block where princess Margaret hospital has been deemed mixed use. This should be mixed with 10% low income. Have you not read reports that people can become who they see. If you build a low project people won't get out of their situation. You have seen high rises the world over fail and become projects and yet you want to bring that to Subiaco. High rise low income simply hasn't worked this is not a fix. And to bring it next to a high school instead of building family homes for school children. Why not build a</p>

No.	Address	Comment
		village for single mums and their kids and that I could get on board with but you are building single units. Single units right next to a school. Shameful.
24	Halesworth St, Jolimont	<p>I tried getting onto the site listed on the letter sent out to local landowners, dated 17 February 2026, but as you can see from the attached response, "Access Denied". I tried several times and information on the QR code was too small for me to see property</p> <p>I am not in favour of this development for the following reasons:</p> <ol style="list-style-type: none"> 1. Too close to Bob Hawke College with possible problems of overlooking children outside. 2. Planned towers are too high. 3. People will not necessarily use public transport and the streets around this area are already overloaded. Check out Roberts Rd and Railway Parade, the latter especially from 4pm onwards. 4. The development will remove a well utilised carpark. 5. The loss of a large amount of public open space.
25	Roberts Road, Subiaco	<ul style="list-style-type: none"> • building interface, including the orientation and design of built form to minimise overlooking into school grounds and to appropriately respond to the proximity of Bob Hawke College; • land use compatibility, particularly in relation to ground floor commercial uses and their operation in a school-adjacent environment; and • traffic and parking impacts, including the need to ensure that student safety and peak school movement periods are appropriately accommodated and not adversely affected by the development.



Appendix 6

Recommended Conditions and Advice Notes

RECOMMENDED CONDITIONS:

- The development is to be undertaken in accordance with the approved plans and documents attached to this approval, details of which are to be provided at planning conditions clearance (working drawings) stage, to the satisfaction of DevelopmentWA. The approved plans and documents of development are listed as follows:

Plan / Document Name	Ref.	Date Received
Cover Page	DA100	8 April 2026
Drawing List	DA001	8 April 2026
Existing Site Plan	DA100	8 April 2026
Demolition Plan	DA101	8 April 2026
Proposed Site Plan	DA102	8 April 2026
Overall – Lower Ground Plan	DA200	8 April 2026
Overall – Upper Ground Plan	DA201	8 April 2026
Overall – Level 01	DA202	8 April 2026
Overall – Level 02/Podium	DA203	8 April 2026
Overall – Level 03-06 / Typical Plates	DA204	8 April 2026
Overall – Level 07 / T1 Roof Services	DA205	8 April 2026
Overall – Level 11 / T3 Roof Services	DA206	8 April 2026
Overall – Level 15 / T2 Roof Services	DA207	8 April 2026
Overall – T4 Roof Services	DA208	8 April 2026
T1 & T2 – Upper Ground Plan	DA210	8 April 2026
T1 & T2 – Level 01	DA211	8 April 2026
T1 & T2 – Top of Podium	DA212	8 April 2026
T1 & T2 – Typical Tower	DA213	8 April 2026
T1 – Roof Services Plan	DA214	8 April 2026
T2 – Roof Services Plan	DA215	8 April 2026
T3 & T4 – Lower Ground Plan	DA220	8 April 2026
T3 & T4 – Upper Ground Plan	DA221	8 April 2026
T3 & T4 – Level 01	DA222	8 April 2026
T3 & T4 – Top of Podium	DA223	8 April 2026
T3 & T4 – Typical Tower	DA224	8 April 2026
T3 - Roof Services Plan	DA225	8 April 2026
T4 - Roof Services Plan	DA226	8 April 2026
South Elevation	DA300	8 April 2026
North Elevation	DA301	8 April 2026
T1 & T2 – South Elevation	DA310	8 April 2026
T3 & T4 – South Elevation	DA311	8 April 2026
T3 & T4 – North Elevation	DA312	8 April 2026
T1 & T2 – North Elevation	DA313	8 April 2026
T1 – West & East Elevation	DA314	8 April 2026
T2 – West & East Elevation	DA315	8 April 2026
T3 – West & East Elevation	DA316	8 April 2026
T4 – West & East Elevation	DA317	8 April 2026
Section A-A	DA320	8 April 2026
Section B-B & Section C-C	DA321	8 April 2026



Landscaping Plan	-	8 April 2026
Sustainable Design Report	-	2 February 2026
Transport Impact Assessment	-	2 February 2026
Waste Management Plan	-	2 February 2026
Noise Management Plan	-	2 February 2026
Pedestrian Wind Assessment	-	2 February 2026
Accessibility Report	-	2 February 2026

2. Elevations and specification detailing high quality, durable materials, finishes and colours for the development, including “colour swatches” or material samples, are to be provided prior to planning condition clearance (working drawings) stage to the satisfaction of DevelopmentWA and the Design Review Panel. (Refer to Advice Note b)
3. A Construction Management Plan is to be provided at planning condition clearance (working drawings) stage and adhered to for the duration of construction to the satisfaction of DevelopmentWA, in consultation with the City of Subiaco, Bob Hawke College and the Public Transport Authority. (Refer Advice Note c)
4. A Dilapidation Survey of the footpaths, kerbs, roads, buildings and open space areas within the immediate locality surrounding the site is to be provided at planning condition clearance (working drawings) stage to the satisfaction of DevelopmentWA, in consultation with the City of Subiaco. Any damage caused to the footpaths, kerbs, roads, drainage infrastructure and open space areas adjacent to the site is to be made good at the applicant's expense prior to occupation of the development, to the satisfaction of DevelopmentWA, in consultation with the City of Subiaco.
5. The final design is to address Public Transport Authority specifications for constructing development adjacent to rail infrastructure, details of which are to be provided at planning condition clearance (working drawings) stage to the satisfaction of DevelopmentWA, in consultation with the Public Transport Authority. (Refer to Advice Note d)
6. A Parking, Service and Delivery Management Plan demonstrating management and allocation of car parking bays (including visitor bays), in accordance with the approved plans, is to be provided at planning conditions clearance (working drawings) stage and implemented thereafter to the satisfaction of DevelopmentWA. (Refer Advice Note e)
7. A Stormwater Management Plan is to be submitted at planning conditions clearance (working drawings) stage to the satisfaction of DevelopmentWA, in consultation with the City of Subiaco. (Refer Advice Note f)
8. A final Waste Management Plan is to be provided at planning condition clearance (working drawings) stage and implemented thereafter to the satisfaction of DevelopmentWA. (Refer Advice Note g)
9. A final Wind Assessment Report is to be provided at planning condition clearance (working drawings) stage to the satisfaction of DevelopmentWA. (Refer Advice Note h)
10. An independent Crime Prevention Through Environmental Design (CPTED) Report, prepared by a suitably qualified person, confirming the design of the development is in accordance with CPTED principles, is to be submitted at planning condition



clearance (working drawings) stage and implemented thereafter to the satisfaction of DevelopmentWA and the Design Review Panel. (Refer Advice Note i)

11. A final Landscape Plan detailing both 'soft' and 'hard' landscaping elements for the development, is to be provided at planning condition clearance (working drawings) stage, with all landscaping to be installed and maintained in accordance with the Landscape Plan thereafter to the satisfaction of DevelopmentWA. (Refer Advice Note j)
12. A Lighting Strategy, detailing how the lighting of the building exterior and ground floor public areas enhances building features, amenity and security, is to be submitted at planning condition clearance (working drawings) stage, and implemented thereafter to the satisfaction of DevelopmentWA. (Refer Advice Note k)
13. An Operational Management Plan is to be submitted prior to occupation of the development and implemented thereafter to the satisfaction of DevelopmentWA. (Refer to Advice Note l)
14. A Universal Access Statement, prepared by a suitably qualified professional, is to be provided at planning condition clearance (working drawings) stage to the satisfaction of DevelopmentWA. (Refer Advice Note m)
15. A final Sustainable Design Assessment Report demonstrating the proposal has been designed to achieve a minimum 5 Star Green Star rating or equivalency in accordance with Development Policy 1 – Green Building is to be submitted at planning conditions clearance (working drawings) stage to the satisfaction of DevelopmentWA. (Refer Advice Note n)
16. A report detailing certification or evidence that the building has achieved the required 5 Star Green Star rating or equivalency, is to be submitted following practical completion, to the satisfaction of DevelopmentWA.
17. A final Acoustic and Vibration Attenuation Report and certificate from a qualified acoustic consultant, confirming that the design and construction of the building will achieve an appropriate level of sound and vibration attenuation in accordance with Development Policy 3 – Sound and Vibration Attenuation is to be provided at planning conditions clearance (working drawings) stage, to the satisfaction of DevelopmentWA. (Refer Advice Note o)
18. A report and certification from a qualified acoustic consultant are to be submitted at practical completion stage and prior to occupation of the building, confirming that all recommendations of the Acoustic and Vibration Report integral to achieving compliance with Development Policy 3 – Sound and Vibration Attenuation, have been implemented, to the satisfaction of DevelopmentWA. (Refer to Advice Note p)
19. Public art is to be provided in accordance with Development Policy 4 – Providing Public Art, details of which are to be provided at planning condition clearance (working drawings) stage, and installed prior to occupancy to the satisfaction of DevelopmentWA. (Refer Advice Note q)
20. A Signage Strategy prepared in accordance with Development Policy 6 – Signage is to be provided at planning condition clearance (working drawings) stage and implemented thereafter, to the satisfaction of DevelopmentWA. (Refer Advice Note r)



21. Affordable housing is to be provided in accordance with Development Policy 9 – Affordable and Diverse Housing, details of which are to be submitted at planning condition clearance (working drawings) stage and delivered prior to occupation to the satisfaction of DevelopmentWA. (Refer Advice Note w)
22. Adaptable housing is to be provided in accordance with Development Policy 10 – Adaptable Housing, details of which are to be submitted at planning condition clearance (working drawings) stage and delivered prior to occupancy to the satisfaction of DevelopmentWA. (Refer Advice Note t)
23. Windows and glazed areas are not to be provided with dark or reflective tinting, visually obtrusive signage, obscured glazing or roller shutters, details of which are to be provided at planning condition clearance (working drawings) stage to the satisfaction of DevelopmentWA. (Refer Advice Note u)
24. All service areas, service related hardware and piped or wired services (such as car park gates, plant areas, fire booster cabinets, service meters, exhaust systems and air-conditioning units) are to be designed as an integral component of the development or screened from public view, to minimise any detrimental impact on the architectural quality of the building and the public realm, details of which are to be provided at planning condition clearance (working drawings) stage to the satisfaction of DevelopmentWA. (Refer Advice Note v)
25. The landowner is to provide consent to a section 70A notification pursuant to the *Transfer of Land Act 1893* being lodged with the Registrar of Titles for endorsement on the Certificate of Title for the subject lot at planning condition clearance (working drawings) stage, to the satisfaction of DevelopmentWA, alerting prospective purchasers that the land is situated in the vicinity of a transport corridor. (Refer Advice Note w)
26. Crossovers, driveways, car parking, vehicle manoeuvring spaces, circulation areas and loading/unloading areas are to be constructed, sealed, kerbed, marked, drained and maintained in accordance with the approved plans, prior to occupation, to the satisfaction of DevelopmentWA in consultation with the City of Subiaco. (Refer Advice Note x)
27. Any existing crossovers not included as part of the proposed development on the approved plan are to be removed and the verge reinstated with new kerbing to the satisfaction of DevelopmentWA, in consultation with the City of Subiaco.
28. All building and tenancy entrance levels are to be consistent with the constructed paving levels of the adjoining public realm, details of which are to be provided at planning conditions clearance (working drawings) stage to the satisfaction of DevelopmentWA.
29. Confirmation that the proposed development does not unduly impact on Perth Airport's Communication, Surveillance and Navigation Facilities, is to be provided at planning condition clearance (working drawings) stage, to the Satisfaction of DevelopmentWA, in consultation with Air Services Australia. (Refer Advice Note y)
30. The development must be substantially commenced within four (4) years from the date of this approval, to the satisfaction of DevelopmentWA. Where an approval has so lapsed, no development shall be carried out without the further approval of DevelopmentWA having first been sought and obtained. (Refer to Advice Note z)



RECOMMENDED ADVICE NOTES:

- a. With regard to Condition 1, a covering letter, final working drawings (digital) and a material samples board are to be submitted to DevelopmentWA prior to an application being made to the City of Subiaco for a building permit and must be cleared prior to the commencement of any works on site. In the event that the building permit applications are staged, DevelopmentWA may consent to a staged condition clearance approach. Such requests should be supported by a staging plan detailing the staging of works and the associated conditions to be cleared at each stage for agreement by DevelopmentWA.

Working drawings are to comply with all of the above conditions and any variations from the approved drawings and documentation are required to be clearly identified.

In accordance with section 62(3) of the *Metropolitan Redevelopment Authority Act 2011* no works are to be undertaken prior to obtaining development approval or in contravention of any condition to which the approval is subject.

Following satisfactory assessment of the working drawings, DevelopmentWA will provide a clearance letter and a digital copy of the plans to the City of Subiaco to enable building permit assessment.

- b. With regard to Condition 2, information of external colours and finishes, including specifications and samples, are to be provided for all external elevations, demonstrating a well resolved high-quality materials palette for the whole project, which has been further detailed and/or enhanced from the development application stage through to the final constructed outcome. Use of aluminium cladding within the development is not supported by the Design Review Panel.
- c. With regard to Condition 3, the Construction Management Plan should detail how construction works will be managed to minimise impacts on the public realm and nearby commercial and residential areas by addressing the following matters, which are to address all phases of development including demolition and construction:
 - i. site huts and staff amenities;
 - ii. high quality fencing/hoarding used to secure the site and associated signage;
 - iii. work hours;
 - iv. truck routes, layover areas and site access plan including consideration to heavy vehicle movements during peak pick-up and drop-off times for the surrounding schools;
 - v. parking arrangements for contractors and subcontractors;
 - vi. noise and vibration management;
 - vii. control of sand, dust and erosion;
 - viii. dewatering (if applicable) and storm water management;
 - ix. materials delivery, storage and collection and waste management;
 - x. protection of all street trees and retained on-site trees during the construction process;
 - xi. any road, footpath or cycle path closures, associated impacts on traffic, pedestrian and cyclist movement and alternate paths of travel;
 - xii. confirmation that railway access tracks and gates will remain unobstructed at all times;
 - xiii. a Consultation Plan, which identifies how stakeholders and affected landowners will be notified of any construction impacts including details of complaint resolution procedures;



- xiv. procedure for addressing unexpected finds (contamination, unexploded ordinances, etc.);
 - xv. Details of proposed crane operations, including exclusion zones, control measures, and engineering documentation, including demonstrating that tower cranes are not to be permitted to sail over the Public Transport Authority rail reserve, including as a result of free-slewing during periods of non-use; and
 - xvi. Confirmation that approval has been obtained from the Public Transport Authority for works in and around and operating railway corridor in accordance with the Public Transport Authority's Procedure 8810-450-003 – *Working In and Around the PTA Rail Corridor, Assets and Infrastructure*.
- d. With regard to Condition 5, engineering drawings and technical reports are required to address the following matters:
- i. Detailed elevations of windows and balconies facing the rail corridor confirming sufficient screening and/or devices have been implemented which prevent objects with maximum sphere of 125mm to pass through;
 - ii. All openings, including balconies, facing the rail corridor and located on floors with a floor height of no less than 2 metres above ground level, being protected with screening of a minimum height of 2.4 metres so as to not permit a 125 mm sphere to pass through;
 - iii. the compliance of the development with Public Transport Authority Specification 8880-450-083 *Design for Rail Collisions*; and
- The applicant is advised to liaise directly with the PTA to confirm an obtain all necessary approvals required prior to undertaking works in the vicinity of the rail corridor.
- e. With regard to Condition 6, the Car Parking and Traffic Management Plan should include details on the following, and be reflected in the working drawings and be informed and supported by the final Transport Impact Assessment:
- i. provision of adequate sightlines at the crossover to ensure safe movement of pedestrians and vehicles;
 - ii. vehicle access is to comply with AS2890.1;
 - iii. bicycle parking and end of trip facilities are designed in accordance with Section 2.12 of the Subiaco Oval and Railway Precinct Design Guidelines;
 - iv. identification and management of ACROD bays in accordance with AS 2890.6;
 - v. provision of electric bays in accordance with Section 2.10 Environmental Sustainability the the Subiaco Oval and Railway Precinct Design Guidelines;
 - vi. the dimensions and numbering of all car parking bays, loading bays, vehicle entrances, crossovers, aisle widths and circulation areas complying with Australian Standard (AS) 2890.1, AS2890.2, AS2890.3 and AS/NZS2890.6;
 - vii. identification of clear, safe and accessible pedestrian paths through car parking and delivery areas; and
 - viii. Identification and management of loading bays to ensure they are used for loading purposes.
- f. With regard to Condition 7, the applicant is advised to liaise with the City of Subiaco regarding on-site stormwater disposal and management.
- g. With regard to Condition 8, and noting private collection is proposed, the Waste Management Plan is to generally respond to Local Government Waste Management



Plan Guidelines (where applicable), and to suitably address the collection of day to day waste as well as recycleables and and no residential tenancies.

- h. With regard to Condition 9, the wind assessment should include a detailed wind tunnelling analysis. The wind amelioration strategies are to be integrated into the final building design, detailing and function. The use of wind break 'add-ons' such as screens and physical barriers are typically not supported as these are considered to obstruct visual and physical permeability of the public realm and limit the integration and activation of the ground floor land uses.
- i. With regard to Condition 10, the independent CPTED Report should address the safety of publicly accessible and communal areas of the development including the pedestrian links and vehicle laneways, as well as CCTV locations and management. The CPTED Statement should inform the final Landscape Plan and Lighting Strategy. All recommendaitons of the CPTED report should be implemented in the final design outcome, as demonstrated through the detailed design documents.
- j. With regard to Condition 11, the Landscape Plan should provide details of the following:
 - i. planting and tree selections, including suitability to Subiaco's climate and proposed locations, including planing on strucutre;
 - ii. specified plant numbers, species, location, size and time of planting, including provision of mature trees, avoiding species prone to Shot-Hole Borer attack;
 - iii. paving selections;
 - iv. irrigation/reticulation details and watering regime;
 - v. maintenance schedule and responsibilities;
 - vi. layout, function and integration of communal open spaces within the development to ensure they are useable;
 - vii. management strategy for communal spaces;
 - viii. street furniture;
 - ix. details of any shade structures;
 - x. clarification if there will be resident access to underlying soils; and
 - xi. Tree protection zones which is a specified area above and below ground level that is set aside for the protection of the trees structural root system. The installation of a TPZ around all City trees adjacent the development site should be installed during a development that may impact on verge trees, in consultation with an Arborist.

The applicant is advised to liaise with the City of Subiaco to ensure the integration of paving materials and landscaping between the public and private realm and the consideration of ongoing maintenance requirements.

- k. With regard to Condition 12, the Lighting Strategy should address:
 - i. include a plan which illustrates how the building will be illuminated to highlight architectural elements, to provide an attractive building at night;
 - ii. demonstrate how lighting and safe access will be provided between the development and the public realm and within loading areas and pedestrian pathways; and
 - iii. be designed to comply with Australian Standard 1158 (Public Lighting Code) and Australian Standard 4282 (Control of the Effects of Outdoor Lighting) in order to ensure that any nuisance light to adjoining properties and to passing vehicular traffic are controlled to an acceptable level.
- l. With regard to Condition 13, the Operational Management Plan should include details of the following:



- i. Community Housing Provider Roles & Responsibilities: Oversight of the development, including on-site management presence, operating hours and responsibilities, protocols for addressing building issues, including afterhours (with the expectation being that the CHP is contactable 24/7), security measures and incident response procedures (security incidents and evacuation plans).
- ii. Delivery, Collection & Waste Management: Identification of delivery, collection, and service areas with sufficient space for vehicle access and manoeuvring, ensuring all deliveries and collections occur within the development, provision of adequate loading bays and waste storage bins, including provision for collection of bulk waste items, strategies to reduce amenity and noise impacts from deliveries/waste.
- iii. Residential Communal Facilities Management: Intended operation and management of communal facilities, booking and procedures for any amenities.
- iv. Ground Floor Commercial Tenancies: Confirmation of Land Use (refer to note below), Intended operating hours, alfresco dining, waste storage and collection, liquor licensing (if applicable), noise management and how operation will minimise impacts on the surrounding area and ensure appropriate compatibility with the adjacent school site.
- v. Green Travel Plan: Information packs for staff, residents and visitors to encourage and support access to/from the site by non-car modes including identification of major public transport routes and identification of major pedestrian and bicycle routes and how to access these routes from the site.
- vi. Bicycle Storage: Tenants to be made aware that CHP, at their cost, will install a bicycle rack within their storeoom upon request.
- vii. Any other relevant operational considerations.

The applicant is advised that prior to occupation, the use of the 'Commercial' tenancies in Lot 11 (Tower 4) as either 'Shop' or 'Café/Restaurant', is to be confirmed with DevelopmentWA prior to commencement of operation. Following the commencement of the confirmed land use, the other land use will cease to have approval for that tenancy and any subsequent changes to the prevailing land use will be subject to lodgement of a development application with DevelopmentWA.

- m. With regard to Condition 14, the development should comply with the requirements of Part D3 of the Building Code of Australia (Access for People with Disabilities), Australian Standard 1428.1 and the *Disability Discrimination Act 1992*.
- n. With regard to Condition 15, to address the requirements of Development Policy 1 – Green Building the development is expected to be designed and built to achieve a minimum 5 Star Green Star rating from the Green Building Council of Australia (GBCA). Where an alternative rating system to Green Star is proposed, the applicant should demonstrate that the rating system and the design of the development is equivalent to or better than the GBCA 5 Star Green Star rating.

The final Sustainable Design Assessment Report is required to confirm that the proposal demonstrates a "whole of building" approach to sustainable design, embedding all initiatives detailed in the preliminary report into the design. Any revisions to the targets of the preliminary assessment is required to address, as a minimum, water efficiency and water reduction strategies, energy efficiency and energy efficient fixtures and appliances, and a naturally comfortable indoor environment including natural ventilation and thermal comfort levels.



- o. With regard to Condition 17, the Acoustic and Vibration Report should identify the location and extent of impact of all noise and vibration sources and confirm that the design of the building will achieve an appropriate level of attenuation, in accordance with Development Policy 3 – Sound and Vibration Attenuation. In particular, the Acoustic and/or Vibration Report should address:
 - i. potential noise impacts from adjacent activities including traffic, shared paths and railway;
 - ii. confirmation of the mitigation strategies and treatments to be implemented into the design and operation of the building; and
 - iii. mechanical service systems (such as exhaust systems and air-conditioning) are to be selected, designed and located to prevent emitted noise levels from exceeding the relevant decibel levels as provided by the *Environmental Protection (Noise) Regulations 1997*.
- p. With regard to Condition 18, the certification submitted at practical completion stage should include results of on-site testing to confirm appropriate levels of sound/vibration attenuation/mitigation have been achieved.
- q. With regard to Condition 19, Development Policy 4 – Providing Public Art requires the provision of public art with a minimum value of \$1.1million based on the estimated the construction cost of \$170 million. Contributions may be either provided as public art or cash-in-lieu paid into the Central Subiaco public art fund.

In accordance with the Providing Public Art Policy, a Public Art Report is to be submitted and approved by DevelopmentWA and is to include design documentation, detailed plans of the artwork, cost calculations, public liability insurance (as applicable) and maintenance details. The artwork(s) are to be consistent with DevelopmentWA's public art strategy for the r Subiaco Oval and Railway Precinct, where applicable. The applicant is encouraged to discuss the approach to public art with DevelopmentWA.

- r. With regard to Condition 20, the Signage Strategy should provide a plan of all proposed external signage, including location and dimensions, demonstrating that such signage will be complementary to the architectural design of the building and not obscure architectural detail and materiality in accordance with DevelopmentWA's Development Policy 6 – Signage, and the Subiaco Oval and Railway Precinct Design Guidelines.
- s. With regard to Condition 21, detailed information demonstrating that the affordable housing units meet the relevant Affordable Housing Performance Standards of Development Policy 9 – Affordable and Diverse Housing should be submitted prior to occupancy of the development.
- t. With regard to Condition 22, an Accessibility Report should be provided demonstrating that a minimum 20% of the total number of dwellings delivered as part of the development conform to the Core Liveable Housing Design Elements outlined in Development Policy 10 – Adaptable Housing.

Accessible parking spaces for these dwellings should be provided in accordance with the Adaptable Housing Policy. The applicant is advised that the development should comply with the requirements of Part D3 of the Building Code of Australia (Access for People with Disabilities) and Australian Standard 1428.1 and the *Disability Discrimination Act 1992*.



- u. With regard to Condition 23, protection of windows from the sun or privacy screening, should be achieved through architectural devices, passive solar design and appropriate glazing specifications, rather than through reflective glazing or coatings. Glazing on all lower levels should be transparent, ensuring a high level of visible indoor activity and passive surveillance of the public realm is maintained in accordance with the Subiaco Oval and Railway Precinct Design Guidelines.
- v. With regard to Condition 24, services and service related infrastructure includes but is not limited to all piped and wired services, car parking areas and associated ventilation, roof plant / plant areas, bin storage areas, service meters and related infrastructure, fire booster cabinets, exhaust systems, air-conditioning units, antennae and satellite dishes.

Fire booster hydrants and service meter boxes should be designed, located, oriented and finished to minimise any visual impact on the adjacent streetscape and maximise visual interaction between the development and the street. The applicant is advised to liaise with service authorities to rationalise and consolidate service infrastructure, where possible.

- w. With regard to Condition 25, the recommended wording for the notification on the title is:

“The land is in close proximity/within a transport corridor and is currently affected, or may in the future be affected by transport noise, vibration and/or future development and construction restrictions.”

- x. With regard to Condition 26, all internal and external car bay dimensions, ramps, crossovers, driveways and circulation aisles should be constructed in accordance with Australian Standard AS 2890.1 and the City of Subiaco parking design and access requirements. Disabled bays are to comply with Australian Standard AS 2890.6. Bicycle parking is to comply with AS 2890.3.

A separate crossover approval from the City of Subiaco is required. Please contact the City of Subiaco’s Technical Services to ensure the appropriate crossover application is made.

- y. With regard to Condition 29, Airservices Australia is required to undertake an assessment of the proposal in accordance with the *Airports (Protection of Airspace) Regulations 1996 (C’t)*. The applicant is also advised:
 - i. The development, including any ancillary structures such as roof-based plant or antennae, shall not exceed a maximum elevation of 97m AHD.
 - ii. The owner/applicant is to notify the Civil Aviation Safety Authority (CASA) of the building/obstacle via the [Vertical Obstruction Data \(VOD\) Form](#), with Perth Airport copied in to the correspondence.
 - iii. Approval is required from Perth Airport via an application made to Subiaco Airport’s Protected Airspace Assessment Tool (PAAT) for the erection of a crane on the subject site during both construction and operation of the subject site in accordance with the *Airports (Protection of Airspace) Regulations 1996 (C’t)*.
- z. With regard to Condition 30, should the development not be substantially commenced within the period specified, the approval shall no longer be valid and no development shall be undertaken without further approval of DevelopmentWA having first been sought.



- aa. For the development to demonstrate the requisite level of Design Excellence in accordance with the Subiaco Oval and Railway Precinct Design Guidelines, the final design documentation is required to verify that the Government Architect's Design Review Panel Chair Comments dated 20 April 2026, are meaningfully addressed in the clearance of Conditions 2, 10, 15 and 16 of this approval relating to materiality, CPTED and sustainability. The following principles of Design Excellence are to be maintained through to construction:
- i. the way in which the proposal goes above and beyond when addressing amenity, sustainability, durability and maintenance, and safety and security across the entire development; and
 - ii. the way in which the proposal goes above and beyond in the way in which the Lot 11 (Tower 4) portion of the development responds to its prominent, public location.